

SITE ALLOCATIONS AND POLICIES (INCORPORATING IP-ONE AREA ACTION PLAN) DEVELOPMENT PLAN DOCUMENT REVIEW

FOREWORD

Following adoption of the Local Plan review in February 2017, the Council has been cooperating in the production of an aligned local plan with the Councils that adjoin the borough boundaries and share in the same housing market with the town. The Council maintains the belief that the planning of our town is one of our key roles and as such we are, once again, pleased to set out our vision, objectives and strategy for the future development of Ipswich. It is vital that we maintain up to date policies that conform with national guidance so that we can protect and enhance the town's key assets. We continue to work for the town's future and seek to manage changes that benefit the town's existing residents, businesses and visitors.

We are pleased to set out within this document our draft policies and proposals for sites allocated for development in the town, and also other sites which we believe will benefit from protection from development.

The overarching framework for development in Ipswich Borough to 2036 is set out in the Final Draft Core Strategy and Policies Development Plan Document. The Final Draft Core Strategy also identifies Ipswich Garden Suburb as a strategic site for development in Ipswich, and the northern end of Humber Doucy Lane as a cross-border allocation for future growth.

This Final Draft Site Allocations and Policies Development Plan Document adds the site-specific detail to that strategy across the whole Borough. It continues to incorporate the 'IP-One' area of central Ipswich. It is accompanied by an updated policies map, showing on an Ordnance Survey base map sites for development and sites for protection.

To find out more please see the Council website www.ipswich.gov.uk/localplan or contact the Planning Policy team at the Council via planningpolicy@ipswich.gov.uk or telephone number 01473 432019.

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Portfolio Holder for Planning & Museums

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CHAPTER 1: Introduction

1.1 This introduction provides an explanation of:

- what the document covers;
- what status this document has and how it relates to other documents forming part of the Ipswich Local Plan; and
- how this stage of the process fits in to the production process for the Site Allocations and Policies (incorporating IP-One Area Action Plan) Development Plan Document.

What the Document Covers

1.2 The formal title of this plan is the 'Final Draft Site Allocations and Policies (incorporating IP-One Area Action Plan) Development Plan Document'. It is hereafter referred to simply as the 'Final Draft Site Allocations Plan'. When the review process has been completed, which is expected in 2020, this Site Allocations Plan will supersede the one adopted in February 2017.

1.3 The Site Allocations Plan covers three main areas of policy. Firstly it identifies a wide range of sites across the whole Borough, which should be allocated for development or afforded a degree of protection from development (Chapter 4). Secondly it sets out policies for town centre uses such as retail and leisure (Chapter 5). Finally it provides development guidelines for six opportunity areas within IP-One where significant development and public realm improvements are anticipated (Chapter 6). Policies and proposals specifically for the IP-One area are included throughout the relevant sections of the plan, with only the additional development guidelines set out in a separate chapter for the Opportunity Areas. The policies contained in the plan are referenced 'SP' meaning sites policy.

1.4 It does not cover the Borough-wide strategy for growth, or overall housing and employment figures for Ipswich. These are dealt with through the Core Strategy and Policies Development Plan Document (the 'Core Strategy'), which is also subject to review. An updated policies map and IP-One Area Inset policies map will be published alongside this Final Draft Site Allocations Plan.

1.5 This Final Draft Site Allocations Plan also includes two non-policy based parts. Part A provides the context to the whole document, which explains among other things the Ipswich Local Plan and how all Ipswich's planning documents fit together. Part D considers policy implementation, targets and monitoring proposals.

1.6 A Sustainability Appraisal incorporating Strategic Environmental Assessment has been carried out alongside the preparation of this draft plan. The full results of that exercise are available in a separate report.

What status this document has

- 1.7 The first Site Allocations Plan was adopted by the Council on 22nd February 2017 and looks ahead to 2031. It forms part of the statutory development plan with the Core Strategy and Policies Development Plan Document, together they form the Ipswich Local Plan. Therefore, it carries significant weight in the determination of planning applications via the Planning and Compulsory Purchase Act section 38(6):

“... for the purpose of any determination to be made under the planning Acts the determination must be made in accordance with the (development) plan unless material considerations indicate otherwise.”

- 1.8 Both of the plans are now under review. This document is an formal draft, setting out the Council’s final site policies and proposals, looking ahead to 2036. The production of the Site Allocations Plan will follow a five stage process as set out below:

Stage 1: Notification of the intention to prepare a plan, inviting representations on its content and considering the representations made (regulation 18)¹ – this was undertaken through Issues and Options consultation in 2017 and Preferred Options consultation in Spring 2019;

Stage 2: Publication of the plan, inviting representations in relation to its soundness, and considering the representations made (regulations 19-20) – this is the current stage of plan preparation;

Stage 3: Submission of the plan and representations to the Secretary of State (regulation 22) – this is expected to take place in Spring 2020;

Stage 4: Independent examination of the plan and publication of the Inspector’s recommendations (regulations 23-25) – this is expected to take place in Summer 2020; and

Stage 5: Adoption of the plan by the Council (regulation 26) – expected in early 2021.

- 1.9 A summary of the tests of soundness is contained at Appendix 1 and a list of all the policies contained in this plan at Appendix 2.
- 1.10 The revised Local Development Scheme (February 2019) provides more details on the various stages and the process involved in producing documents.

¹ Town & Country Planning (Local Planning) (England) Regulations 2012

Part A

The Context

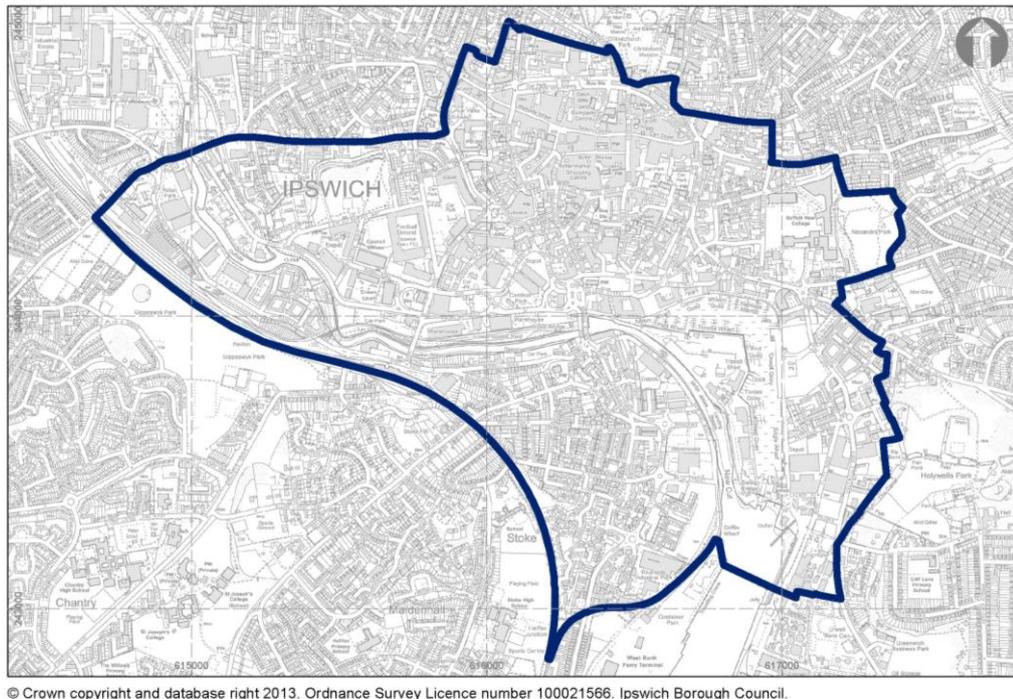
CHAPTER 2: The Ipswich Local Plan

- 2.1 The Ipswich Local Plan consists of two development plan documents (DPDs), supported by policies maps. The DPDs for Ipswich are the adopted Core Strategy and Policies Development Plan Document Review and the adopted Site Allocations and Policies (incorporating IP-One Area Action Plan) Development Plan Document. In addition to the Local Plan, there are other planning documents for Ipswich:
- Supplementary Planning Documents – for example the Local List (Buildings of Townscape Interest) and Ipswich Garden Suburb supplementary planning documents; and
 - Supporting documents: the Statement of Community Involvement Review March 2018, Local Development Scheme, Supplementary Guidance and Authority Monitoring Report.
- 2.2 The Council's adopted Core Strategy and Policies Development Plan Document Review sets out a Borough-wide growth strategy up to 2031 in terms of numbers of homes and jobs needed and also the spatial distribution of growth. The Core Strategy Review favours brownfield development early in the plan period, in order to maintain the momentum of urban regeneration around the Waterfront. Greenfield development becomes progressively more significant throughout the plan period. The Core Strategy Review identifies the Northern Fringe of Ipswich (known as the Ipswich Garden Suburb) as the main area for housing growth on greenfield land.
- 2.3 The adopted Site Allocations Plan provides the detailed policies, which give effect to certain strategic policies of the Core Strategy, including policies CS7 (housing) and CS13 (employment). The two development plan documents together form the Ipswich development plan. Proposed development is assessed against all relevant policies contained within the two plans. Both plans are under review to extend their timescale to 2036.
- 2.4 The national context for the Ipswich Local Plan is provided by the National Planning Policy Framework (NPPF). It was revised in July 2018 and February 2019. One of the tests of soundness for development plan documents is consistency with the policies in the NPPF². In addition, Planning Practice Guidance is published by the Government to provide more detailed advice to local planning authorities.
- 2.5 The Site Allocations Plan incorporates the IP-One Area Action Plan. Core Strategy Review policy CS3 commits the Council to preparing the Area Action Plan to implement the spatial strategy of urban renaissance. Therefore the key elements of the IP-One Area Action Plan have been incorporated into this Site Allocations Plan. The area covered by IP-One is indicated on the map below. It includes areas known as the Waterfront, Portman Quarter

² NPPF 2019 paragraph 35.

(previously called Ipswich Village), the Education Quarter and the town centre. It was defined as an area where significant change is expected during the plan period. Policies applying within the IP-One area and the boundaries of the areas referred to above are shown on an IP-One inset policies map.

Figure 1 IP-One Boundary



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- 2.6 IP-One consists of 301 hectares (just over 3 square kilometres) of low-lying land adjacent to the River Orwell. The ground rises to the north, east and south to ridges, which in many places are wooded and provide a distinctive and attractive green backdrop to the central area. The River represents a positive focus for regeneration, but also something of a barrier to movement, both north-south and east-west. The area includes two large conservation areas - Central (1) and Wet Dock (11) – and the smaller Stoke Conservation Area (9) and a small part of the St Helen’s conservation area (6).
- 2.7 The IP-One area divides into several identifiable areas of activity: the Central Shopping Area where retailing is the dominant use; the wider town centre where leisure (such as Ipswich Town Football Club) and office uses predominate; the Waterfront with its mix of commercial, leisure, port-related and residential uses; Portman Quarter in the west of IP-One which is a mixed-use neighbourhood primarily of homes and offices, and the Education Quarter in the east where the new University of Suffolk and Suffolk New College are the main land uses. There is a degree of overlap between some of the areas.

Key Issues for the Site Allocations Plan

2.8 The Council is required to keep under review matters which may be expected to affect the planning and development of the Borough³. Thus a body of evidence underpins the Final Draft Core Strategy and the Final Draft Site Allocations Plan. Key elements of the local evidence base include:

- Ipswich Economic Area Sector Needs Assessment, September 2017,
- Ipswich Employment Land Supply Assessment, April 2018,
- the Ipswich Strategic Housing and Employment Land Availability Assessment update November 2018 and updated November 2019 (SHELAA),
- the Ipswich and Waveney Housing Market Areas Strategic Housing Market Assessment (SHMA) Part 1 and Part 2 (including January 2019 update) Reports, May 2017,
- New Anglia Local Enterprise Partnership Economic Strategy for Norfolk and Suffolk 2017,
- Ipswich and Suffolk Coastal Retail and Commercial Leisure Study 2017 and Retail Position Update Statement August 2019,
- Settlement Sensitivity Assessment Volume 1: Landscape Fringes of Ipswich, July 2018,
- 2011 Census ONS,
- Authority Monitoring Report 13 2017-2018,
- Ipswich Strategic Flood Risk Assessment 2011 (SFRA) and 2019 refresh,
- Ipswich Wildlife Audit Update 2012-13 and 2019 Review,
- Suffolk County Council Historic Environment Record and Urban Archaeological Database;
- Ipswich Transport Modelling, 2019, and
- Gypsy, Traveller, Travelling Showpeople and Boat Dwellers Accommodation Needs Assessment for Ipswich, Babergh, Mid Suffolk, Suffolk Coastal and Waveney, in partnership with Suffolk County Council, 2017.

2.9 These documents and others may be found on the Council's website www.ipswich.gov.uk within the Planning Policy Core Document Library and on the page entitled New Evidence Base Documents for the Ipswich Local Plan Review to 2036.

2.10 The key strategic challenges for Ipswich are identified in the Core Strategy Review paragraph 5.25. Some of the more detailed issues emerging from the evidence, which this plan needs to address, are as follows:

- allocating sufficient land to meet objectively assessed housing need in accordance with the National Planning Policy Framework;
- allocating enough good quality land to attract and retain jobs within the Borough;

³ *Planning and Compulsory Purchase Act 2004, Part 2 Section 13.*

- strengthening the role of Ipswich town centre in response to ongoing changes to the way in which it serves Ipswich's residents and visitors and allocating suitable and available sites for future town centre development;
- managing the additional travel demands that growth will generate and guiding as many trips as possible to sustainable modes for the good of the environment, economy and health;
- tackling deprivation in the worst affected wards;
- supporting the completion of key regeneration projects at the Waterfront;
- enhancing pedestrian and cycle connections across and around the town, particularly between the Waterfront and town centre;
- addressing the conservation and enhancement of heritage assets and biodiversity;
- ensuring that policies and allocations support the health and wellbeing of people in Ipswich; and
- ensuring that demands placed on new development do not render it non-viable.

CHAPTER 3: Vision and objectives

- 3.1 The Core Strategy Review vision for Ipswich Borough provides the context for this more detailed Site Allocations Plan. The Core Strategy Review vision is as follows:

The Core Strategy Review Vision

Our Vision is to improve the quality of life, health and well-being for all who live in, work in, learn in and visit Ipswich by supporting growth and ensuring that development happens in a sustainable manner so that the amenities enjoyed by local people are not harmed and the town is enhanced.

By 2036, Ipswich town centre will embrace a greater mix of uses providing opportunities for shopping, leisure and cultural activities, learning, working and living. The town centre, Waterfront and Portman Quarter will provide a range and choice of higher density homes to meet needs including on a redeveloped Island Site, whilst recognising that not everyone wishes to live in a flat. The Princes Street corridor will be a busy office quarter providing modern, flexible Grade A floorspace. Movement around the town centre will be easiest by foot and cycle, using an integrated network of safe, convenient and attractive routes, supported by a high-quality public transport network. The town centre will be inclusive, addressing the needs of the less mobile within a high quality public realm.

Outside central Ipswich, thriving district and local centres will provide local shopping, leisure and services close to people's homes and be at the hub of strong and cohesive communities. Three new neighbourhoods will be completed at Ipswich Garden Suburb with housing delivered alongside a country park and highway, water, energy, education, green and health infrastructure.

By 2036, a range of new homes will be provided across the Borough and within the Housing Market Area to meet needs. The Ipswich economy will have grown hand in hand with the skills and knowledge of the local workforce, providing opportunities for all as part of a strong and prosperous Ipswich Functional Economic Area supported by a successful university. Growth will be supported by modern, fit for purpose infrastructure, and Ipswich will be well connected and offer a range of sustainable transport modes to all its users. Future development will be adaptable to the implications of climate change impact.

The distinctive network of beautiful parks and open spaces, green infrastructure and open water will be protected and enhanced, to support their use by people and wildlife. The town's tree canopy will be extended and opportunities taken to enhance biodiversity and adapt to climate change. The town's built, natural and historic environments will be conserved and

enhanced and growth will respect the character and distinctiveness of neighbourhoods within Ipswich.

The Ipswich of 2036 will have grown and changed in ways that respect the past and look to the future; it will be a county town that everyone in Suffolk will feel proud of.

3.2 The Core Strategy Review goes on to identify objectives in paragraph 6.8. Those with a geographical element which will need to be addressed through this Site Allocations Plan are:

Objective 2 At least (a) 8,010 new dwellings shall be provided to meet the needs of Ipswich within the Housing Market Area between 2018 and 2036 in a manner that addresses identified local housing needs and provides a decent home for everyone, with 31% at the Ipswich Garden Suburb and 15% in the remainder of the Borough being affordable homes; and (b) approximately 9,500 additional jobs shall be provided in Ipswich to support growth in the Ipswich Strategic Planning Area between 2018 and 2036.

Objective 3 The development of the Borough should be focused primarily within the central Ipswich 'IP-One' area, Ipswich Garden Suburb, the Northern end of Humber Doucy Lane and within and adjacent to identified district centres (these areas are identified on the key diagram).

Objective 4 Development must be sustainable, environmentally friendly and resilient to the effects of climate change.

Objective 5 Every development should contribute to the aim of reducing Ipswich's carbon emissions below 2004 levels.

Objective 6 To improve accessibility to and the convenience of all forms of transport, and achieve significant modal shift from the car to more sustainable modes. This will: (a) promote choice and better health; (b) facilitate sustainable growth, development and regeneration; (c) improve integration, accessibility and connectivity; and (d) promote green infrastructure as alternative 'green' non-vehicular access around the town and urban greening of existing routes. Specifically:

- Significant improvements should take place to the accessibility to and between the three key nodes of: the railway station (including the wider Portman Quarter environment), the Waterfront (and particularly the Education Quarter) and the Central Shopping Area;
- Additional east-west highway capacity could be provided within the plan period in the Ipswich area to meet the needs of the wider population and to provide the potential to reallocate some central road space;
- Comprehensive, integrated cycle routes should be provided; and
- Ipswich Borough Council aspires to an enhanced public transport system.

Objective 7 To enhance the vitality and viability of the town centre and district centres in response to changing consumer habits.

Objective 8 A high standard of design will be required in all developments. Development should conserve and enhance the historic environment of Ipswich, including historic buildings, archaeology and townscape.

Objective 9 To protect and enhance high quality, accessible strategic and local open spaces rich in biodiversity and geodiversity for people to visit and use.

Objective 10 To retain and provide high quality and sustainable education, health and sports facilities and other key element of community infrastructure to meet local demand.

Objective 11 To tackle deprivation and inequalities across the town.

- 3.3 As part of the Ipswich Local Plan, this Final Draft Site Allocations Plan aims to deliver the Draft Core Strategy Review vision through the policies and proposals contained in the following chapters.

The Site Allocations Plan incorporates the IP-One Area Action Plan referred to in Core Strategy policy CS3. The IP-One area, as explained in Chapter 2, covers central Ipswich including the town centre. The Council is a signatory to the Ipswich Vision document for the town centre, which is called 'Turning our Town Around' and was published in 2015. It sets out a vision for the town centre as follows. The Ipswich Vision is to create 'East Anglia's Waterfront Town' and demands:

- A town centre that will attract new investment
- A town centre that is true to its history
- A town centre that is bold and ambitious
- A town centre that recognises the need for change
- A town centre that will excite those who visit it
- A town centre that will appeal to those beyond its immediate catchment.

- 3.4 Where appropriate and consistent with the evidence base, the draft policies and proposals set out in this plan support the achievement of the Waterfront Town Centre vision, for example, by allocating sites for development and protecting the town's built heritage.

- 3.5 The Vision brings together the aspirations of the partners on a range of issues and identifies a series of actions for the next few years. Some of these are relevant to the Local Plan and others are not, because they relate to matters beyond the remit of the planning system. The two documents (the statutory

'Local Plan' and the 'Vision for Ipswich') are considered to complement each other in a helpful way.

Part B

The Policies

CHAPTER 4: Site Allocations

- 4.1 The Final Draft Core Strategy identifies targets for housing, jobs and retail growth through policies CS7, CS13 and CS14. This chapter sets out the site allocations to deliver some of that growth. Other Final Draft Core Strategy policies also address the matter, for example CS10 Ipswich Garden Suburb. Ipswich town centre retailing is dealt with in Chapter 5 'IP-One Area', but housing, employment, mixed use, open space and community facility site allocations are addressed below.
- 4.2 The threshold site size for making allocations is 0.1ha or 10 dwellings in the IP-One Area, and 0.2ha or 10 dwellings in the rest of the Borough. Sites below these thresholds are not usually allocated but may come forward. They are accounted for in supply calculations through small sites allowances (where they already have planning permission) or windfall allowances.

Policy SP1 The Protection of Allocated Sites

- 4.3 This policy introduces measures to reserve allocated sites for the uses proposed and thereby means any inappropriate alternative uses would be a departure from the development plan.

Policy SP1 The protection of allocated sites

Sites will be safeguarded for the use(s) for which they have been allocated. The Council will only permit alternative uses on allocated sites if they are compatible with other plan objectives and policies, they do not harm the plan strategy and the applicant can demonstrate that the allocated use is:

- a. **No longer needed to meet planned development needs; or**
- b. **Not viable or deliverable and likely to remain so during the plan period.**

Where an allocated mix of uses is not viable or deliverable, the Council will prioritise the primary use and community uses (including open space) identified through the policies and the site sheet at Appendix 3A and negotiate the remainder of the mix.

- 4.4 The Council has specific requirements for the delivery of housing, jobs and retail development in the period up to 2036. Sites are allocated in this plan to enable the targets to be met. The allocations need to be backed by policy to ensure that they cannot easily be developed for alternative uses and thereby harm the Council's ability to achieve its growth targets. To demonstrate that the allocated use is not viable or deliverable in accordance with clause b., applicants will be required to produce evidence that the site has been marketed actively for a continuous period of at least twelve months from the date of the first advertisement for the allocated use, in a manner consistent with the Council's marketing requirements set out in Appendix 6 of the Final Draft Core Strategy.

Policy SP2 Land Allocated for Housing

- 4.5 Through Final Draft Core Strategy policy CS7 'The Amount of Housing Required', the Council commits to allocating land to provide at least an additional 6,100 dwellings net to 2036. The following policy provides the detailed site allocations to deliver a proportion of that housing requirement.

Policy SP2 Land allocated for housing

The following sites are allocated for residential development, or part residential development within mixed use developments as indicated in Table 1. Development will take into account appropriately the constraints identified through the site sheets contained in Appendix 3 of the plan.

Table 1 Land allocated for residential use or residential-led mixed use

| Site ref. | Site name and development description | Site size ha (% residential on mixed use sites) | Indicative capacity (homes) | Capacity evidence | Likely delivery timescale (S, M, L) |
|-----------|--|---|-----------------------------|----------------------------------|-------------------------------------|
| IP003 | Waste tip and employment area north of Sir Alf Ramsey Way Parts of the site are covered by facility retention policies of the Suffolk County Council Minerals and Waste Local Plan. Alternative sites will need to be agreed with the County Council and the site operators for the relocation of the Concrete Batching Plant and Household Waste Recycling Centre before the sites can be made available | 1.41 (90%) | 114 | 90dph (DM23a lower end of range) | L |
| IP004 | Bus depot, Sir Alf Ramsey Way Allocated for mixed residential & B1 office | 1.07 (c. 50%) | 48 | 90dph (DM23a lower end of range) | L |

| Site ref. | Site name and development description | Site size ha (% residential on mixed use sites) | Indicative capacity (homes) | Capacity evidence | Likely delivery timescale (S, M, L) |
|-----------|---|---|-----------------------------|--|-------------------------------------|
| | use; historic depot to be retained and converted as part of B1. | | | | |
| IP009 | Victoria Nurseries, Westerfield Road | 0.39 | 12 | 30dph (DM23c). Low density to reflect suburban location. | S |
| IP010 a | Co-op Depot, Felixstowe Road Approximately 25% of the site is safeguarded for an extension to Rosehill School. | 2.22 (c.75%) | 75 | 45dph (DM23b) | M |
| IP010 b | Felixstowe Road Current uses retained on c. 50% of site (including Hughes and BT). | 2.79 (c. 50%) | 62 | 45dph (DM23b) | L |
| IP011 a | Lower Orwell Street former Gym & Trim (formerly listed as Smart Street/Foundation Street), | 0.15ha | 18 | 110dph on 100% of site DM23 | M |
| IP011 b | Smart Street, Foundation Street (South) Redevelopment is dependent on the appropriate relocation of existing uses. | 0.62 | 56 | 90dph (DM23a lower end of range) | L |
| IP011 c | Smart Street, Foundation Street (North) Allocated for residential | 0.08 | 7 | 90dph (DM23a lower end of range) | M |

| Site ref. | Site name and development description | Site size ha (% residential on mixed use sites) | Indicative capacity (homes) | Capacity evidence | Likely delivery timescale (S, M, L) |
|-----------|---|---|-----------------------------|--|-------------------------------------|
| | development Site IP011b has been split to reflect the ownerships. | | | | |
| IP012 | Peter's Ice Cream | 0.32 | 35 | 110dph (DM23a higher end of range)). | M |
| IP014 | Hope Church Redevelopment is dependent on the appropriate relocation of existing uses. | 0.21 | 23 | 110dph (DM23a, higher end of range) | M |
| IP015 | West End Road Surface Car Park Primary allocation for long stay parking with secondary residential. The site is to be masterplanned with IP083 adjacent. | 1.22 (c. 55%) | 67 | 100dph (DM23a) | M |
| IP031 a | Car Park, Burrell Road | 0.44 | 20 | 45dph (DM23b). | M |
| IP031 b | 22 Stoke Street | 0.18 | 18 | 100dph (reflective of highly accessible location. Maximum dph due to heritage constraints) | S |
| IP032 | King George V Field, Old Norwich Road Allocated for 80% residential and 20% open space. The allocation is subject to the provision of | 3.7 (c. 80%) | 99 | 35dph (Development Brief) | M |

| Site ref. | Site name and development description | Site size ha (% residential on mixed use sites) | Indicative capacity (homes) | Capacity evidence | Likely delivery timescale (S, M, L) |
|-----------|---|---|-----------------------------|--|-------------------------------------|
| | replacement playing fields and ancillary facilities (e.g. changing rooms and spectator accommodation) in a suitable location. | | | | |
| IP033 | Land at Bramford Road (Stocks site) Allocated for 50% residential and 50% open space. | 2.03 (50%) | 55 | 55dph (DM23b / SHELAA) | S |
| IP035 | Key Street / Star Lane / Burtons (St Peter's Port) Residential-led mixed use scheme. Additional uses could include office, leisure or small scale retail. | 0.54 (80%) | 86 | DM23a at a higher density of around 200 dph – below the density of development on the northern quays to reflect the historic environment setting | M |
| IP037 | Island Site Allocated for housing and open space alongside existing Marina and small commercial uses to support enterprise zone. These proportions will be firmed up through the preparation of a master plan. See also Opportunity Area development principles and guidelines in Part C. Additional vehicular | 6.02 (c. 70%) | 421 | 100dph (DM23a midrange) | L |

| Site ref. | Site name and development description | Site size ha (% residential on mixed use sites) | Indicative capacity (homes) | Capacity evidence | Likely delivery timescale (S, M, L) |
|-----------|---|---|-----------------------------|--|-------------------------------------|
| | and foot/cycle access (including emergency access) will need to be provided to enable the site's development. | | | | |
| IP039a | Land between Gower Street & Gt Whip Street | 0.48 | 45 | 95dph (DM23a lower-mid range) | L |
| IP040 | Former Civic Centre, Civic Drive (Westgate) This site is allocated for 10% retail and leisure development at ground/first floor level but primarily residential use. | 0.73 (90%) | 59 | 90dph (DM23a) | S |
| IP041 | Former Police Station, Civic Drive | 0.52 | 58 | 110dph (DM23a higher end of range). | S |
| IP043 | Commercial Buildings, Star Lane | 0.70 (80%) | 50 | 90dph (DM23a lower end of range). 11/00267/FUL for 65 student rooms pending although unlikely to proceed. | S |
| IP047 | IP047 Land at Commercial Road | 3.11ha | 173 | 55dph on 80% as part of a mixed use scheme of 80% housing, 5% hotel/ | S/M |

| Site ref. | Site name and development description | Site size ha (% residential on mixed use sites) | Indicative capacity (homes) | Capacity evidence | Likely delivery timescale (S, M, L) |
|-----------|--|---|-----------------------------|---|-------------------------------------|
| | | | | leisure/ retail, 5% public open space and 10% enhanced river path, Based on pending application (19/00148/O UT) | |
| IP048 a | Mint Quarter / Cox Lane East regeneration area Primary school and car parking development to the north of Upper Barclay Street, retaining the locally listed façade to Carr Street. Residential development to the south of Upper Barclay Street. Development to include new public open space and short stay parking in a medium sized multi-storey car park (location in relation to Cox Lane to be determined). A development brief for the whole site will be prepared but development will come forward incrementally. | 1.33 (c. 40%) | 53 | 100dph (DM23a mid-range) | M |
| IP048 b | Mint Quarter / Cox Lane West regeneration area | 1.34 (c. 30%) | 36 | 90dph (DM23a lower end of | L |

| Site ref. | Site name and development description | Site size ha (% residential on mixed use sites) | Indicative capacity (homes) | Capacity evidence | Likely delivery timescale (S, M, L) |
|-----------|---|---|-----------------------------|--|-------------------------------------|
| | Residential and retail mix incorporating short stay car parking for shoppers and civic/open space. A development brief for the whole site (a and b) will be prepared but development will come forward incrementally. | | | range) | |
| IP054 b | Land between Old Cattle Market and Star Lane The site now excludes the former Archant site to the east of Turret Lane and is allocated primarily for residential use alongside small scale retail and leisure and an extended or replacement electricity sub-station. | 1.08 (60%) | 40 | 60dph. High density area (DM23a) but a mix of flats and town houses would fit the character of locality. Hence density higher than medium range. | L |
| IP061 | Former School Site, Lavenham Road Allocated for part development (60%) on the basis of improving the remainder (40%) of the open space. The south-west corner of the land (0.18ha) is being developed (18/00991/FPC) as 4 general housing units and 4 respite care units and has been excluded from the site area. | 0.9 (60%) | 23 | 40dph (DM23 b.- c.) | M |
| IP064 | Land between | 1.20 | 66 | 45 dph | M |

| Site ref. | Site name and development description | Site size ha (% residential on mixed use sites) | Indicative capacity (homes) | Capacity evidence | Likely delivery timescale (S, M, L) |
|-----------|---|---|-----------------------------|--|-------------------------------------|
| a | Holywells Road and Holywells Park Redevelopment is dependent on the appropriate relocation of existing uses | | | (DM23b within IP-One area and close to Waterfront | |
| IP066 | JJ Wilson and land to rear at Cavendish Street Redevelopment is dependent on the appropriate relocation of existing uses | 0.85 | 55 | 65dph (DM23b higher than average density as adjacent to IP-One). | M |
| IP067 a | Former British Energy Site This is the northern section only and is subject to resolving odour issues to the satisfaction of IBC | 0.38ha | 17 | 45dph | L |
| IP080 | 240 Wherstead Road | 0.49 | 27 | 55dph (DM23b). Linear layout would allow for slightly higher than average density. | S |
| IP089 | Waterworks Street | 0.31 | 23 | 90dph less 5 existing units (DM23a lower end of range) | L |
| IP096 | Car Park Handford Road East | 0.22 | 22 | 100dph (DM23a lower end of range) | M |

| Site ref. | Site name and development description | Site size ha (% residential on mixed use sites) | Indicative capacity (homes) | Capacity evidence | Likely delivery timescale (S, M, L) |
|-----------|---|---|-----------------------------|--|-------------------------------------|
| IP098 | Transco, south of Patteson Road | 0.57 | 62 | 110dph (DM23 higher end of range) | M |
| IP105 | Depot, Beaconsfield Road | 0.33 | 15 | 45dph (DM23b) | M |
| IP119 | Land east of West End Road Redevelopment is dependent on the appropriate relocation of existing uses | 0.61 (45% to avoid development directly opposite sub station) | 28 | 125dph (DM23a lower end of range) | L |
| IP120 b | Land west of West End Road Redevelopment is dependent on the appropriate relocation of existing uses | 1.03 (80% to avoid development adjacent to sub station) | 103 | 125dph (DM23a lower end of range) | L |
| IP125 | Corner of Hawke Road and Holbrook Road | 0.25 | 15 | 60dph | S |
| IP132 | Former St Peters Warehouse Site, 4 Bridge Street | 0.18ha | 73 | 73 dwellings as part of a mixed use scheme (with B1a office use, leisure or small scale retail) | M |
| IP133 | South of Felaw Street | 0.37 | 45 | 120dph (DM23Based on 10/00418/VC for 47 dwellings expired Feb 2014 & pre-application discussion) | M |

| Site ref. | Site name and development description | Site size ha (% residential on mixed use sites) | Indicative capacity (homes) | Capacity evidence | Likely delivery timescale (S, M, L) |
|-----------|---|---|-----------------------------|---|-------------------------------------|
| IP135 | 112-116 Bramford Road, Application for car wash approved 17/00266/FUL. Temp permission expires 01.10.2019 | 0.17ha | 19 | 110dph based on location within Local Centre) | M |
| IP136 | Silo, College Street This site is primarily allocated for residential with secondary uses to include offices, leisure and/or small scale retail. | 0.16 (c. 80%) | 48 | Assume 6 flats per floor within 10 storey development with commercial on lower two floors (DM23a and higher than average density consistent with other schemes along Quay). | L |
| IP143 | Former Norsk Hydro, Sandyhill Lane | 4.51 | 85 | 17/00769/O UT | S |
| IP150d | Land south of Ravenswood – Sports Park (part adjacent to Alnesbourn Crescent only – to be master planned) | 1.8 | 34 | DM23 c. low density as part of mixed use with sports park | M |
| IP150e | Land south of Ravenswood (excluding area fronting Nacton Road) – to be master planned | 3.6 | 126 | DM23 c. low density as part of mixed use with B1 employment uses | M |
| IP172 | 15-19 St Margaret's | 0.08 | 9 | DM23110dph | M |

| Site ref. | Site name and development description | Site size ha (% residential on mixed use sites) | Indicative capacity (homes) | Capacity evidence | Likely delivery timescale (S, M, L) |
|------------|---|---|-----------------------------|---|-------------------------------------|
| | Green | | | . Planning permission for student accommodation lapsed and unlikely to proceed (08/00511/FUL) | |
| IP188 | Websters Saleyard site, Dock Street | 0.11 | 9 | As per permission (19/00173/FUL). | S |
| IP221 | Flying Horse PH, 4 Waterford Road | 0.35 (c. 50% retaining the public house) | 12 | 35dph (DM23c). Application for 12 dwellings in application 06/01007/FUL expired. | L |
| IP279 B(1) | Land north of Former British Telecom Office, Bibb Way | 0.44 | 18 | Based on master planning work with IP279A | S |
| IP279 B(2) | Land south of Former British Telecom Office, Bibb Way | 0.61 | 29 | Based on master planning work with IP279A | S |
| IP307 | Prince of Wales Drive | 0.27 | 12 | DM23 b. | S |
| IP309 | Former Bridgeward Social Club, 68a Austin Street | 0.28 | 15 | 54dph DM23 b. (Higher end based on proximity to local centre)_ | S |
| IP354 | 72 (Old Boatyard) Cullingham Road | 0.34 | 24 | 70dph (Less than 90dph | S |

| Site ref. | Site name and development description | Site size ha (% residential on mixed use sites) | Indicative capacity (homes) | Capacity evidence | Likely delivery timescale (S, M, L) |
|--------------|--|---|-----------------------------|---|-------------------------------------|
| | | | | due to site constraints) | |
| IP355 | 77-79 Cullingham Road Site needs to safeguard capacity for a footpath and cycle path through the site to connect Cullingham Road with Bibb Way. | 0.06 | 6 | 100dph (based on location in Portman Quarter) | S |
| Total | | | 2,750 | | |

Policy SP4 allocates land for residential development, or part residential development within mixed use developments, to be known as ‘opportunity sites’. These are sites which the Council wishes to see redeveloped and regenerated but where constraints may result in later delivery.

- 4.6 The Council is required to support the Government’s objective to significantly boost the supply of homes and, through the Local Plan, identify a sufficient supply and mix of sites, taking into account their availability, suitability and viability.⁴
- 4.7 The indicative capacity of the sites in table 1a listed in the policy above is 2,750 dwellings. These will contribute to meeting the minimum housing requirement of 8,010 dwellings by 2036, as identified through Policy CS7 of the Final Draft Core Strategy. In addition, the Final Draft Core Strategy allocates land for the development of approximately 3,500 dwellings at Ipswich Garden Suburb (the Ipswich Northern Fringe) through policy CS10, with delivery expected to start in 2019 and end in 2036. The Core Strategy review also identifies a cross-border allocation for future development (within Ipswich Borough and Suffolk Coastal Local Plan area) for housing delivery, appropriately phased with the delivery of the Ipswich Garden Suburb and its associated infrastructure at the northern end of Humber Doucy Lane adjacent to Tuddenham Road, through policy ISPA4.
- 4.8 The housing land requirement and supply figures are set out in Table 2 of the Final Draft Core Strategy. The likely delivery timescale shown in the policy shows the Council’s expectation based on current knowledge; sites could come forward sooner than indicated.

⁴ NPPF paragraph 59 and 67

- 4.9 The Government considers that small and medium sized sites can be built out relatively quickly. Therefore, the Council is required to ensure that 10% of the housing requirement will be delivered on sites of under 1ha (NPPF paragraph 68). In the supply as identified, 26% of the capacity is on sites of under 1ha.
- 4.10 Within mixed use schemes, the use proportions indicated in the policy represent the Council's preferred outcome and form the basis of supply calculations. However, the figures are indicative other than in the case of open space requirements and community facilities, to help ensure that developments can be viably delivered. Housing delivery will be closely monitored through the Authority Monitoring Report.
- 4.11 Appendix 3 provides additional information about the sites allocated through this policy. Information on development constraints contained in the site sheets must be taken into account in development proposals, in accordance with the Ipswich Local Plan Development Management policies. An Urban Archaeological Database for Ipswich has been established, to make available desk-based information on known heritage assets.
- 4.12 In allocating sites for development the Council has followed the sequential approach, to ensure that sites are not allocated in areas with a greater probability of flooding if sites in lower risk areas are available. It has also applied the exception test to ensure that the benefits to the community of development outweigh flood risk, and ensure that development will be safe. Planning applications for sites which lie within Flood Zones 2 and 3 will need to be supported by a Flood Risk Assessment. The Council's supplementary planning document on Development and Flood Risk provides more guidance.
- 4.13 Housing is a key issue for older people. There is a need to ensure a suitable mix of housing for older people, whether market housing or types of supported housing. The Council encourages new housing to be built to be accessible and adaptable, which makes it easier for people to remain in their own homes as their mobility needs change. In order to create an environment which enables older people to have a good quality of life, the Council supports the implementation of the 'Lifetime Neighbourhoods' principle in new development. This concept covers the built environment, access to services and resident empowerment in addition to housing mix and design.

Policy SP3 Land with Planning Permission or Awaiting a Section 106

- 4.14 There are sites in the Borough which currently:
- have planning permission for schemes that have not commenced to date; or
 - have planning permission for residential schemes that have commenced and stalled; or
 - have a committee resolution to grant planning permission but await the signing of a Section 106 Agreement; or
 - have planning permission for student accommodation but have not commenced to date.
- 4.15 Such sites form an important potential supply of housing land, but given changes in the market the schemes originally envisaged may not come forward. Therefore, policy needs to be put in place to guide the potential future development of the sites and safeguard or allocate them for residential use, should the planning permissions fail to be issued or implemented or the development fail to be completed as originally envisaged.

Policy SP3 Land with planning permission or awaiting a Section 106 Agreement

As at 1st April 2019, the sites listed in Table 2 below have planning permission for residential development, student accommodation or mixed use development including residential use, which has not been implemented or has started and then stalled, or are awaiting the completion of a Section 106 Agreement. Should the permissions fail to be issued or implemented and lapse during the plan period or the development fail to come forward or be completed in accordance with the permission, the Council will protect the sites for residential use or residential-led mixed use.

Table 2 Sites with planning permission or awaiting a Section 106 Agreement

| Site | Address and current situation | Site area ha (% for residential) | Capacity | Evidence | Time-scale |
|-------|---|----------------------------------|----------|--|------------|
| IP005 | Former Tooks Bakery, Old Norwich Road 80% residential and c. 20% is safeguarded for the provision of a new | 2.8 (c. 80%) | 60 | 45dph (Development Brief) Capacity in line with draft | S |

| Site | Address and current situation | Site area ha (% for residential) | Capacity | Evidence | Time-scale |
|-------------|--|----------------------------------|----------------|--|------------|
| | health centre. | | | proposals) (16/00969/FPI3) | |
| IP042 | Land between Cliff Quay and Landseer Road | 1.64 | 222 | 15/01040/OUT awaiting a S106 | M |
| IP048 C | 6-10 Cox Lane and 36-46 Carr Street (upper floors) | 0.2 | 33 | 18/00740/P3JPA | S |
| IP054 a | 30 Lower Brook Street | 0.56 | 62 | 16/01037/FUL | S |
| IP059 a & b | Arclion House and Elton Park, Hadleigh Road | 2.63 | 103 within IBC | 16/01220/OUT | S/M |
| IP074 | Land at Upper Orwell Street | 0.07 | 9 | 16/01179/FUL approved 03.02.17 | S |
| IP088 | 79 Cauldwell Hall Road | 0.30 | 17 | 17/01115/VC approved 22/02/18 | S |
| IP106 | 391 Bramford Road | 0.33 | 11 | 19/00045/FUL | S |
| IP109 | R/O Jupiter Road & Reading Road | 0.42 | 13 | 12/00192/FUL (pending) | S |
| IP116 | St Clement's Hospital Grounds | 11.85 | 108 | 14/00721/OUT 108 dwellings outstanding at 01/04/2019 | S |
| IP131 | Milton Street | 0.28 | 9 | 15/01158/FUL (& 18/00552/FUL) | M |
| IP142 | Land at Duke Street Allocation to provide for public open space (25%). | 0.39 (75%) | 44 | 90dph (DM23a lower end of range) (17/00570/FUL) | M |
| IP150 a | Ravenswood U, V, W | 2.23 | 94 | 07/00765/OUT for part of outline site | S |
| IP161 | 2 Park Road | 0.35 | 14 | 19/00065/FUL (pending) | S |
| IP165 | Eastway Business Park, Europa Way | 2.08 | 78 | As per approved scheme (13/00943/OUT) | S |

| Site | Address and current situation | Site area ha (% for residential) | Capacity | Evidence | Time-scale |
|-------|-------------------------------|----------------------------------|----------|--|------------|
| | | | | and now on site. Under construction 78 outstanding. | |
| IP169 | 23-25 Burrell Road | 0.08 | 4 | 12/00087/FUL approved 22.03.2012. Under construction 4 outstanding. | S |
| IP200 | Griffin Wharf, Bath Street | 0.79 | 113 | 17/00382/FUL pending. Previous pps: 11/00507/FUL approved 01.09.2011 (132 flats) 05/00819/FUL. | S |
| IP205 | Burton's, College Street | 0.19 | 14 | 02/01241/FUL (196 in total, 71 completed April 2014). Application 19/00624/FP13 on part of site for 14 self-contained flats) | S/M |
| IP206 | Cranfields, College Street | 0.71 | 134 | 04/00313/FUL (337 in total, 197 completed April 2014) 16/00092/VC (replaced 16 of the flats with 43 (29 studios + 14 one-beds) | S |
| IP211 | Regatta Quay, Key Street | 0.85 | 156 | 09/00130/FUL (pending) 05/00296/FUL added 25 extra units. | S |

| Site | Address and current situation | Site area ha (% for residential) | Capacity | Evidence | Time-scale |
|---------|--|----------------------------------|--------------|---|------------|
| | | | | 150 (at winerack) and 6 at 16/00346/FUL | |
| IP214 | 300 Old Foundry Road | 0.02 | 12 | 10/00805/VC expired Dec 2013. 17/00144/FUL | S |
| IP245 | 12-12a Arcade Street | 0.06 | 14 | 18/00899/FUL | S |
| IP256 | Artificial hockey pitch, Ipswich Sports Club, subject to the requirements of policy DM5 being met. | 0.6 (excludes access road) | 28 | 16/00987/FUL. Awaiting S.106 | S |
| IP279 a | Former British Telecom, Bibb Way | 0.63 | 104 | 18/00470/P3JPA | S |
| IP283 | 25 Grimwade Street. Student Union Club and adjacent car park, Rope Walk | 0.27 | 14 | 17/00049/FUL | S |
| | Total | | 1,470 | | |

4.16 Sites with planning permission or a resolution to grant planning permission would not normally be allocated through a development plan, as the expectation is that in normal market conditions they would come forward for development in due course. However, it is considered necessary to include this policy in the plan because of:

- The residential capacity of the Borough taking into account the tightly drawn boundary; and
- The number of schemes for high density flats within IP-One, which have been affected by weak market conditions and may be revised to slightly lower density schemes. The trend towards lower densities has been observed on other sites within the Ipswich housing market, for example at Griffin Wharf.

4.17 The Council's concern is to retain the principle of residential or residential-led mixed use on the sites, which form an important element of the housing land supply. Information about constraints affecting the sites is set out in Appendix 3B.

- 4.18 The sites with planning permission for residential development listed in the policy above have already been included in supply calculations in Final Draft Core Strategy Table 2 and, therefore, are not counted again here. The delivery of the sites will be closely monitored through the Authority Monitoring Report.
- 4.19 The success of the University of Suffolk is a priority for the Council. Sufficient student accommodation is needed to support the University's targets for student numbers and ensure that potential impacts on residential neighbourhoods can be managed. The University is not a direct provider of bespoke student accommodation, but relies on the market to make provision, which the University accredits if appropriate. Therefore the University's approach to provision by developers is to accredit accommodation up to the number of bed spaces needed, but not beyond.
- 4.20 Experience in Ipswich to date indicates that there are more student bed spaces with planning permission than the University needs. If planning permissions for student accommodation are not implemented, the Council will encourage the alternative development of the sites for housing use not limited to students, on the basis that the market for student accommodation is saturated.

Policy SP4 Opportunity Sites

Policy SP4 Opportunity Sites

Four opportunity sites have been identified within IP-One that have potential for housing-led redevelopment and would contribute to the regeneration of the Waterfront and Town Centre. These are listed below and described in Appendix 4. The Council will work with land owners and other interested parties to investigate opportunities and bring them forward through the development management process, taking into account constraints set out in the Appendix 4 site sheets.

IP028b Jewsons, Greyfriars Road

Potential mix: residential, leisure (A3-A5/D2), office

Indicative capacity: 40 dwellings

Regeneration role: a site with potential for a more efficient and appropriate town centre use than the current builders' yard, which would continue the regeneration of Greyfriars Road achieved through the residential redevelopment adjacent at Wolsey Street and improve pedestrian routes from Cardinal Park to the Central Shopping Area by reducing 'dead' frontage.

IP045 Land bounded by Toller Road, Cliff Lane and Holywells Road

Potential mix: residential and employment, community, arts, open space or tourism

Indicative capacity: 148 dwellings

Regeneration role: a key site for the regeneration of the eastern quays, lying between IP226 to the west (see below) and residential allocation IP064 to the east. The site would provide opportunities to improve pedestrian linkage from the eastern quays to open space at Holywells Park.

IP052 Land at Star Lane/Lower Orwell Street

Potential mix: residential, employment, leisure

Indicative capacity: 29 dwellings

Regeneration role: an important site for the regeneration of Lower Orwell Street and improvements for pedestrians and cyclists on Star Lane, with residential and mixed-use allocations across Lower Orwell Street at IP011a and IP011b.

IP226 Helena Road/Patteson Road

Potential mix: residential and uses appropriate at the Waterfront (policy SP11)

Indicative capacity: 337 dwellings

Regeneration role: a key site for the regeneration of the eastern quays providing a bookend to the Waterfront development and linking through to the brewery site IP042 to the south, with residential allocation IP098 adjacent.

- 4.21 The opportunity sites are in locations suitable for development and important for regeneration, but current uses raise deliverability issues which have deterred the Council from allocating them. The sites are suitable for development and potentially achievable, but their availability is uncertain. The Council's ambition and preference is to see them redeveloped for residential-led uses.
- 4.22 Two of the sites – IP052 and IP226 -were allocated through the 2017 Ipswich Local Plan. However, through the process of updating the Strategic Housing and Employment Land Availability Assessment (SHELAA) 2018-19, the Council has not been able to ascertain with certainty that the sites will come forward over the plan period.

Policy SP5 Land Allocated for Employment Use

- 4.23 Final Draft Core Strategy policy CS13 refers to the allocation of at least 23.2ha of employment land across the Borough to deliver jobs growth. The following policy addresses this requirement.

Policy SP5 Land allocated for employment use

The following sites are allocated for employment development within Use Classes B1, B2 or B8, either in their entirety or as part of mixed use developments as specified in Table 3. Appropriate employment-generating sui generis uses, defined through policy DM33, will also be permitted where specified.

Table 3 Land allocated for employment uses

| Site | Address | Site Area ha | Notes |
|------------|-------------------------------|--------------------------|---|
| IP029 | Land at Bramford Road | 2.26ha (45%) = 1ha | Suitable for employment uses B1(c), B2 and B8 and appropriate employment-generating sui generis uses as defined through policy DM33. |
| IP067 b | Former British Energy Site | 4.18 | Suitable for B1 (excluding office use B1a) or B8 and appropriate employment-generating sui generis uses as defined through policy DM33 – residential use is proposed on the northern section and therefore uses should be compatible with residential |
| IP094 | Land to rear of Grafton House | 0.31 | Suitable for B1a office |
| IP140 | Land north of Whitton Lane | 6.93 | Suitable for B1, B2 and B8 and appropriate employment-generating sui generis uses as defined through policy DM33. Delivery expected in the medium to long term. Should be planned comprehensively as part of a larger scheme with adjacent land |

| Site | Address | Site Area ha | Notes |
|------------|--|-------------------------|--|
| | | | in Mid Suffolk but the two areas could come forward in phases. Subject to suitable access being provided. |
| IP141 a | Land at Futura Park, Nacton Road | 4.78 | Suitable for employment uses B1b, B1c, B2, B8 and appropriate sui generis uses as defined through policy DM33. |
| IP150 c | Land south of Ravenswood | 1.18 | Suitable for B1 (excluding office use B1a) and appropriate employment-generating sui generis uses as defined through policy DM33 |
| IP152 | Airport Farm Kennels | 7.37 | A site for longer term development subject to access improvements. Suitable for B1 (excluding office use B1a), B2 or B8 and appropriate employment-generating sui generis uses as defined through policy DM33. Development will be subject to the preparation of a development brief to address matters including the nationally designated Area of Outstanding Natural Beauty. The Council will explore the feasibility of park and ride being incorporated into this site. |
| IP004 | Bus depot Sir Alf Ramsey Way | 1.07 (50%) = 0.53 | 50% employment as part of mixed use scheme with housing |
| IP043 | Commercial Buildings & Jewish Burial Ground Star Lane | 0.70 (20%) = 0.14 | Suitable for B1 / leisure as within the town centre boundary, as part of a mixed use development with housing |
| IP051 | Old Cattle Market Portman Road | 2.21 (80%) = 1.77 | Prime town centre site for new B1a office development 80% B1a and 20% main town centre uses such as hotel / leisure |

| Site | Address | Site Area ha | Notes |
|-------|--|----------------------------|--|
| | | | (excluding retail). Numerically like for like replacement of existing long-stay car parking provision in this area will be required prior to the parking being lost. Offices with large floor plates in an office campus design approach are likely to be developed. |
| IP119 | Land east of West End Road | 0.61ha (15%) = 0.1ha | Suitable for employment uses B1(c), B2 and B8 and appropriate employment-generating sui generis uses as defined through policy DM33. |
| IP132 | Former St Peters Warehouse Site, 4 Bridge Street | 0.18ha / 0.05ha | B1a office, leisure, small scale retail as part of a mixed use scheme of 73 dwellings |
| | TOTAL | 28.34.ha | |

- 4.22 The availability of land and buildings is an important factor in economic development and job creation, not just in terms of quantity but also the range of sites available and choice offered in terms of quality and location.
- 4.23 Final Draft Core Strategy policy CS13 sets a target of approximately 9,500 jobs to be provided 2018-2036 through a number of measures, including the allocation of at least 23.2ha of land for employment development. In the interest of ensuring a range and choice of sites across the Borough and the plan period, this policy allocates land for employment equivalent to 28.34ha.
- 4.24 All the employment sites allocated through the adopted Local Plan 2017 have been reviewed and only those included where the Council is confident development can be delivered over the plan period and, in the vast majority of cases, where the Employment Land Supply Assessment 2018 concludes that the sites are of good quality. The Council has investigated the constraints affecting such sites and will be proactive in supporting their delivery. The constraints are identified on the site sheets included at Appendix 3A and need to be taken into account by prospective developers.
- 4.25 The Suffolk Growth Strategy and the New Anglia Local Enterprise Partnership's (NALEP) Economic Strategy for Norfolk and Suffolk 2017 identify the economic sectors which are already significant in Suffolk (and Norfolk) and those which are forecast to grow⁵: energy, advanced manufacturing and engineering, information and communication technology (ICT), tech and digital creative, financial services and insurance, advanced agriculture food

⁵ Suffolk Growth Strategy SCC March 2013 and NALEP Economic Strategy for Norfolk and Suffolk 2017

and drink, transport freight and logistics, visitor economy - tourism and culture, life sciences and biotech and construction and development. Several of these sectors have a strong base in Ipswich. As well as allocating land for employment development, the Council is encouraging jobs growth in Ipswich through its Economic Strategy, the Ipswich City Deal and work with the Suffolk Growth Group and NALEP. Through the Economic Strategy, NALEP are providing focused support for business and innovation, improved skills and targeted infrastructure investment.

- 4.26 The sites allocated are reserved for Class B uses as identified in the Use Classes Order 1987 (as amended and updated), other than where a mix of uses or appropriate employment-generating sui generis uses are specified in Table 3 above. On those sites allocated for a mix of uses including employment, the proportions indicated in the policy represent the Council's preferred outcome and form the basis of supply calculations. However, the figures are indicative other than in the case of open space requirements and community facilities, to help ensure that developments can be viably delivered. Appropriate employment-generating sui generis uses are defined through policy DM33 and will need to comply with other plan policies including DM18. Appendix 3 provides additional information about the sites allocated through this policy.

Policy SP6 Land Allocated and Protected as Open Space

- 4.27 Open space is essential to the quality of life in Ipswich. Core Strategy Review policies CS16 and DM5 protect open spaces and the policy below adds site specific detail to this by identifying sites and allocating new open spaces.

Policy SP6 Land allocated and protected as open space

Existing open spaces are defined on the policies map. Within the defined open spaces, Core Strategy Review policy DM5 shall apply.

Site IP083 Banks of the river upriver from Princes Street is allocated for public open space. Any development shall retain the river path and its setting.

Development of the following sites shall include more than the minimum amount of on-site public open space provision required through policy DM6, as specified in Table 4 below:

Table 4: Sites proposed for a mix of uses including open space

| Site | Address | Preferred Options Ref | %Open Space | %Other Uses |
|-------|---|-----------------------|--|--|
| IP029 | Land opposite 674-734 Bramford Rd | UC030 | 67% Open Space | 33% Employment; also potential link road |
| IP032 | King George V Field, Old Norwich Rd | UC033 | 20% Open space/playing pitches | 80% Housing |
| IP033 | Land at Bramford Rd (Stocks Site) | UC034 | 50% Open Space | 50% Housing |
| IP037 | Island Site | UC038 | 15% Open Space | 70% Housing, 5% existing boat-related uses, small scale retail/café/restaurant |
| IP047 | Land at Commercial Road | UC048 | 15% Public Open Space and enhanced river path | 80% housing, 5% retail. |
| IP048 | Mint Quarter / Cox lane Regeneration Area | UC051 | 20% open space – could span the parts of the site east | 80% Housing, primary school on east side, retail on west side, car |

| | | | | |
|-------|----------------------------------|-------|---------------------------------------|---|
| | | | & west of Cox Lane (and car parking). | parking. |
| IP061 | Former school site Lavenham Road | UC064 | 40% Open Space | 60% Housing – development to deliver improvement to remainder of open space |
| IP142 | Land at Duke Street | UC259 | 25% open space | 75% Housing |

- 4.28 On some sites there is a requirement to provide more than the standard amount of open space. This may be because the land is currently used as open space and therefore the loss needs to be compensated through improvements to what remains, or because the land is in effect used as open space even though it has previously been earmarked for development, and is in an area of open space deficit. Improvements to the remainder of the open space could include making it more usable, accessible, biodiverse or multi-functional.
- 4.29 Paragraph 8.184 of the Final Draft Core Strategy indicates broadly which areas of the Borough are short of open space against the current standards. There are no parts of the Borough which currently have a surplus of all types of open space. Site IP083 is located at the boundary of the Central and South West areas. The Central area currently has a significant deficit of natural and semi-natural green space.
- 4.30 The open spaces protected by this policy include all the different types shown on the policies map including playing fields, allotments and country parks.
- 4.31 The Waterfront does not currently have public open green space provision. This is addressed through the policy by allocating land for this use at site IP047 and within the Island Site redevelopment. Appendix 3 provides additional information about the sites allocated through this policy.

Policy SP7 Land Allocated for Leisure Uses or Community Facilities

Policy SP7 Land allocated for leisure uses or community facilities

Land is allocated for leisure uses or community facilities as specified in Table 5 below, on stand alone sites or as part of larger development sites:

Table 5: List of sites proposed for leisure uses or community facilities

| Site | Address | % Community facilities / leisure | Community or leisure use | Other Uses |
|-------------|--|---|---------------------------------|---|
| IP005 | Former Tooks Bakery, Old Norwich Rd As part of a residential-led mixed use redevelopment | 20% | Health centre | Housing |
| IP010a | Co-op Depot, Felixstowe Rd As part of a residential development | 25% | Primary school extension | Housing |
| IP037 | Island Site | To be determined through Masterplanning | Early Years and leisure | Housing, Employment, Restaurant/ small retail and amenity green space |
| IP048a | Mint Quarter / Cox Lane East regeneration area | 60% | Primary School | Housing and Car Parking |
| IP129 | BT Depot, Woodbridge Road | 100% | SEND Free School | |
| IP150b | Land at Ravenswood (7.8ha) | 100% | Sports park | Housing adjacent |

- 4.32 It is essential that community facilities are provided to keep pace with the growth in Ipswich's population. Ipswich Borough Council liaises closely with infrastructure and services providers, including Suffolk County Council and the Health Trusts, in order to identify the need for facilities for which land may need to be identified and safeguarded.
- 4.33 The Borough is seeing a rapid rise in the number of primary school aged children and therefore it is important to make adequate provision for new and expanded education facilities over the plan period. Sites have not been allocated for all the early years provision, but Final Draft Core Strategy policy CS15 promotes co-location with primary schools or location within district and local centres to promote linked trips. The need for early years provision in association with new development should be discussed with the County Council at an early stage in the planning process.
- 4.34 The full infrastructure needs of the Borough are being identified and costed in an Infrastructure Delivery Plan as a stepping stone towards putting in place a Community Infrastructure Levy for Ipswich. Key infrastructure is identified through Chapter 10 of the Final Draft Core Strategy, which will be supported by a separate Infrastructure Delivery Plan.
- 4.35 It is also important to plan for leisure uses that will be delivered over the plan period, whether they are on Council-owned or private land.
- 4.36 The need for community infrastructure associated with the Ipswich Garden Suburb development is dealt with through the Core Strategy Review and supplementary planning document. Appendix 3 provides additional information about the sites allocated through this policy.

Policy SP8 Orwell Country Park Extension

Policy SP8 Orwell Country Park Extension

Site IP149 Land at Pond Hall Carr and Farm is allocated as an extension to Orwell Country Park, to provide better management of visitors to this part of the Orwell Estuary Special Protection Area.

- 4.37 The Orwell Estuary is designated as a Special Protection Area for birds. It is an important over-wintering site for wading birds, however, the birds can be easily disturbed by recreational activity including dogs running loose off leads.
- 4.38 The Appropriate Assessment to the adopted Core Strategy advised that a range of measures were needed including visitor management measures at Bridge Wood, to protect the Orwell Estuary Special Protection Area. A management plan for the whole of the Orwell Country Park is being prepared, but the adjacent Pond Hall Farm provides an opportunity to extend the Country Park and, in doing so, to provide a better network of footpaths and viewpoints over the estuary for visitors. Information could also be provided about the Special Protection Area, in order to raise awareness and discourage access to the foreshore. A visitor survey of Orwell Country Park undertaken during March 2015 concluded that the extension of the park into land at Pond Hall Carr and Farm is not likely to have harmful impacts on the Special Protection Area. The Council is currently in discussion with Natural England about the England Coast Path route in this area. The England Coast Path will be subject to its own Habitats Regulations Assessment.

Policy SP9 Safeguarding Land for Transport Infrastructure

Policy SP9 Safeguarding land for transport infrastructure

Development of the following sites for the uses allocated through other policies in this plan shall incorporate provision for transport infrastructure, as specified in Table 6 below.

The park and ride site at Anglia Parkway is safeguarded for future re- use for park and ride. The Council will investigate the feasibility of park and ride on part of IP152 Airport Farm Kennels

Table 6: List of development sites which will include provision for transport infrastructure

| Site | Address | Site Area (ha) | Allocation |
|-------------|-------------------------------------|----------------|---|
| IP010a or b | Felixstowe Road | 5.01 | Land reserved for a pedestrian and cycle bridge over the railway to link the District Centre with housing areas to the north. |
| IP059a | Land at Elton Park Works | 2.63 | Land reserved for a pedestrian and cycle bridge over the river to link to the river path on the northern bank. |
| IP037 | Island Site | 6.02 | Additional vehicular access needed to enable the site's development. Additional cycle and pedestrian connections also required in accordance with policy SP15. Development layout should not prejudice future provision of a Wet Dock Crossing. |
| IP029 | Land opposite 674-734 Bramford Road | 2.26 | Link road through the site joining Europa Way and Bramford Road (subject to impact testing). |

4.40 It is important that provision is made for needed transport infrastructure within certain development sites. The aspiration to provide a pedestrian and cycle bridge over the railway at site IP010 (a or b) to link residential areas to the north with the District Centre is not a firm proposal at present in the Local Transport Plan, but the Highway Authority has identified the potential benefits of a link and the opportunity presented by the two IP010 sites.

Therefore, pending further work on its deliverability, development at the IPO10 sites should not preclude its possible future provision.

- 4.41 The River Strategy identifies the aim of providing a river crossing between Hadleigh Road and Boss Hall for pedestrians and cyclists. The Elton Park site is considered the most suitable location to link up with future development at the Former Sugar Beet Factory site in Babergh.
- 4.42 The Island site is an outstanding development opportunity at the Waterfront. Access improvements will be needed to enable its redevelopment.
- 4.43 The provision of a road link through site IP029, land opposite 674-734 Bramford Road, to join Bramford Road and Europa Way could help to ease pressure on the Bramford Road/Sproughton Road junction. It was included as a proposal in the 1997 Ipswich Local Plan, but has not to date been implemented. The potential impacts of the link on the traffic network are subject to testing and need to be clearly understood. However, in the interim, development of the site should not preclude its future provision.
- 4.44 The park and ride site at Anglia Parkway, Ipswich is currently disused. However, it remains laid out as a park and ride facility and is safeguarded for that use in future. Once lost, sites are difficult to replace. The need for and deliverability of additional park and ride at IP152 Airport Farm Kennels will be explored as a potential measure to address congestion, accessibility and air quality.
- 4.55 The Council is working with the Highway Authority and neighbouring authorities to agree a Transport Mitigation Strategy and detailed action plan to support modal switch and sustainable travel choices in the Ipswich area, which is needed to support growth. Core Strategy policy CS20 outlines the potential measures the action plan will draw from.

CHAPTER 5: IP-One Area

- 5.1 IP-One is a large area in the centre of Ipswich, which contains a rich mix of uses ranging from shopping, business, public administration and leisure to education and living. It incorporates several smaller areas, each of which has its own identity, character and issues: the medieval town centre, Waterfront, Education Quarter and Portman Quarter. An Action Plan is needed to help to deliver regeneration where needed and ensure the areas link together and complement one another to provide a strong, attractive and vibrant centre to Ipswich.
- 5.2 The area of central Ipswich that falls within IP-One contains the greatest concentration of the town's designated heritage assets, including a number of important historic and archaeological sites. Much of IP-One is also designated as an Area of Archaeological Importance as it covers the Anglo-Saxon and Medieval town, aspects of which are internationally recognised.
- 5.3 The Final Draft Core Strategy provides the strategic level of policy for centres in Ipswich, particularly through policy CS2 the Location and Nature of Development and CS14 Retail Development. It defines a network of town, district and local centres; recognises the importance of Ipswich town centre as an economic driver and a focus for shopping, cultural and leisure activities, civic functions and community life; and it sets a target for retail floorspace growth. The development management policies of the Core Strategy Review protect the vitality and viability of centres through managing development in defined centres and outside them.
- 5.4 The Council's vision for Ipswich town centre combines elements of the Core Strategy Review vision and the Ipswich Central vision for a 'Waterfront Town Centre' and includes:
- More people living and working in the town centre
 - Focusing new development on the town centre
 - Improving the shopping offer
 - Improving pedestrian links between the central shopping area and the Waterfront, Village, Education Quarter, railway station and northern gateway
 - Putting pedestrians, cyclists and public transport users first
 - Enhancing existing parks and open water and greening the streets
 - Developing the Education Quarter
 - Enhancing and increasing culture and leisure facilities
 - Creating a place that is outgoing and welcoming, vibrant and dynamic and which embraces new ideas; and
 - Reconnecting the town centre with the Waterfront.
- 5.5 The Council's focus is to strengthen the north-south axis, creating better linkages between the town centre and the Waterfront. The Town Centre Opportunity Areas report recommended a complementary role for the town

centre and the Waterfront and opportunities for improving and strengthening linkages between the two. This will be supported through the delivery of site allocations along Turret Lane, Lower Brook Street, Foundation Street, Lower Orwell Street and the Waterfront. Retail uses on these sites should be limited to a small scale as defined through the Core Strategy Review so they do not compete directly with the existing town centre offer.

- 5.6 Thus this section of the plan sets out policies which:
- Define the Education Quarter, Waterfront and Portman Quarter and guide development within the areas;
 - Allocate sites for town centre and district centre retail development;
 - Define and safeguard routes for transport proposals including cycling and walking; and
 - Manage car parking provision in the town centre.
- 5.7 The historic environment within IP-One is addressed in a variety of ways. Within the plan, the site sheets in Appendix 3 identify where there are historic environment constraints which will need to be taken into consideration in the redevelopment of the sites. Core Strategy Review policies CS4 and DM13/DM14 set out the framework for considering the impacts of development on the historic environment through the development management process. The Council will also consider the heritage impacts of allocating the most sensitive sites within IP-One. The Opportunity Area development guidelines in Chapter 6, which focus on areas which are likely to undergo the greatest change, also highlight heritage issues.
- 5.8 IP-One contains parts of several conservation areas: Central, Wet Dock, Stoke and St Helen's. The Council has produced Conservation Area Character Appraisals for all the conservation areas and these are reviewed every five years. The Council has also adopted an Urban Character supplementary planning document to cover parts of the town outside the conservation areas. Buildings at risk within the Borough are concentrated within IP-One. They are reviewed annually and action is underway to address all the buildings currently at risk, through negotiation with the owners, supporting the preparation of funding bids, compulsory purchase of sites or repairs being undertaken by owners. Grade I and II* buildings in Ipswich are dealt with through the national Heritage at Risk register.

Policy SP10 Retail Site Allocations

Policy SP10 Retail Site Allocations

Sites are allocated in the Central Shopping Area for retail development to meet the forecast need for comparison shopping floorspace to 2031 at:

- a. New site – IP347 Mecca Bingo, Lloyds Avenue (650 sq m net);
- b. IP040 The former Civic Centre, Civic Drive ('Westgate') as part of a residential-led development (2,050 sq m net);
- c. IP048b west part of Mint Quarter as part of a mixed use residential development (4,800 sq m net); and
- d. Units in upper Princes Street (675 sq m net).

The former British Homes Stores, Butter Market, is safeguarded to include some future A1 retail provision.

The Central Shopping Area is amended to extend to the south-west part way down Princes Street and contract at its western extend to exclude the former police station (site IP041) and adjacent housing.

Land is also allocated at the former Co-Op Depot, Boss Hall Road (315 sq m net), to meet the need for comparison shopping floorspace as part of the new Sroughton Road District Centre. Development will be at an appropriate scale for a district centre in accordance with CS14.

The allocations and the extent of the Central Shopping Area are illustrated on the policies map and the IP-One Area Inset policies map.

- 5.9 The National Planning Policy Framework (NPPF) advises that local planning authorities should set out policies for the management and growth of town centres over the plan period. The Council's Retail and Commercial Leisure Study (2017) identified a need for additional retail floorspace in the town centre. It is needed to meet the needs of a growing population and secure the town's position as a regional shopping centre, which can compete effectively with other centres.
- 5.10 However, more recent evidence in the study undertaken by DTZ (2013) to advise the Council on the delivery of additional retail floorspace in the town centre indicates that, in the current economic climate and given the pressures facing town centre retailing, the quantum of deliverable new floorspace is likely to be less than previously thought. The subsequent 2017 Retail Study identifies that within the first five years of the Local Plan period, there is no need for any additional retail floorspace. The 2019 retail position update states that over the 10-year period required by the NPPF (2019), there is a need for 9,900sq m (10,500 sq m including Copdock) net comparison floorspace to 2029.

- 5.11 The focus should be on strengthening the existing centre, particularly the prime pitch, prioritising sites and supporting the delivery of one scheme in a development cycle (10 years) for new retail floorspace. This evidence has informed Core Strategy Review policy CS14 and policy SP10 above.
- 5.12 Delivering new retail investment in town centres can be challenging in this economic climate. The Mecca Bingo site, Westgate site and west side of the Mint Quarter are identified as a key opportunities to achieve new development, as they are located in close proximity to the existing retail core and would build upon the existing well functioning retail centre.
- 5.13 New retail floorspace here helps to address the qualitative deficiencies in the town centre, such as the lack of choice of large floor plate shop units.
- 5.14 A development brief will be prepared to guide the redevelopment of the Mint Quarter.
- 5.15 In addition to the new allocations, existing units which are vacant or in A2 use are allocated and safeguarded, consisting of the upper part of Princes Street currently primarily in A2 use but with two vacant units, and the former BHS store on the Butter Market which has potential for over 3,000 sq m of floor space in a large floorplate building in the primary shopping area.
- 5.16 The allocations above, and small scale retail floorspace likely to be delivered within mixed use developments in IP-One (840 sq m net), and the new District Centre allocated at Ipswich Garden Suburb through policy CS10 provide for at least 10,000 sq m (net) of new comparison retail floorspace. The need for retail floorspace will be kept under review.

Policy SP11 The Waterfront

Policy SP11 The Waterfront

The Waterfront is defined on the IP-One Area inset policies map. The Waterfront remains the focus for regeneration within central Ipswich to create high quality, mixed use neighbourhoods in accordance with Core Strategy Review policies CS2 and CS3.

Within the Waterfront, new development should contain a mix of uses. Residential, community, office, arts, culture, open space, boat-related and tourism uses will be permitted. Final Draft Core Strategy policy DM23 a. shall apply in relation to residential density.

Where the Waterfront overlaps with the town centre at the northern quays, all the main town centre uses will be permitted with the exception of retail uses, applications for which will be considered against Final Draft Core Strategy policy DM31.

The Education Quarter is addressed through policy SP12 and arts, culture and tourism through policy DM28 (formerly SP14).

- 5.17 The Waterfront consists of over 80ha of land and buildings around the Wet Dock, which was completed in 1842. It includes the historic port area located to the north of the modern commercial port. It is characterised by a mix of buildings of varying scales. The Wet Dock was designated a conservation area in 1991. The area contains a number of important heritage assets, including listed buildings, which new development will need to take into account. Final Draft Core Strategy policy DM13 addresses heritage assets and conservation.
- 5.18 The regeneration of the Waterfront as a mixed use area started in 1999 with the completion of apartments at Neptune Quay. To date, much development has been delivered, including residential and commercial uses and academic and student accommodation buildings for the University of Suffolk.
- 5.19 However, a few key sites remain to be redeveloped and the recession has seen some large developments stall. Therefore the regeneration focus needs to remain on this area. The flood defence barrier is important for the release of development sites at the Waterfront (and the wider area in the flood zone). It was completed in 2019.
- 5.20 The 'main town centre uses' are defined through the National Planning Policy Framework as retail, leisure, entertainment facilities, more intensive sport and recreation uses, offices, arts culture and tourism. The Waterfront extends further south than the town centre but the two areas overlap at the

northern quays. Within the Waterfront, office, arts, culture open space, boat-related and tourism uses are considered appropriate in order to support the delivery of a mixed use area, to provide some flexibility to support its continued regeneration and because the accessibility of the Waterfront is good. At the northern quays additional main town centre uses are permissible with the exception of retail uses. A key element of the Council's strategy for the Waterfront is to ensure that it complements, but does not compete with, the shopping focus in the Central Shopping Area.

- 5.21 Ipswich Port is situated within and adjacent to the Waterfront and therefore new development should take account of its operational needs.
- 5.22 This policy helps to implement Policies CS2 and CS3 of the Core Strategy Review. CS2 'The Location and Nature of Development' sees a focus on residential and community facility development within the Waterfront in order to support the regeneration and sustainable growth of Ipswich, and mixed use development within the town centre. CS3 identifies the importance of the Waterfront and town centre as areas within IP-One.
- 5.23 Parts of the Waterfront also fall within the town centre and the Education Quarter. The town centre is addressed by policy DM31 in the Final Draft Core Strategy. The Education Quarter is addressed by policy SP12 in this plan.

Policy SP12 Education Quarter

Policy SP12 Education Quarter

The Education Quarter is defined on the IP-One Area inset policies map, comprising the Suffolk New College campus and the University of Suffolk campus. Within the defined Education Quarter, development for education and ancillary uses such as student accommodation or offices will be permitted.

On sites which fall within the Education Quarter and the Waterfront, the Council would consider Waterfront uses on their merits, provided they would not compromise the ability of the University to function or expand and to meet future education needs.

Development of site reference IP049 No 8 Shed Orwell Quay will be required to include an element of public car parking in accordance with policy SP17.

- 5.24 University Campus Suffolk (UCS) grew from around 3,000 students in 2008/09 to over 3,900 in 2011/12. In August 2016 it became the University of Suffolk. It makes many important contributions to the town e.g. through raising levels of educational attainment, its links with the business sector, attracting young people to locate or stay in Ipswich, adding vibrancy to the Waterfront and town centre and indirectly helping to support shops and businesses providing goods and services to students.
- 5.25 Suffolk New College is a tertiary college established in 2007 in new premises on Rope Walk, providing for students studying for a range of qualifications. Like the University of Suffolk, it makes an important contribution to Ipswich life and to raising the levels of educational attainment amongst Ipswich residents and beyond.
- 5.26 The Council wishes to safeguard the Education Quarter for predominantly education uses, because of its importance to the town and the benefits that can flow from locating educational uses in close proximity. The institutions need to be able to grow and adapt over coming years. The policy allows for education uses, but also offers some flexibility for appropriate uses provided this would not compromise future use or expansion for education purposes. Proposals for development within the Education Quarter should demonstrate how sustainable modes of transport will be achieved.
- 5.27 The co-location of knowledge-based businesses in close proximity to the University campus is recognised by the Council as an important

consideration, therefore the policy would allow for spin-off businesses to locate within the Education Quarter. This would also help to support the institution's success in placing its graduates into employment.

- 5.28 Ancillary uses are those uses which have a functional relationship with the main education use. Proposals for retail development within the Education Quarter will be considered in relation to retail policies of the Core Strategy Review. Policy DM28 (formerly SP14) states the Council's 'in principle' support for the development of flexible conference and exhibition space at the Waterfront. This could occur within the Waterfront section of the Education Quarter in accordance with this policy.
- 5.29 The need for an element of public car parking at site reference IP049 which falls within the Education Quarter and the Waterfront was identified through the emerging Parking Study. Development principles for the Education Quarter are set out in Chapter 6 (see 'Opportunity Area D').

Policy SP13 Portman Quarter (formerly Ipswich Village)

SP13 Portman Quarter

The Portman Quarter is defined on the IP-One Area inset policies map as a focus for regeneration in the west of IP-One. The Council's vision for the Portman Quarter is a mixed-use neighbourhood of residential use, open spaces and main town centre uses, excluding retail, where they accord with Final Draft Core Strategy policy DM31.

- 5.30 Ipswich Village is an area focused on the Portman Road football ground and has been re-named 'the Portman Quarter' to reflect this key leisure facility and align with the Ipswich Vision document. The area has undergone a significant amount of change already with new office, court and residential uses replacing older industries through developments such as the County Court, Voyage, Endeavour House and Grafton House. The Council wishes this regeneration to continue and, to this effect, allocations for development within the area are made elsewhere in this plan.
- 5.31 The east part of the Portman Quarter lies within the town centre boundary where main town centre uses such as offices and leisure are permitted. The Princes Street corridor, part of which lies within Ipswich Village, gained Enterprise Zone status in 2015. The western part of the area is identified as an Opportunity Area through Chapter 6 of this plan.

**Policy SP14 Arts, Culture and Tourism Policy moved to sit with
the Development Management policies.**

Policy SP15 Improving Pedestrian and Cycle Routes

5.32 The Local Transport Plan for Suffolk 2011-2031 identifies the following key issues for Ipswich:

- Road condition
- Urban realm improvements
- Tackling congestion
- Modernisation of bus stations
- Reducing separation between town centre and waterfront
- Better facilities for walking and cycling
- Stronger neighbourhoods
- Longer term – crossing for improved access to wet dock island site
- Town centre masterplan
- A14 improvements
- Ipswich – Transport fit for the 21st Century (now known as Travel Ipswich)
- Extensive Air Quality Management Areas
- A14 Orwell Bridge and Seven Hills Interchange Congestion

5.33 The Local Transport Plan is under review. A draft Transport Mitigation Strategy has been published by Suffolk County Council, with a detailed action plan to mitigate the impacts of planned growth under discussion by the Ipswich Strategic Planning Area Board (see policy CS20 of the Final Draft Core Strategy). Walking and cycling route and infrastructure improvements to support and encourage healthy and sustainable travel will form a key component of the strategy.

Policy SP15 Improving Pedestrian and Cycle Routes

The Council will support improvements to pedestrian and cycle routes within the IP-One area and linking the town centre to residential areas and beyond. It will seek opportunities to deliver the following specific improvements through safeguarding routes where necessary, new developments and/or seeking funding opportunities:

- **The provision of safe cycle and pedestrian access across the lock gates at the entrance to the Wet Dock to create a circular route;**
- **The provision of new foot and cycle bridges across the new Cut linking Stoke Quay to St Peter’s Wharf and the Island site to Felaw Street;**
- **An improved pedestrian environment on key walking routes from the Waterfront to the Central Shopping Area - Turret Lane, Lower Brook Street, Foundation Street and Lower Orwell Street;**
- **Improved pedestrian links through Cardinal Park linking the station and Central Shopping Area;**

- **Enhanced walking and cycling links between the railway station and the Waterfront via the river path;**
- **Improved pedestrian and cycle links from Handford Road to Sir Alf Ramsey Way;**
- **Improved pedestrian and cycle routes linking St Matthew’s Church, the New Wolsey Theatre, Westgate Street and the proposed cultural hub at High Street; and**
- **The pedestrianisation of Princes Street North and Upper Brook Street.**

Throughout the Borough, development should improve linkages to the rights of way network, including cross boundary links, where opportunities exist to do so.

Pedestrian and cycle measures are also supported outside IP-One, specifically:

- a. **A pedestrian and cycle bridge across the River Gipping in west Ipswich; and**
- b. **A pedestrian and cycle bridge across the railway line at Felixstowe Road District Centre.**

5.34 Some improvements have been made under the Travel Ipswich scheme to walking routes from the railway station via Princes Street to the Central Shopping Area.

5.35 However, other improvements are also needed as listed in the policy, providing links across water or enhancing routes between key nodes or improving the pedestrian and cycle environment. The Council will work with the Highway Authority, developers and landowners to deliver new routes and improvements to existing routes. The Council has also adopted a Public Realm Strategy Supplementary Planning Document for the town centre which identifies the priorities in the town centre and Waterfront for environmental enhancements.

5.36 The Town Centre and Waterfront Public Realm Strategy SPD (2019) states that accessibility to and around the town centre for walking and cycling is fair but there is a need to increase the quality and safety of routes, to improve information and to provide more facilities for crossing the busy roads at the edge of the central area. It recommends making Star Lane more pedestrian friendly and easier to cross and improving links for pedestrians between the Waterfront and Town Centre.

5.37 The Council adopted a Cycling Strategy supplementary planning document in July 2016. Opportunities to implement the strategy will be sought through development proposals and any funding opportunities that arise as a result of the preparation by the Highway Authority of a Walking and Cycling Infrastructure Plan. The river path is a key route for active travel which links into adjacent districts. Currently the route is obstructed in the vicinity of Boss Hall to cyclists and people with reduced mobility by a structure formerly

needed for the operation of a sluice gate. Opportunities will be sought to secure its removal.

- 5.38 Where land within development sites is needed for walking and cycling infrastructure, it is safeguarded through policy SP9.

Policy SP16 Transport Proposals in IP-One

Policy SP16 Transport Proposals in IP-One

The Council supports the aspiration identified in the Local Transport Plan for the provision of a new Wet Dock Crossing, linking the east bank in the vicinity of Toller Road with the west bank in the vicinity of Felaw Street. The crossing would facilitate access to the Island Site and may provide for through traffic. Its design would maintain boat access through the lock and navigation along the New Cut. The design and layout of development on the Island Site IP037 should not prejudice the future delivery of a Wet Dock Crossing should a firm proposal be included in future updates of the Local Transport Plan.

The Council also supports measures to improve pedestrian and cycle access between the Waterfront and Central Shopping Area.

- 5.39 The geography of central Ipswich is such that vehicular movement between its eastern and western sides is constrained by the mediaeval core and the existence of the Wet Dock. This limits options for such movements and means that the Star Lane Gyratory is a key east-west corridor. However, it causes several problems including congestion, poor air quality⁶ and a physical barrier to pedestrian movement between the Waterfront and the Central Shopping Area.
- 5.40 The Ipswich Waterfront Study 2007 suggested reducing the Gyratory to one lane in each direction⁷. However, the Council concluded that it could be supported only if a compensatory alternative east west route could be found. Through the Final Draft Core Strategy policy CS20, the Council has identified that this could be achieved through a Wet Dock Crossing, or an Ipswich northern by-pass. The Wet Dock Crossing would provide access to the Island Site. It could also provide a route for through traffic, but this is yet to be determined.
- 5.41 In March 2015, the New Anglia Local Enterprise Partnership obtained funding in order to carry out a feasibility study for the Wet Dock Crossing. It was re-named the Upper Orwell Crossings Project and involved three bridges: bridge A vehicular link between the east and west banks; bridge B a pedestrian and cycle bridge across the lock gates; and bridge C a vehicular link over the New

⁶ See IBC website <https://www.ipswich.gov.uk/airqualitymanagement>

⁷ Ipswich Waterfront Transport Study, 2007, Suffolk County Council. Core Document Library reference ICD45

Cut West. In January 2019, Suffolk County Council formally cancelled the project but feasibility work on bridges B and C continues.

- 5.42 Final Draft Core Strategy paragraphs 8.246 and 8.247 address access to the Island Site, which as a minimum will require a road bridge from the west bank to the Island Site and a pedestrian and cycle bridge across the Wet Dock lock gates to enable any significant development (equivalent to Upper Orwell Crossings Project bridges 2 and 3). The Island Site IP037 does not require a full Wet Dock Crossing to enable its delivery. The proposal is included as an aspiration in the Local Transport Plan, which is currently under review
- 5.43 Any crossing here would pass through the Island Site and the Wet Dock Conservation Area and therefore its design would need to take into account heritage issues. The Wet Dock Crossing must avoid unacceptable impact on vessel access to the Wet Dock, on Cliff Road, on the West Bank Terminal railhead, on port security and on New Cut navigation.

Policy SP17 Town Centre Car Parking

Policy SP17 Town Centre Car Parking

The Council will pursue a town centre car parking policy with the twin aims of supporting the economy of the town centre and limiting congestion, through encouraging the use of sustainable modes of transport.

To this end, a Central Car Parking Core is identified on the IP-One inset policies map. Within this area, Core Strategy Review policy DM22 shall apply.

Sites are allocated for multi storey car parks providing additional short stay shopper and visitor parking or long stay commuter parking as specified below:

- a. IP015 West End Road – long stay parking;**
- b. IP048 Mint Quarter – short stay parking;**
- c. IP049 No 8 Shed Orwell Quay – long stay parking.**

The provision of a multi-storey car park at site IP015 West End Road will replace the existing on-site surface parking. It will also replace existing long stay parking at IP051 Old Cattle Market, Portman Road, if this is not replaced on site through redevelopment.

All new permanent car parks will be required to achieve good design and quality, and include electric vehicle charging points and variable messaging technology.

Proposals for additional temporary car parks within the town centre will not be permitted. Proposals to renew existing planning consents for temporary short stay public parking within the town centre will not be permitted when the permanent provision allocated above has been delivered. Until then, temporary car parks will be expected to achieve the same level of quality as permanent ones.

- 5.44 Promoting sustainable transport choices is important to tackle congestion in Ipswich and its associated disadvantages for businesses, the environment and human health. It is also important for equality and inclusion, as 27.8% of Ipswich households do not have access to a car or van (2011 Census ONS Table KS404EW) and therefore it is important that public transport services can be sustained through high levels of use. Car parking policies are an important tool, alongside other planning and transport measures, to promote sustainable transport choices.

- 5.45 At the same time, providing sufficient car parking of good quality in the right places is essential to support the vitality and viability of the Central Shopping Area and enable it to compete with other centres, out of centre shops and internet shopping.
- 5.46 It is important to weigh the need to control car parking in Ipswich town centre with the need to support the town centre economy. Ipswich town centre also serves a rural hinterland where car ownership is higher and public transport services are less available. In the three adjacent districts to Ipswich, the average percentage of households with no car or van available is only 13.1%.
- 5.47 The provision of public car parking provision within central Ipswich has been reviewed through a parking study, which will inform the preparation of an Ipswich Parking Strategy. The study identified that central Ipswich contains approximately 6,817 public, off-street car parking spaces in and around the town centre (including 147 disabled spaces). Some 4,885 of the spaces within the study area provide the option for long-stay parking with the remaining 1,932 providing for short-stay parking only. Pricing mechanisms are used as a disincentive to using short stay car parks for long stay visits.
- 5.48 Short stay car parks support the economy of town centre and culture and leisure based activities. Through the Local Plan, the aim is to ensure a reasonable supply of conveniently located and reasonably priced spaces. Short stay is defined as up to 5 hours. Short stay car parks may be located within or outside the central car parking core.
- 5.49 Long stay parking is mainly for commuters. The aim is to discourage commuting trips from entering the town centre, in order to manage the morning and evening peaks, whilst recognising that there needs to be some supply for those commuters who will only or can only drive (some or all of the time).
- 5.50 The emerging findings from the parking strategy (which does not differentiate future demand in terms of long or short stay) suggest a net deficit of spaces by 2036 in central Ipswich of fewer than ten spaces. Geographically, the areas of deficit are in the vicinity of the railway station and office quarter around Russell Road, the Waterfront and in the north of the town centre. The Council is preparing a Ipswich Area Parking Plan to indicate how new, permanent parking provision will be made to replace the existing temporary spaces, in accordance with the evidence. In doing so, the Council will have regard to the County Council's draft Transport Mitigation Study and emerging action plan to ensure that parking is considered as part of a comprehensive approach to travel into central Ipswich.
- 5.51 The principle reflected through this policy is to allocate sites to meet the need for additional capacity in locations at the key entry points of the town centre. This approach seeks to limit driver 'churn' looking for appropriate

spaces and needing to drive across the town centre. These could be single sites offering both long and short stay or they could be separate sites, outside the parking core if they are long stay car parks, and inside the core if they are short stay car parks.

- 5.52 The policy aims to strike an appropriate balance between providing sufficient, correctly priced car parking to encourage shoppers and visitors into Ipswich town centre, without adding to the burden of congestion or undermining sustainable travel options. Whilst short stay temporary car parking has been allowed on a number of sites awaiting redevelopment within the town centre, it is considered that any more would undermine work to encourage mode switching. Therefore the policy does not permit additional provision of such car parking.
- 5.53 The National Planning Policy Framework states that local authorities should seek to improve the quality of parking in town centres so that it is convenient, safe and secure, alongside measures to promote accessibility for pedestrians and cyclists (see policy SP15).
- 5.54 The number of spaces to be provided at the sites allocated will be determined in relation to the delivery of additional floorspace in the town centre for the main town centre uses and spaces being lost to redevelopment. Short stay parking is that which provides for shoppers or leisure visitors visiting the town centre for part of a day or evening, whilst long stay parking is whole-day parking for workers. The difference is usually established by the location and pricing structure of the car park. When designing proposals, consideration should be given to Secured by Design guidance relating to car parks.

Part C

IP-One Opportunity
Areas

CHAPTER 6: IP-One Opportunity Areas

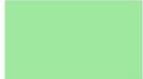
- 6.1 Eight Opportunity Areas have been identified within the IP-One area defined on the map in Figure 1 in Chapter 2 (see list below). They are areas where there are clusters of development opportunities, which together present an important opportunity to enhance the townscape and public realm. The Opportunity Area descriptions, development principles and plans which follow will act as concept plans to guide the development strategically that is expected to take place. They should be adhered to unless evidence submitted with applications indicates that a different approach better delivers the plan objectives. The allocation policies of the Plan take precedence over the Opportunity Area guidance and site sheets in the event of any discrepancy.
- A Island Site (this area includes part of the former Opportunity Area E Over Stoke Waterside)
 - B Merchant Quarter
 - C Mint Quarter and surrounding area
 - D Education Quarter and surrounding area
 - E Westgate
 - F River and Princes Street Corridor (this area reflects a re-focus of the former Opportunity Area G River Corridor)
 - G Upper Orwell River and Canalside – this is a new area added to reflect potential development sites in this area of IP-One, allocated through policy SP2
 - H Holywells Area
- 6.2 For each Opportunity Area two plans are included: the first is a site analysis plan and the second a plan to illustrate the development options and design guidelines. The development options shown illustrate how development could be laid out on allocated sites.

Key to Diagrams - Site Analysis

-  Opportunity area boundary
-  Major routes
-  Minor routes
-  Pedestrian and cycle routes
-  Listed buildings
-  Landmark buildings
-  Positive frontages
-  Frontages having a neutral impact on urban quality
-  Frontages detracting from the urban structure
-  Green space
-  Potential bridging points or existing bridge improvements
-  Urban connections to be created or improved
-  Scheduled Ancient Monument

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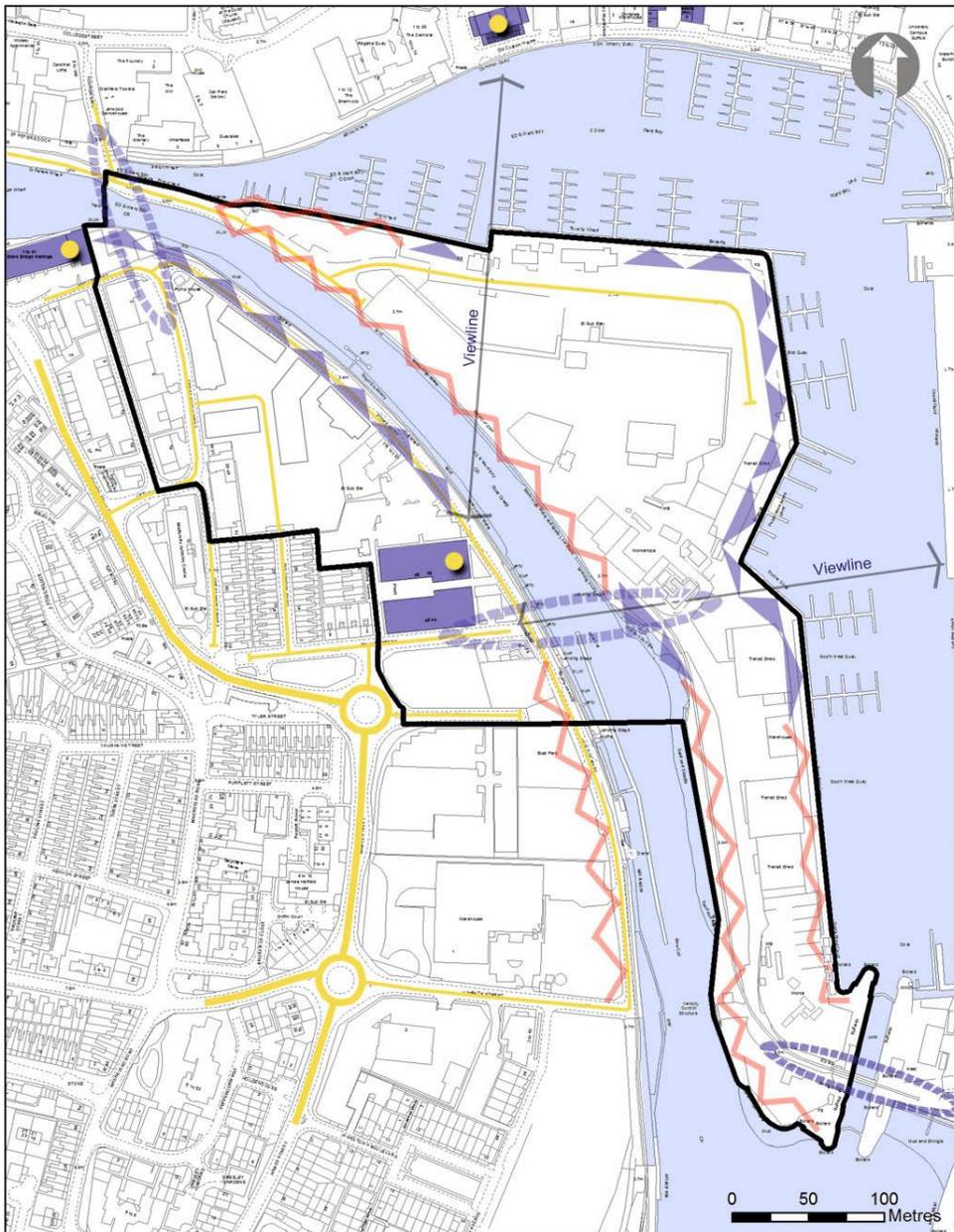
Key to Diagrams - Development Options

-  Opportunity area boundary
-  Major routes
-  Minor routes
-  Pedestrian and cycle routes
-  Listed buildings
-  Development Option
-  Landmark buildings
-  Green space
-  New or improved urban focal space

A – Island Site. Site Analysis.

A - Island Site

Analysis

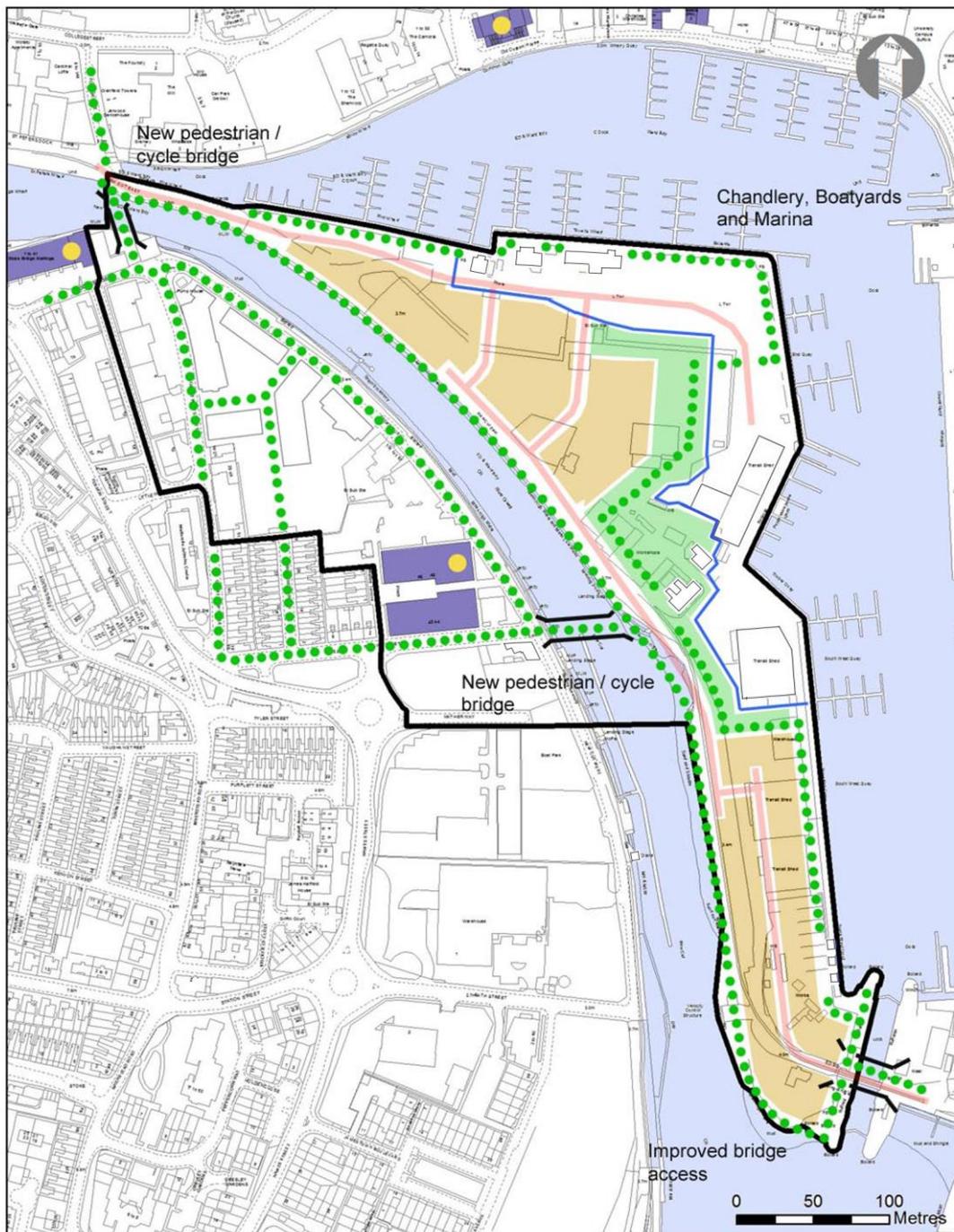


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A – Island Site. Development Options.

A - Island Site

Development Option



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Opportunity Area A – Island site

The Island is located in the heart of the Ipswich Waterfront on land that was reclaimed when the Wet Dock was constructed over 150 years ago creating an island linked to the ‘mainland’ by a narrow connection at St Peter’s Dock. In recent decades the usage of the Island has changed as industrial port activities have given way to leisure uses based around the development of the marina. The Island represents a key development opportunity in the regenerated Waterfront.

The development of the Island site can contribute in a variety of ways to the regeneration of the Waterfront area. It is appropriate for provision of low to medium rise development which would maintain the essential character of the Wet Dock Conservation Area and protect significant views across from the outer edges of the Waterfront. Space is available to provide some much needed green areas including reinstatement of the tree lined promenade. The old lock gate area provides a natural focus for leisure uses while there is still space for further development of marina related activity.

West of the New Cut, opportunities exist for new mixed use development which can enhance the setting of historic buildings such as Felaw Maltings and the use of the quayside promenade and historic connecting routes such as Great Whip Street and Felaw Street.

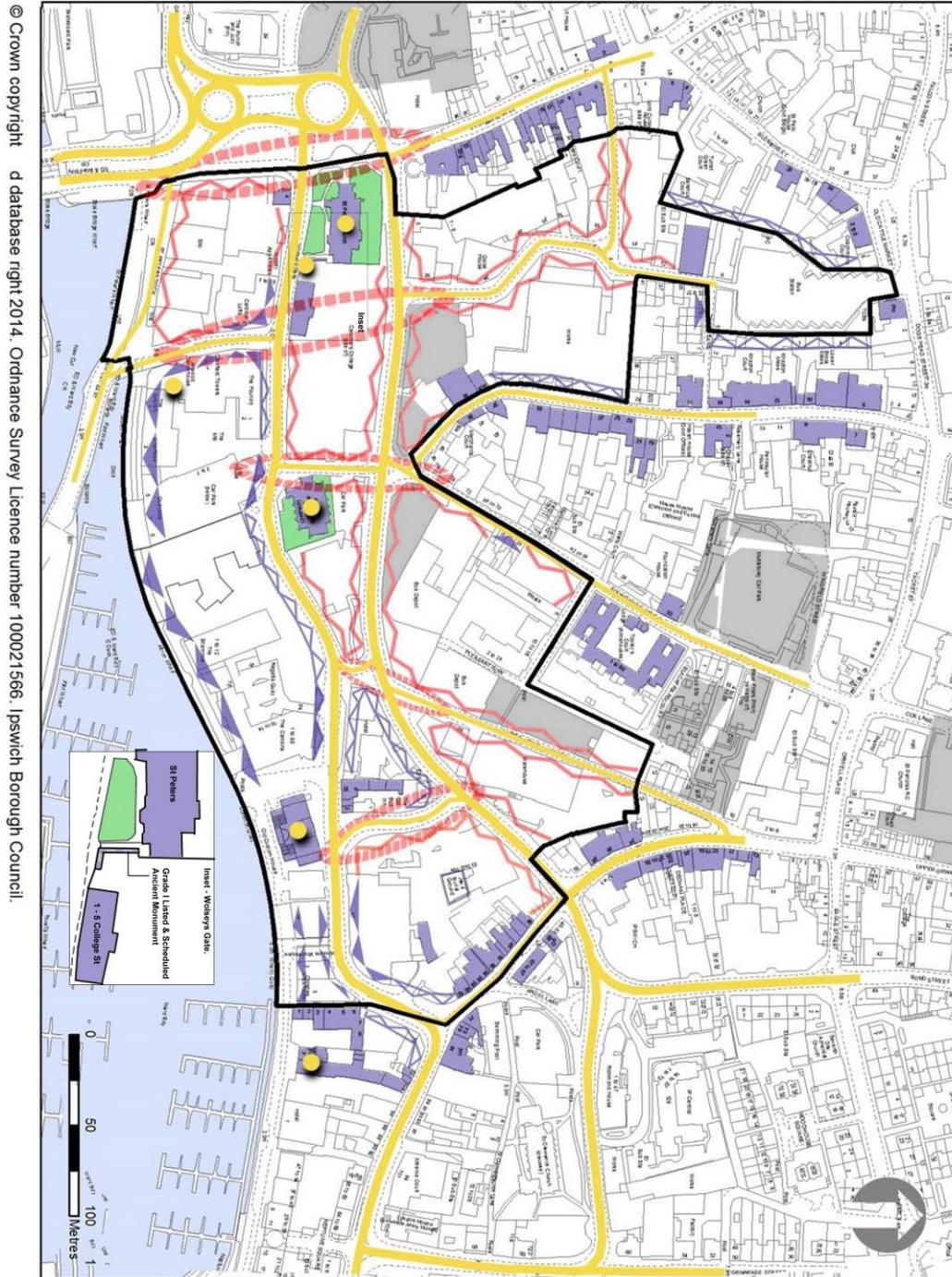
| Development Opportunities | Development Principles |
|--|---|
| <p>Mixed use development comprising (percentage is indicative):</p> <ul style="list-style-type: none"> • Residential (70%) could include live-work units • Marina moorings and shore based facilities at the south end of the Island • Marine related industry including boat building • Employment including office use • Small scale retail, cafes and restaurants • Heritage/cultural based visitor attraction • Public open space • Conversion of historic buildings • Waterfront promenade | <ul style="list-style-type: none"> • Retention, refurbishment and conversion of historic structures (Public Warehouse, Lock Keepers Cottages and Harbour Masters House) • Reinterpretation of historic lock as focus to new public space • Protection of key vistas across the island • Protection of predominantly open character of water area • Generally low to medium rise development (3 ,4 and 5 storeys) • High quality public realm/open spaces • Waterfront promenades to Wet Dock and Riverside • Development to provide vehicular access (including |

| | |
|--|--|
| | <p>emergency vehicles) and bridge across New Cut to link to Felaw Street</p> <ul style="list-style-type: none">• Layout should not prejudice the potential provision of a full Wet Dock crossing• Provision of cycle/pedestrian bridge across lock gate and westward pedestrian and cycle access across the New Cut• Layout to facilitate location of new foot/cycle bridge from New Cut to St Peter's Wharf• Layout and design to address flood risk• Development to take account of heritage issues including archaeology and Wet Dock Conservation Area.• Ensure suitable public transport provision |
|--|--|

B – Merchant Quarter. Site Analysis.

B - Merchants Quarter

Analysis

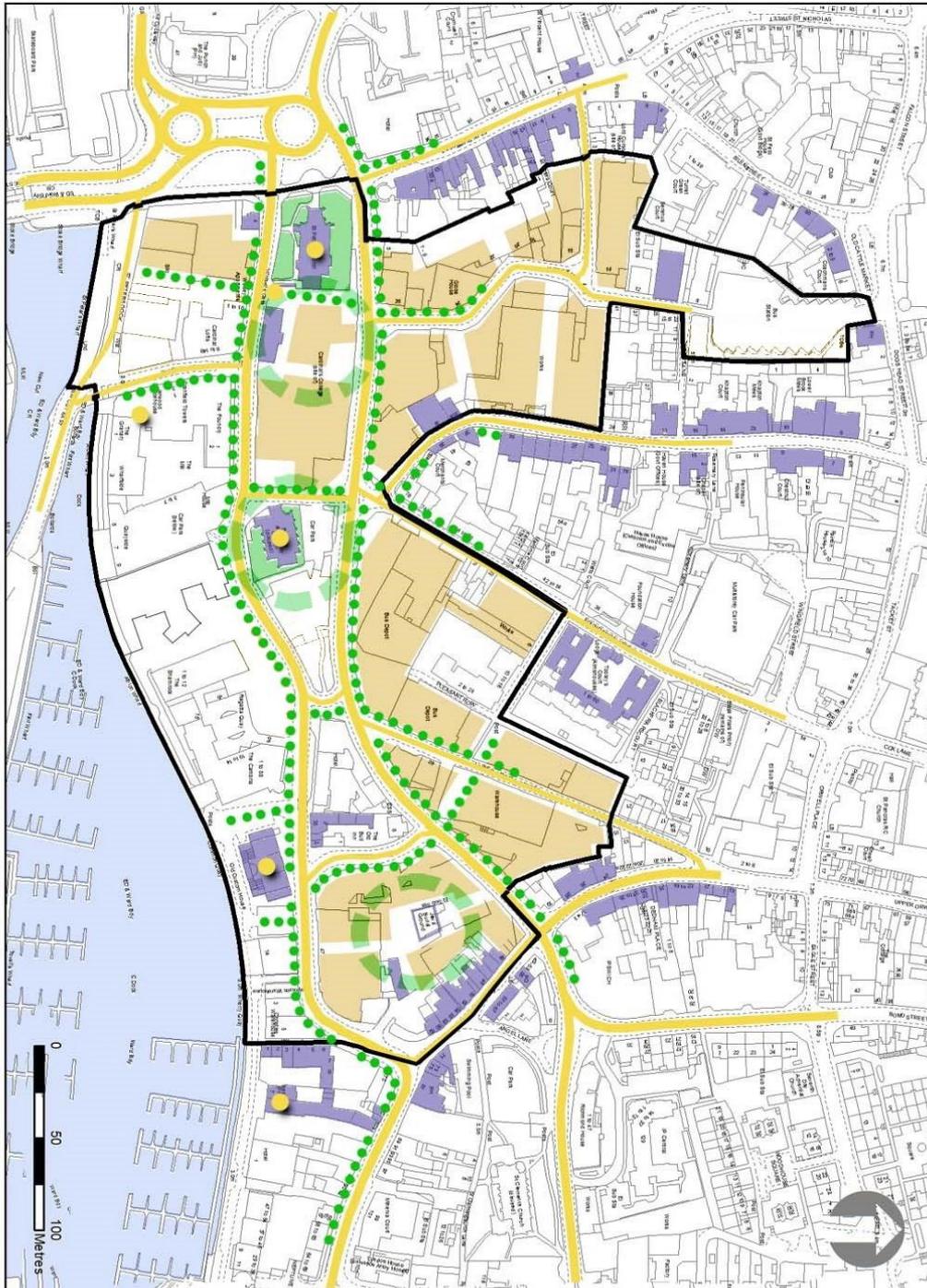


B – Merchant Quarter. Development Options.

B - Merchants Quarter

Development Option

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Opportunity Area B – Merchant Quarter

The area between the Wet Dock and the Central Shopping Area presents an opportunity to improve the links between the regenerated Waterfront area and the centre of town. The area includes several vacant and underused sites, the redevelopment of which would knit the area together from its present fragmented state.

Having undergone a period of development which has regenerated the Northern Quays, progress has now stopped, leaving a number of unfinished developments and gap sites. There are still issues of lack of connectivity between the Waterfront and the traditional Town Centre. The area between the old centre and Star Lane contains a network of streets, chiefly running north-south, but the linkage between this area and the Waterfront is affected by the barrier presented by the Star Lane gyratory road system. There is a poor quality environment in some parts of the area with several underused/vacant sites. Linked with proposals to lessen the impact of the Star Lane corridor, opportunities exist to reinforce the existing historic character and street pattern, promote improved pedestrian connection through and across the area, and migration of activity between the Town Centre and the Waterfront with the redevelopment of these underused/vacant sites. This area includes much of the internationally important area of the Anglo-Saxon and Medieval town and waterfront, including scheduled monuments and will involve considerable commitment of resources to archaeology.

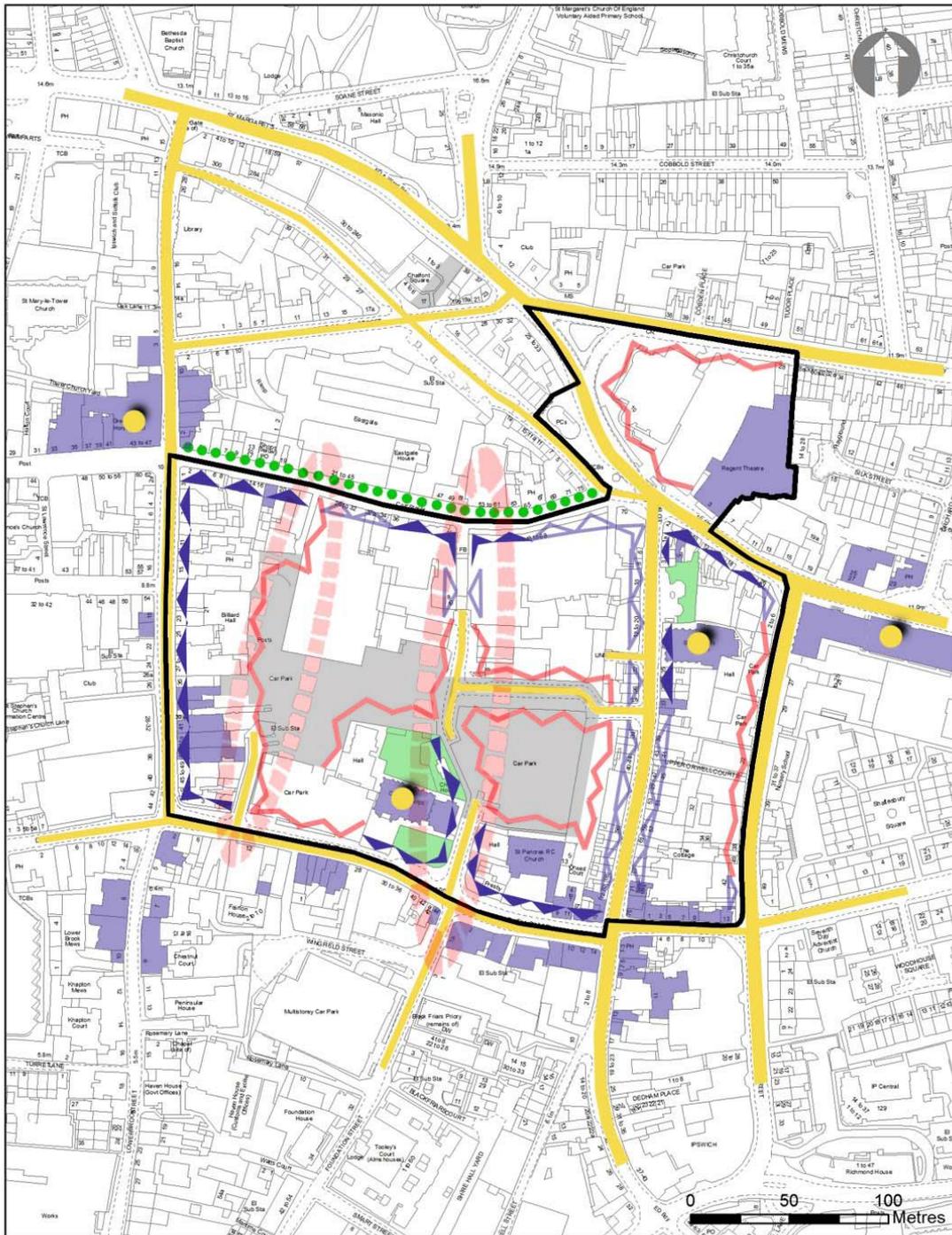
| Development Opportunities | Development Principles |
|---|--|
| <p>Mixed use development comprising:- Residential (could include live work units)</p> <p>Non-residential use</p> <ul style="list-style-type: none"> -Offices/businesses -Cafes/restaurants -Small scale retail | <ul style="list-style-type: none"> • Layout to relate to historic street pattern • Fine grain development of generally low rise (3 storeys) with increased scale at focal points, up to a maximum of 5 storeys, to reflect historic scale and grain. Taller buildings may be permitted in the tall building arc defined through policy DM15. • Enhance pedestrian linkage between town centre and waterfront with upgraded public realm. • Development to address street frontages – particularly Star Lane – and if possible allow for widening on Star Lane to |

| | |
|--|--|
| | <p>accommodate tree planting, cycle provision or wider footways.</p> <ul style="list-style-type: none">• Development to respect and enhance setting of Listed and historic buildings.• Development to address scheduled monuments and archaeology.• Replacement site for major EDF electricity sub-station.• Layout and design to address flood risk. |
|--|--|

C – Mint Quarter and surrounding area. Site Analysis.

C - Mint Quarter

Analysis

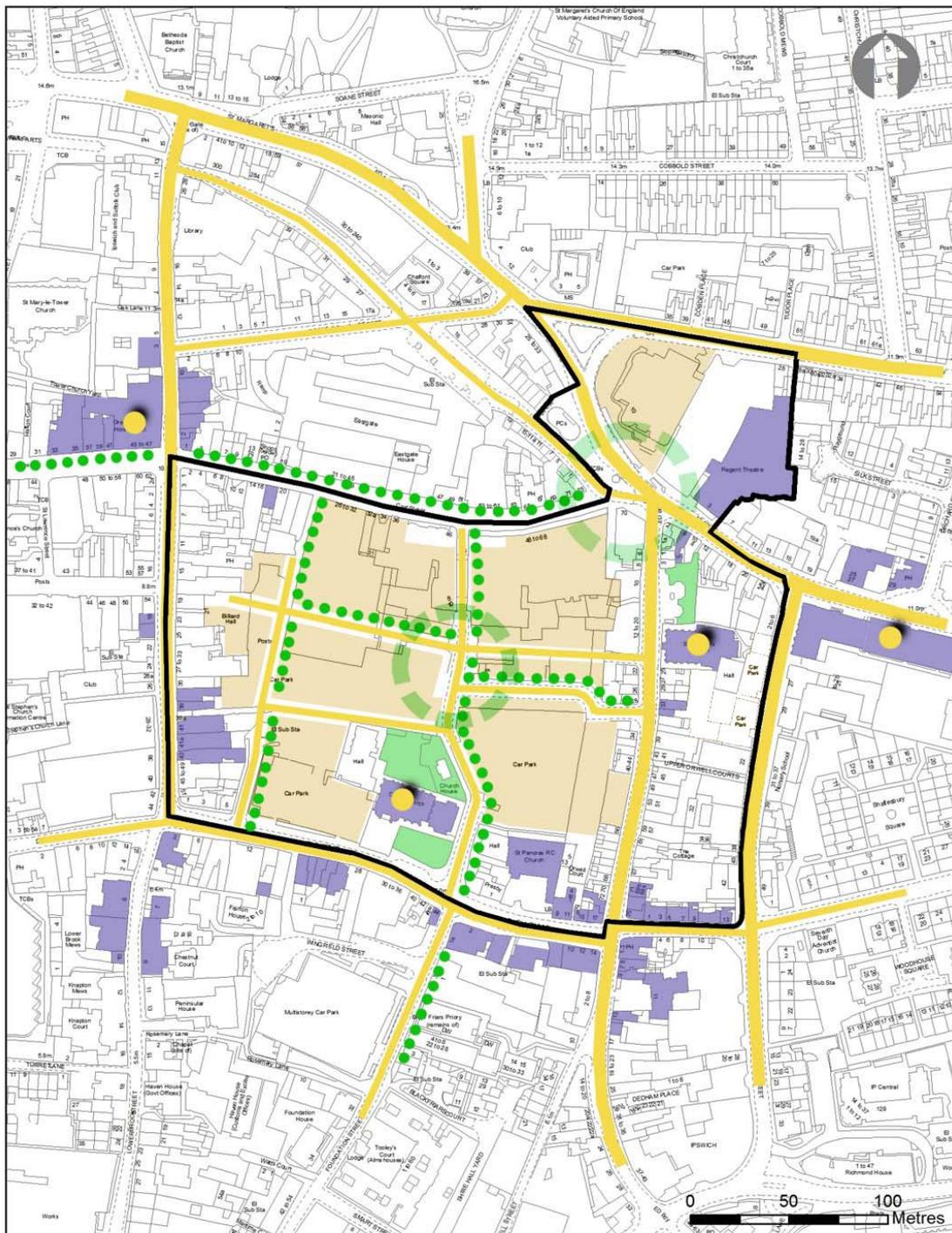


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C – Mint Quarter and surrounding area. Development Options.

C - Mint Quarter

Development Option



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Opportunity Area C – Mint Quarter / Cox Lane regeneration area and surrounding area

The ‘Mint Quarter’ is the name which has been applied to the proposed redevelopment area located within the block bounded by Carr Street, Upper Brook Street, Tacket Street/Orwell Place and Upper Orwell Street. Redevelopment of the Mint Quarter represents an opportunity for a mix of predominantly non-retail uses. Existing areas of surface parking will be replaced by a multi-storey short stay car park and redeveloped urban blocks with a legible layout of streets and public spaces. Development of the area will promote the residential-led regeneration of the adjoining peripheral shopping streets and much needed improvements to the public space at Major’s Corner.

Development of the main Mint Quarter area should be based around a continuation of the historic urban block structure, with a new pedestrian spine forming a continuation of Butter Market linking through to a new urban square located on the historic north-south route of Cox Lane. Ancillary routes should link through to the main routes enclosing the wider block and to acknowledge historic routes and features as appropriate. Residential accommodation should be provided at upper floors to provide an appropriate form and scale of development.

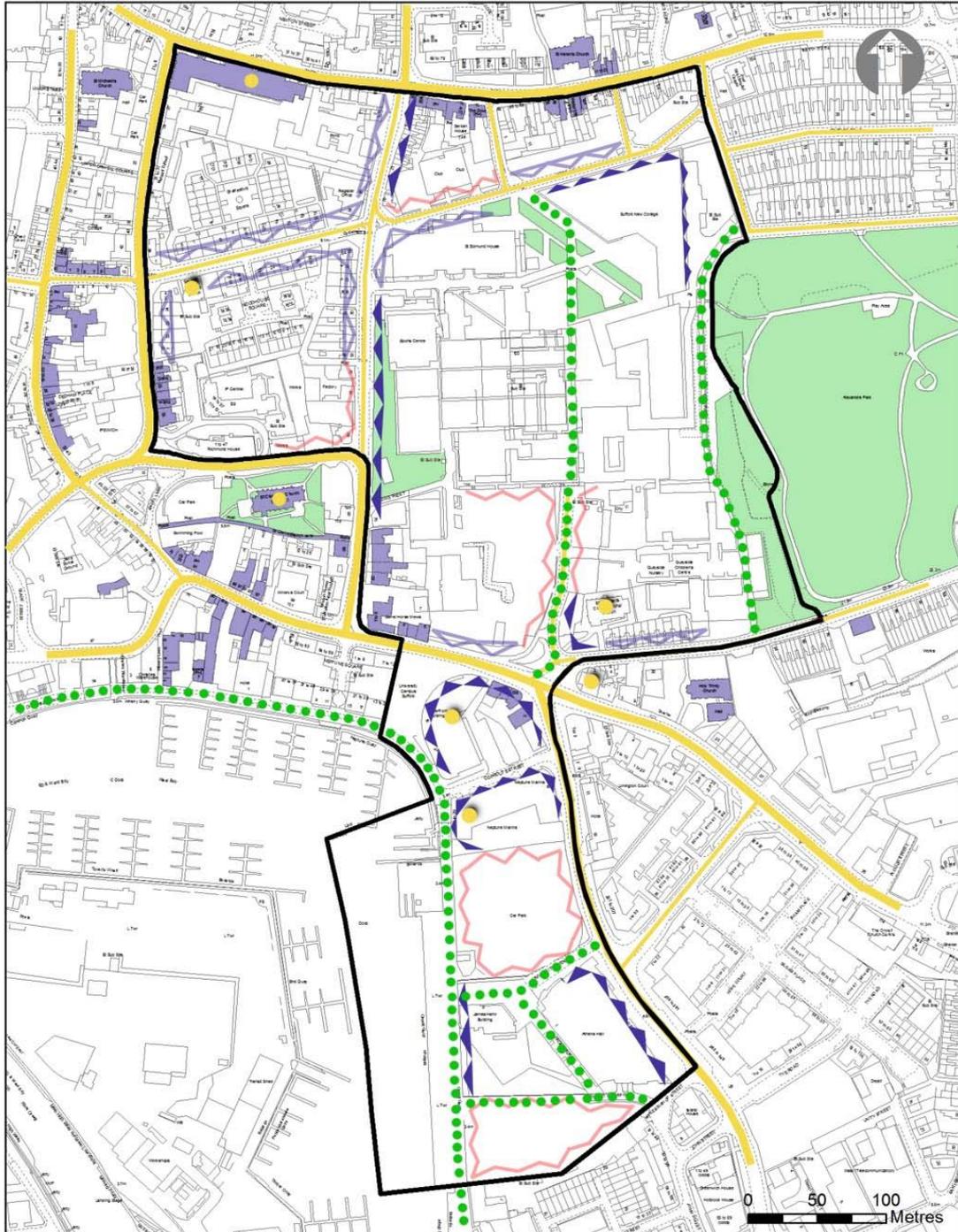
| Development Opportunities | Development Principles |
|--|--|
| <p>Predominantly non-retail mixed use development comprising:</p> <p>Residential</p> <p>Shoppers short stay car parking (multi storey)</p> <p>Public open space</p> <p>Café/restaurant uses</p> <p>Some retail on the western part of the site</p> <p>School</p> | <ul style="list-style-type: none"> • Pedestrian connection opposite Butter Market with new urban space • Enhanced pedestrian permeability east-west and north-south across site • Layout to promote active frontages at ground floor level • Development to respect and enhance setting of Listed and historic buildings • Development to address scheduled monuments and archaeology (much of the Mint Quarter site is a scheduled monument) including conservation principles and, where relevant, mitigation for |

| | |
|--|--|
| | <p>impacts on archaeological remains and enhancement of public understanding.</p> <ul style="list-style-type: none">• Development to provide appropriate building scale to historic street frontages and to be appropriate in the context of the Conservation Area.• Enhancement of linkage to Regent Theatre• Provision of major new landscaped public space at focus of scheme• Scheme to promote regeneration of Upper Orwell Street including environmental enhancements. |
|--|--|

D – Education Quarter and surrounding area. Site Analysis.

D - Education Quarter

Analysis

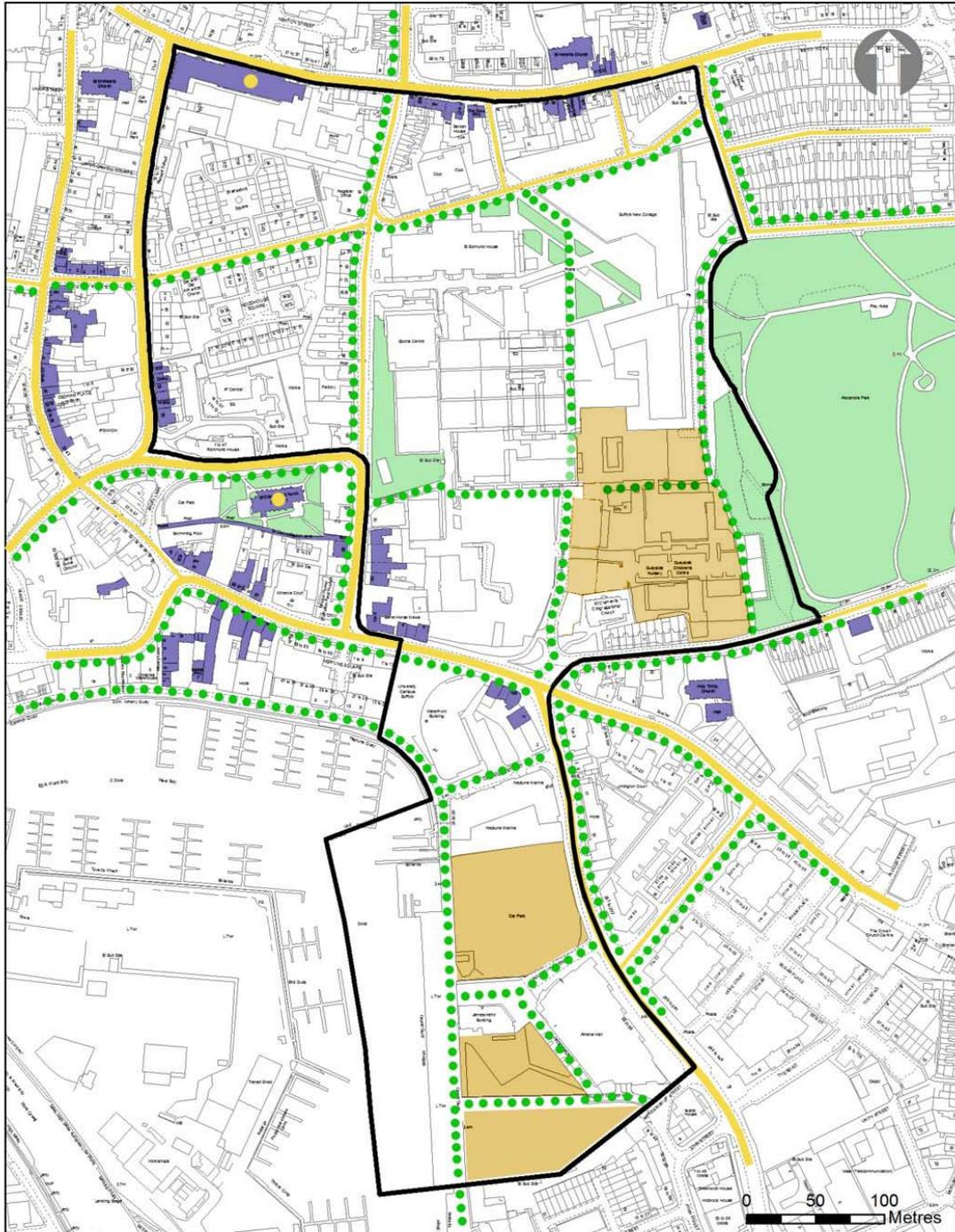


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D – Education Quarter and surrounding area. Development Options.

D - Education Quarter

Development Option



Opportunity Area D – Education Quarter and surrounding area

The ‘Education Quarter’ is located on the eastern side of the town centre, extending from just north of Rope Walk down to the Waterfront. It includes the higher and further education sites occupied by the University of Suffolk and Suffolk New College. The area also includes existing residential, commercial and leisure uses, which have the potential to create a vibrant mixed use quarter, providing a major activity node and linking the Town Centre, the Waterfront and the eastern side of the town.

The major investment associated with further UoS and SNC developments will not only lead to a physical reinvigoration of the area, with quality new buildings and public spaces, but will also generate increased activity levels and prosperity in the area, which can support spin off employment and service activities.

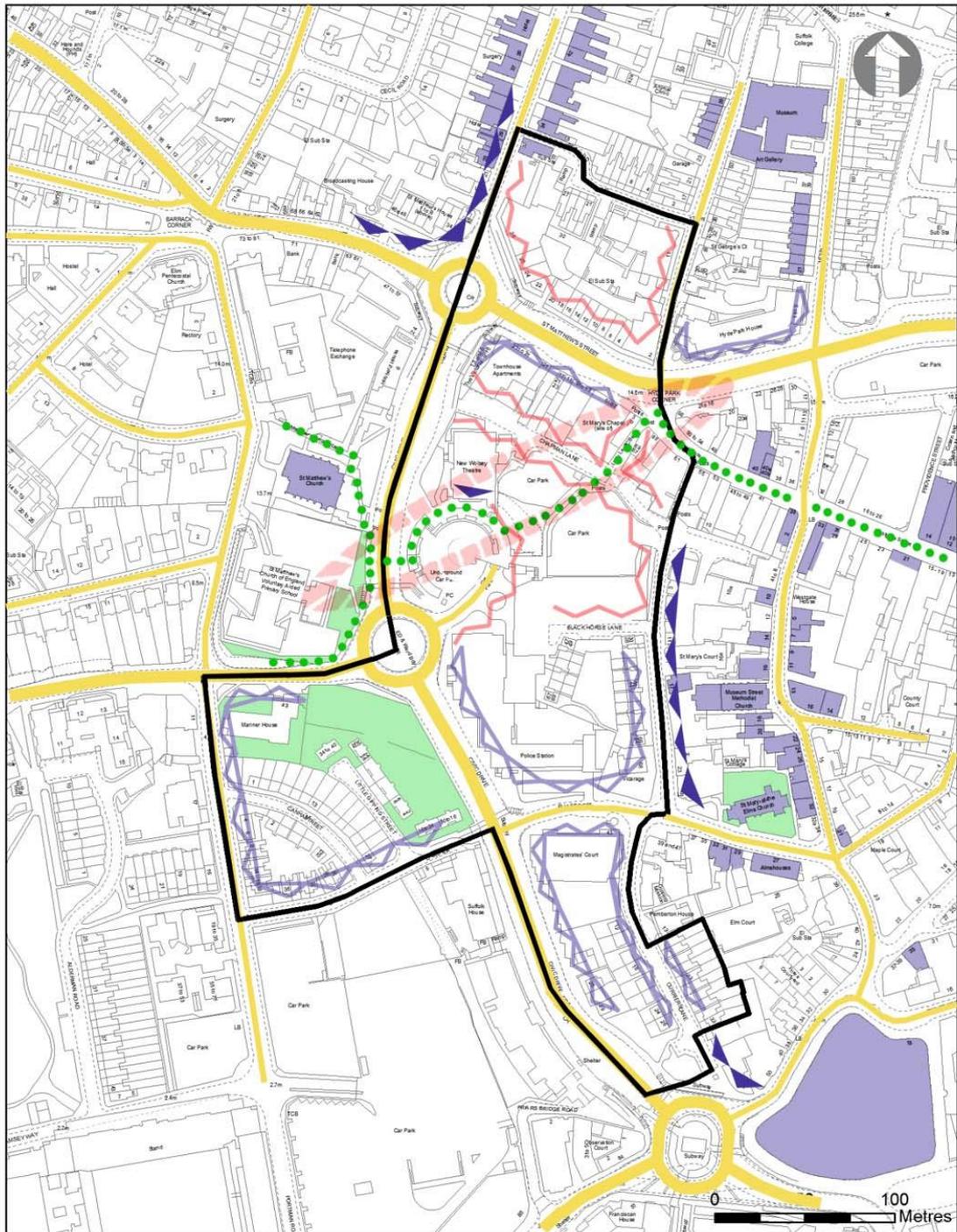
| Development Opportunities | Development Principles |
|---|--|
| <p>(percentage is indicative)</p> <p>Higher & Further Education uses (75%)</p> <ul style="list-style-type: none"> - Academic facilities - Support facilities - Student accommodation <p>Residential development</p> <p>Hotel</p> <p>Car parking (inc. public)</p> <p>Small scale retail, café/restaurant</p> <p>Offices/business</p> | <ul style="list-style-type: none"> • Scale, mass and form of development on Waterfront to be of varied height (minimum 6 storeys), responding to waterfront setting with layout maintaining glimpse views through to tree-lined skyline. • Fine grain, low rise (2-3 storeys) development north of Rope Walk to integrate with historic scale and character of St Helen’s Street and create a suitable transition to the modern buildings of the college campus. • Development to respect and enhance setting of Listed and historic buildings. • Development to address archaeology and Wet Dock, Central, and St Helen’s Conservation Areas. • Layout and design to address flood risk. |

| | |
|--|--|
| | <ul style="list-style-type: none">• Enhanced pedestrian and cycle permeability through the area and linking into wider networks. |
|--|--|

E – Westgate. Site Analysis.

E - Westgate

Analysis

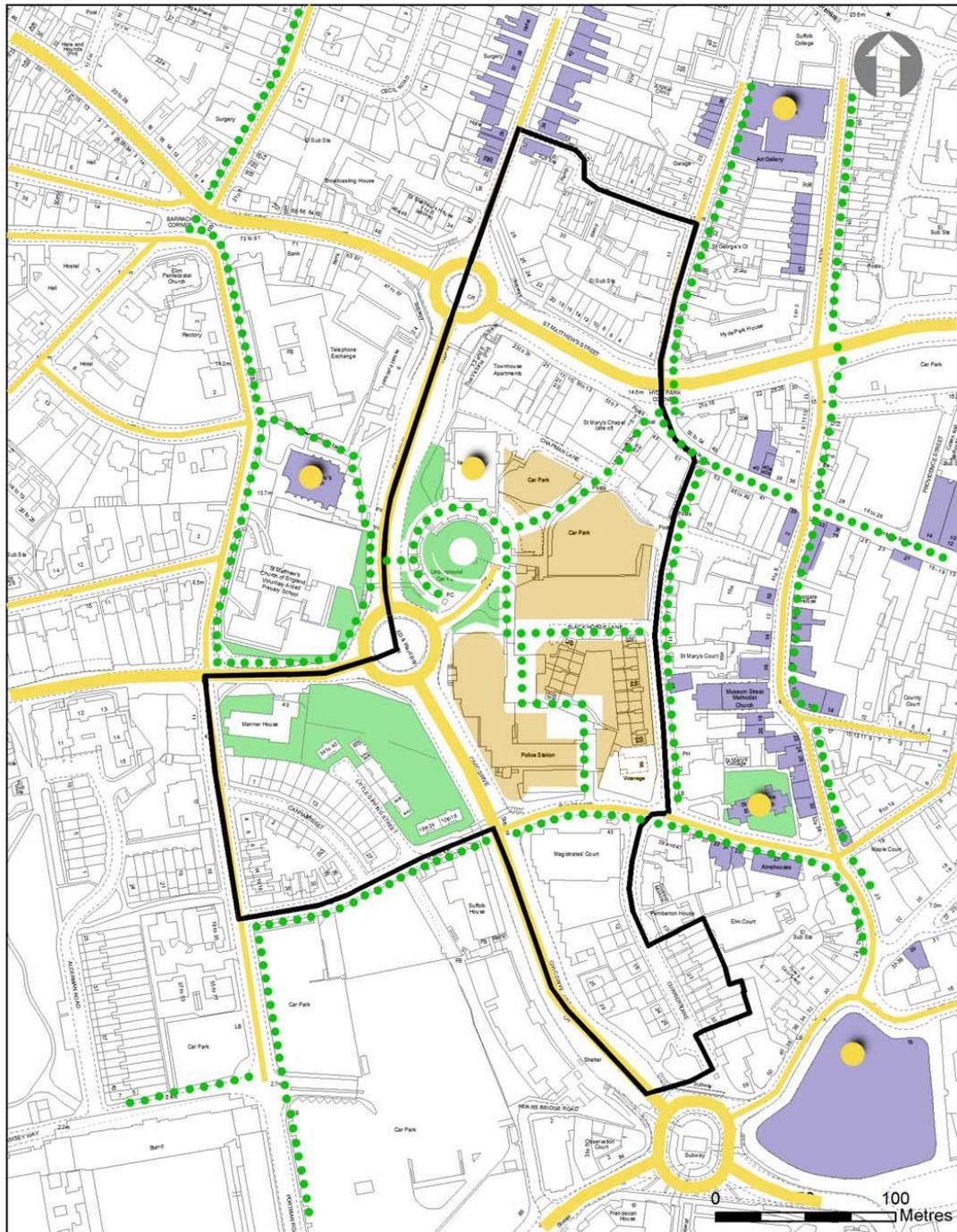


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E – Westgate. Development Options.

E - Westgate

Development Options



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Opportunity Area E – Westgate

Fringing the western end of the Town Centre, the ‘Westgate’ quarter is a mixed area with a varied collection of retail, business, civic, cultural and residential uses. To the west of the area, Civic Drive presents a barrier to linkage between the Town Centre and the inner suburban areas. With the demolition of the Civic Centre and the long standing need for regeneration of the area to the north of St Matthew’s Street, there is a significant opportunity to reinvigorate the area, with better integration of pedestrian movement and a more coherent built character.

Redevelopment of the former Civic Centre complex would form the centrepiece of a regenerated “Westgate” built around an enhanced pedestrian link through the site from the end of Westgate Street to a reactivated space in front of the New Wolsey Theatre. The barrier to pedestrian movement formed by Civic Drive and the series of underpasses beneath the St Matthew’s Street roundabout would be addressed by new surface level crossings as has been done at the Friars Street Princes Street end of Civic Drive as part of the Travel Ipswich scheme. There is the potential to enhance pedestrian links northwards across St Matthews Street, connecting with the Ipswich Museum site which has been identified for major investment. Existing developments of inappropriate scale would be replaced by new development of a scale more befitting their town centre location.

| Development Opportunities | Development Principles |
|--|---|
| <p>Residential led mixed use development with some retail on the former Civic Centre site IP040</p> <ul style="list-style-type: none"> - Residential - Retail - Café/restaurant uses - Shoppers car parking - Improved cultural network | <ul style="list-style-type: none"> • Layout to promote enhanced pedestrian linkage between Westgate Street and the New Wolsey theatre, northwards to Ipswich Museum on High St and eastwards between the retail allocation and Museum Street • Surface level pedestrian/cycle crossing across Civic Drive • Creation of new urban space at Westgate Street • Enhancement of plaza fronting New Wolsey Theatre, including replacement of redundant water feature • Redevelopment of the Civic Centre site to incorporate landmark building • Higher density housing to Civic Drive, Gt Gipping Street & Curriers |

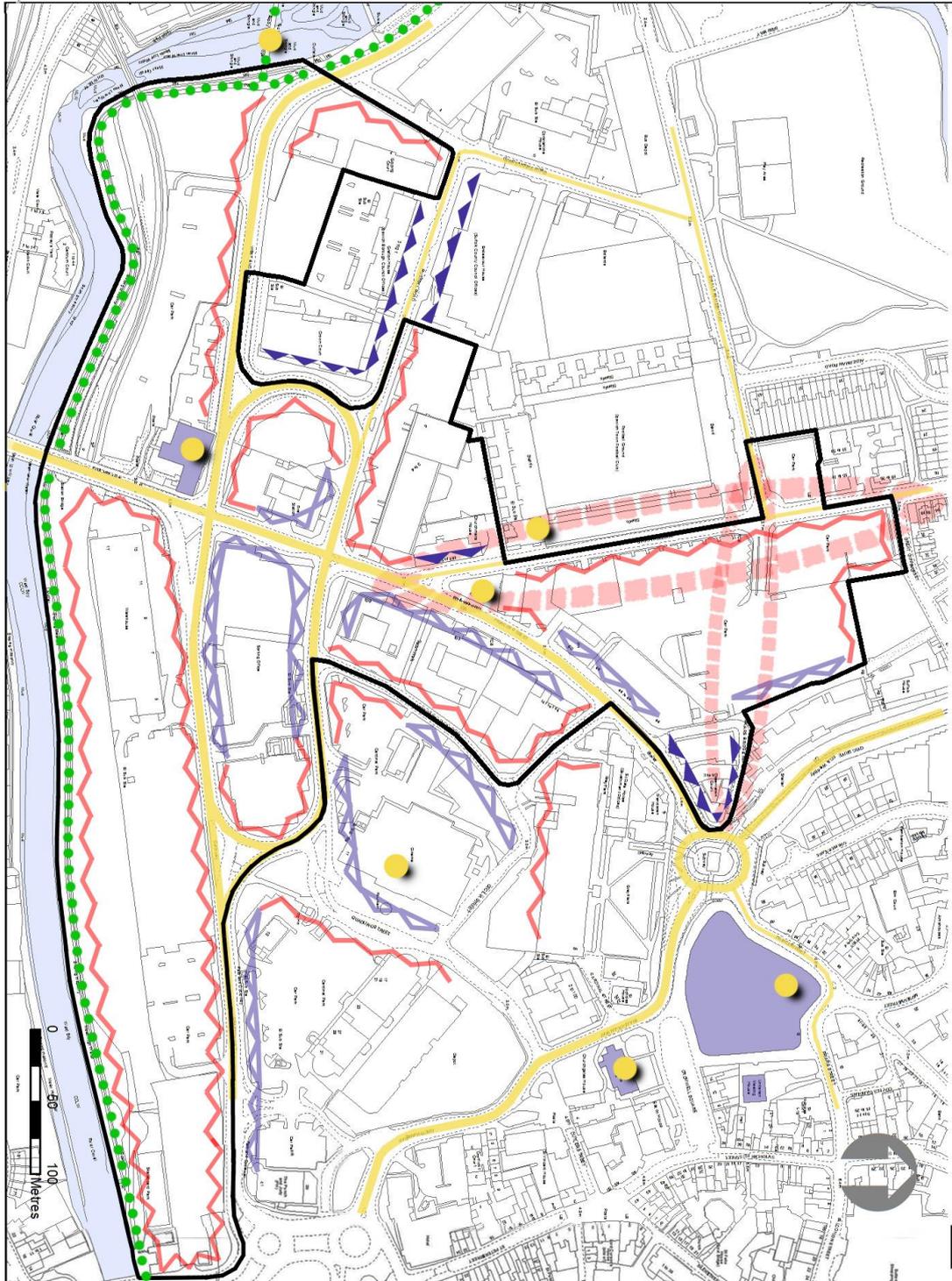
| | |
|--|---|
| | <p>Lane sites (4-6 storeys) as opportunities arise – housing on Black Horse Lane now excluded from allocations</p> <ul style="list-style-type: none">• Enhanced public realm in Elm Street• Development to take account of nearby heritage assets and archaeology• Ancillary routes should acknowledge historic routes and features as appropriate (in order to aid public understanding and appreciation of heritage). |
|--|---|

F – River and Princes Street Corridor. Site Analysis.

F - River & Princes Street Corridor

Analysis

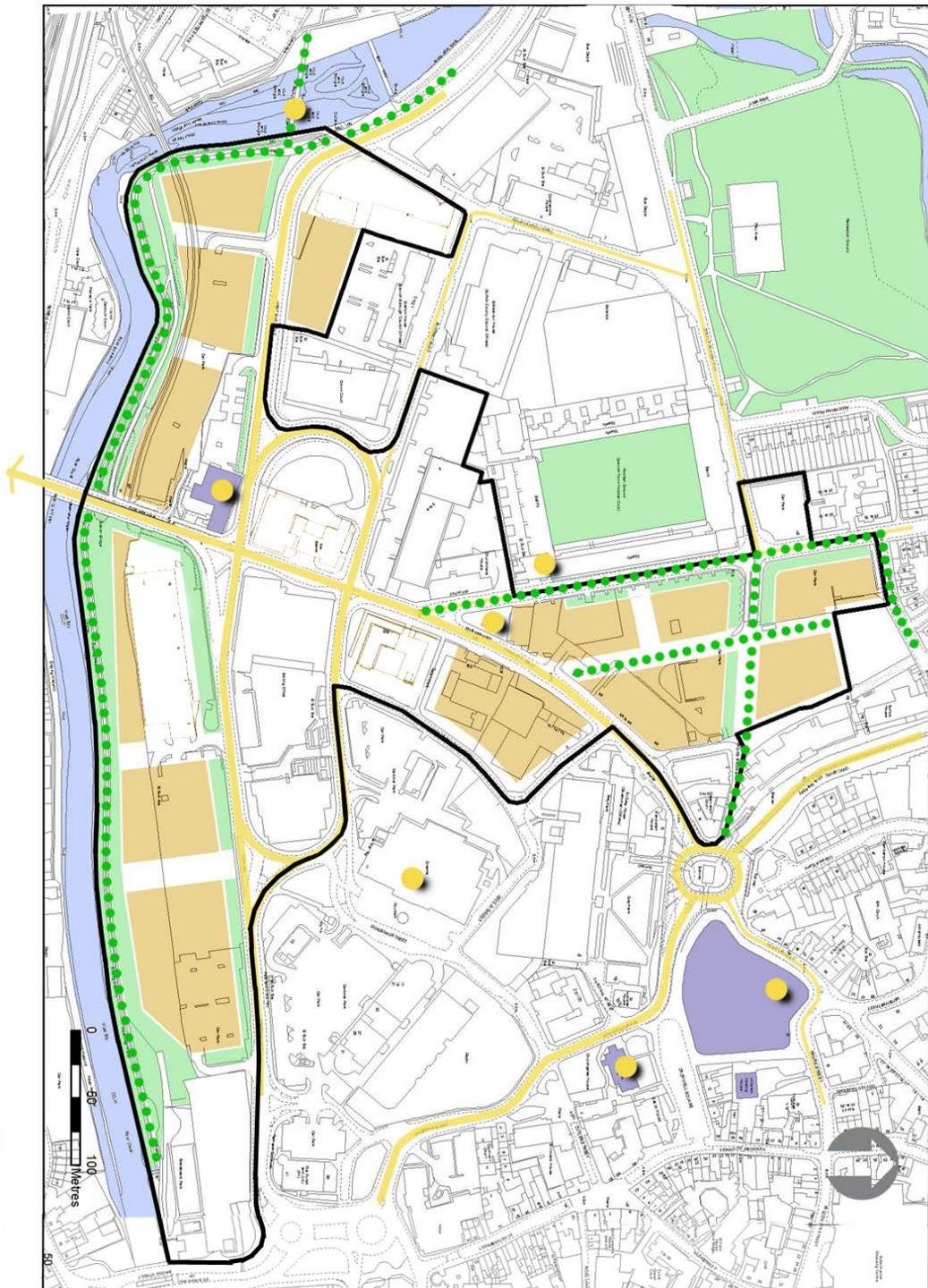
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F - River and Princes Street Corridor. Development Options.

F - River & Princes Street Corridor

Development Option



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Opportunity Area F – River and Princes Street Corridor

The 'River Corridor' occupies the large area to the north of the river in the vicinity of the railway station while the 'Princes Street Corridor' links the station to the centre of town and is designated as an Enterprise Zone. The riverside area is low lying and formed a fertile, grassy edge to the main settlement area since early times. Medieval watermills were replaced in the 19th century by waterside industries and the cattle market, then the road link to the railway station and Ipswich Town Football ground. The two corridors include many underused or vacant commercial sites, with a generally poor environmental quality and very fragmented townscape. Regeneration within the area presents an opportunity to consolidate the urban form either side of the important route from the railway station to the town centre, creating an attractive gateway to Ipswich and high quality public realm. The riverside connection to the Waterfront area can be enhanced as a setting for new pedestrian and cycle links.

The area of car parking to the east of Portman Road has potential to be redeveloped as a mixed use (predominantly office) environment, reconnecting the edge of the town centre to the east with inner urban housing and parkland to the west.

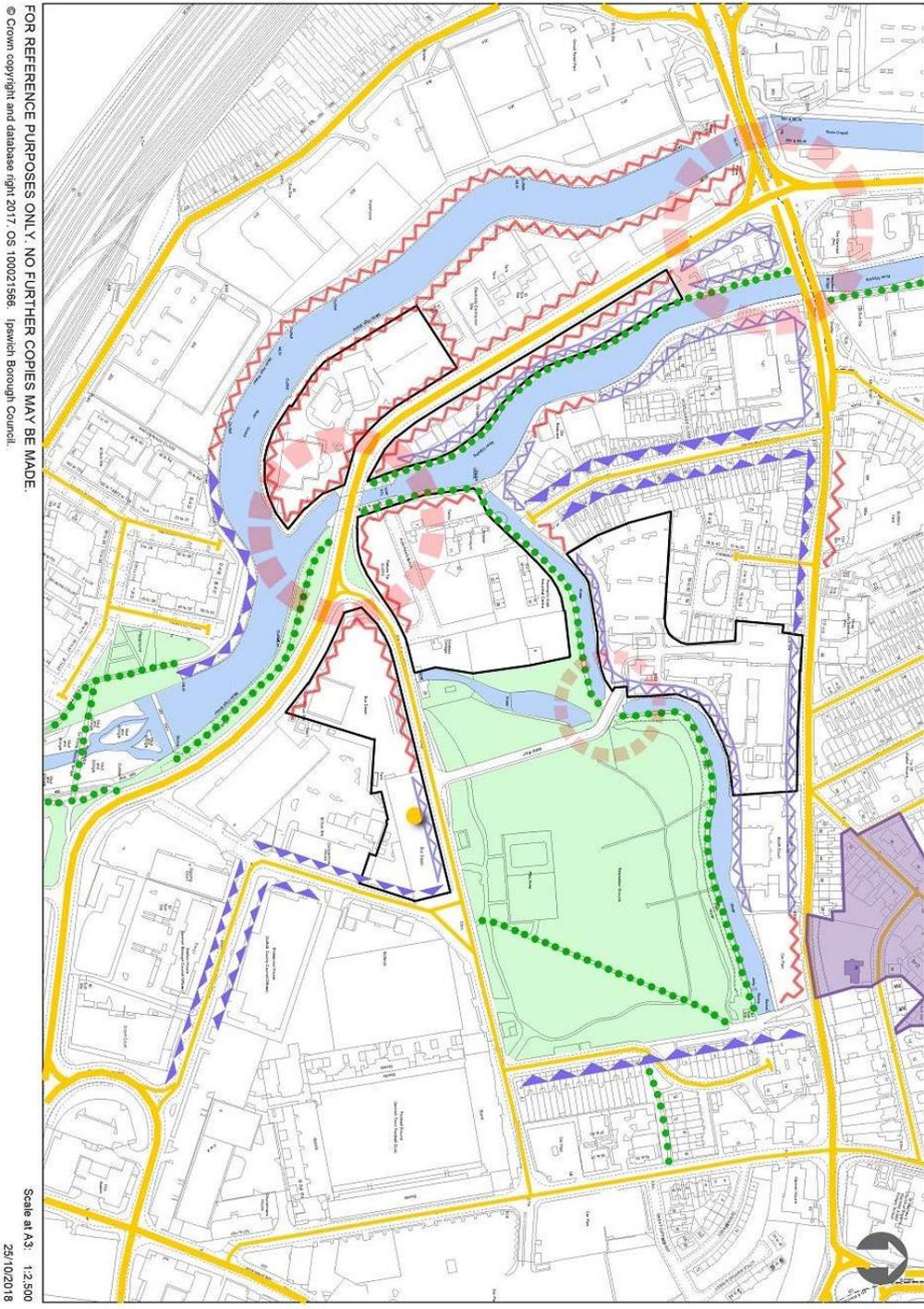
| Development Opportunities | Development Principles |
|---|---|
| <p>Office-led mixed use development</p> <ul style="list-style-type: none"> - Leisure - Car parking <p>Enhanced environment for people arriving at Ipswich by rail</p> <p>Residential uses where appropriate within mixed use developments adjacent to the river</p> | <ul style="list-style-type: none"> • Riverside green corridor with enhanced pedestrian and cycle routes to the Waterfront • Creation of new townscape east of Portman Road, with well-defined blocks and through routes • Development of Princes Street as a civic boulevard and gateway to town centre • Scale of development medium rise (4-6 storeys) with opportunities for enhanced scale in key locations • Layout and design to address flood risk • Layout and design to take account of the historic environment including archaeology • Enhancement of pedestrian/cycle access to the station from the town centre and along the river |

| | |
|--|----------|
| | corridor |
|--|----------|

G Upper Orwell River and Canalside

Opportunity Area G – Upper Orwell River and Canalside

Analysis



Opportunity Area G Upper Orwell River and Canalside

This area is characterised by a series of sites in commercial occupation, grouped around the under-used assets of Alderman Park and the waterside frontages of the Rivers Orwell and Gipping and the Alderman Canal. It offers opportunities for mixed-use redevelopment in a sustainable edge of town-centre location. The opportunity exists to create a riverside and parkland environment for development in place of the existing road-dominated layout.

Redevelopment would capitalise on the proximity to the Park and to the river / “canal” side. New developments would be located to take advantage of views of these amenity assets and to provide natural surveillance. Redevelopment of the Portman Walk industrial site for residential purposes will provide an enhanced environmental quality and encourage provision of a new pedestrian / cycle access through to Handford Road, aiding accessibility for both existing and new residents. Conversion of the historic tram-shed and reconfiguration of the old turning area into a quality public space will provide a much-needed focus to the area. Increased residential use and activity levels and adoption of revised traffic access arrangements will assist in reducing anti-social activity.

| Development Opportunities | Development Principles |
|--|--|
| <p>Riverside residential uses, where appropriate within mixed use developments.</p> <p>Non-residential uses could include offices or small scale retail or leisure uses.</p> | <ul style="list-style-type: none"> • IP003 - medium rise residential development (3 – 4 storeys) north of Sir Alf Ramsey Way with opportunity for feature block up to 6 storeys at west end of site, alongside River Gipping; layout to address park setting to the east, canal side to the north and river to the west. • IP120b - residential development west of West End Road, 3–4 storeys high and up to 8 storeys at southern end; design and layout to address river setting on both sides; landscape buffer to sub-station. • IP004 – mixed use development; existing historic Tram Shed building retained and converted for office use; residential development to the west fronting West End Road, up to 6 storeys in height. |

| | |
|--|--|
| | <ul style="list-style-type: none">• IP119 – residential development up to 2-3 storeys; landscape buffer to sub-station and at southern tip.• New foot / cycle connection to Handford Road, via Bibb Way or Cullingham Road.• Residential development to adopt perimeter block layout, with landscaped frontages addressing River Gipping, Alderman Canal and wildlife area.• Traffic calming to Sir Alf Ramsey Way.• Layout & design to address flood risk.• Cycle and pedestrian connections created where possible across the river and canal;• Public access to riverside incorporated into layouts and linking to the Waterfront.• Layout and design to take account of the historic environment including archaeology. |
|--|--|

H Holywells

Opportunity Area H – Holywells

Holywells

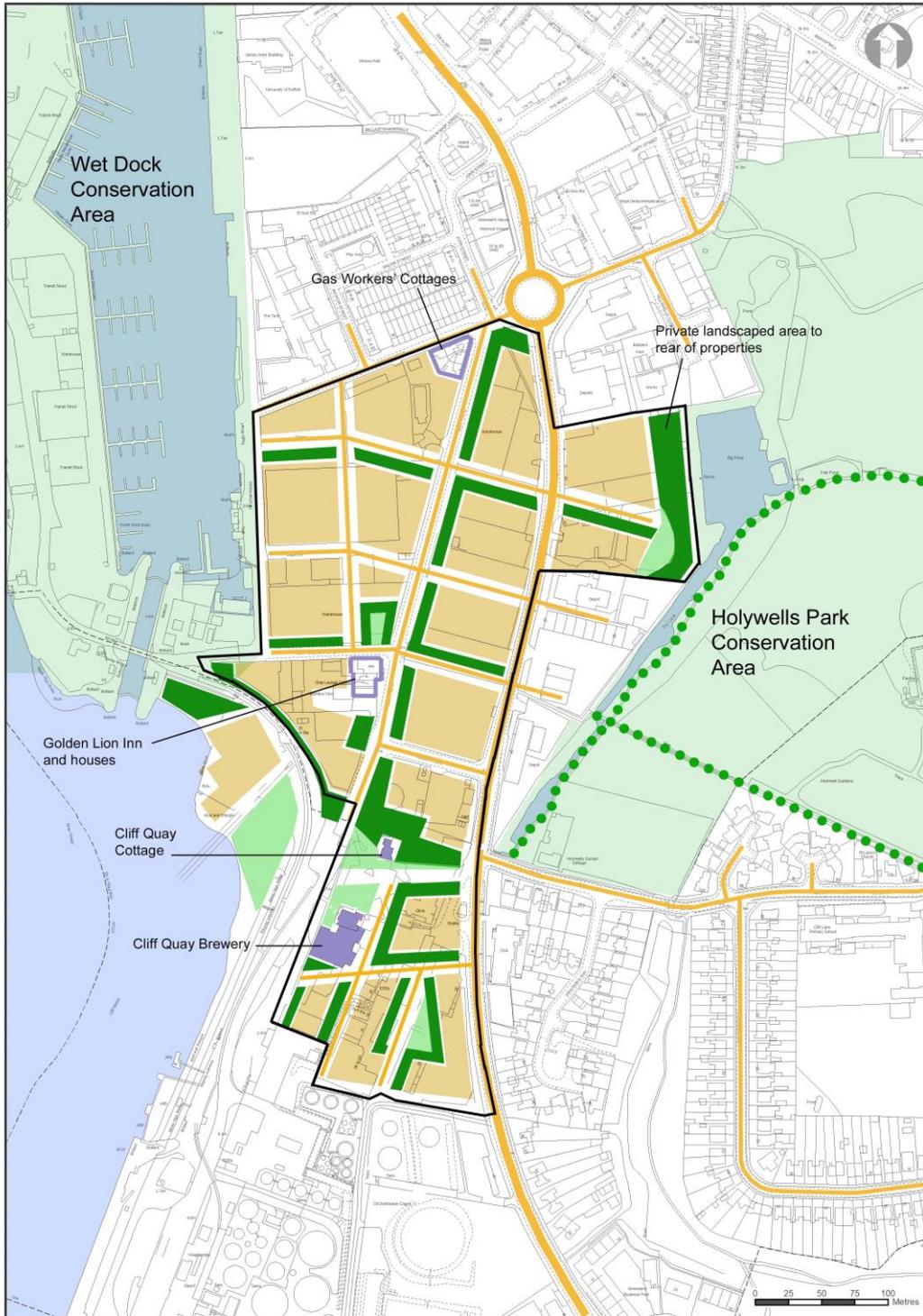
Analysis



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Holywells

Development Option



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Opportunity Area H Holywells

The area between the Wet Dock and Holywells Park presents an opportunity to link the Waterfront area to the green lung and to improve integration with the residential areas to the east. The area includes the vacant historic Tolly Cobbold Brewery complex. Also included in this opportunity area is the timber store yard (Anglo-Norden) which, when redeveloped, has the potential to act as a book-end to the Waterfront and complete the successful transformation of this area.

The first phases of regeneration of the Ipswich Waterfront have focused on sites on or close to the Wet Dock, but the recent development at Parkside (Duke Street) and of sites at Wherstead Road have highlighted the opportunities that exist for regenerating sites away from the immediate Waterfront, offering environmental improvement and enhanced integration with established residential communities which are sited away from the Dock area. Proposals for the area build on schemes previously approved for residential-led mixed-use development such as the major Eagle Mill proposals by Persimmon Homes at Cliff Road / Helena Road.

| Development Opportunities | Development Principles |
|---|---|
| <ul style="list-style-type: none"> • Residential-led development. • Riverfront regeneration and associated leisure uses • Secondary employment, community, leisure, arts and cultural development. • Pedestrian/ cycle links • Public Open Space | <ul style="list-style-type: none"> • Enhanced pedestrian/ cycle permeability east-west through the opportunity area, connecting the Waterfront and Wet Dock Crossing with Holywells Park. • Facilitate new Wet Dock vehicular crossing / bridge • Development to address street frontages • Promote redevelopment of former Shipyard area as “destination”, linked to regenerated Cliff Brewery via new Riverside esplanade • Scale of development generally medium-rise (3-5 storeys), with opportunities for taller buildings at key locations • Maintain views of treed skyline to East • Protect key view of Cliff Brewery from Helena Road • Reduce impact of Port related traffic, via traffic management & improved public realm |

| | |
|--|--|
| | <ul style="list-style-type: none">• Relocate Cliff Quay access control point south of Cliff Brewery, to improve public accessibility to Riverside and facilitate link between Brewery and Shipyard sites• Layout & design to address flood risk• Development to address risk from major hazard site (Vopak terminal) |
|--|--|

Part D

Implementation,
Targets, Monitoring
and Review

CHAPTER 7: Implementation, Targets, Monitoring and Review

Implementation

- 7.1 A key element of the Local Plan is the need to set out clear mechanism and targets for delivery. The Core Strategy sets out the main mechanisms by which the Council will assist with the delivery of the Framework (chapter 10).
- 7.2 It focuses on key partnerships that have been developed to assist in bringing forward for appropriate development the sites identified within this document.
- 7.3 Thus the key partnerships now which can support or assist with the delivery of development and infrastructure as set out in this development plan document are as follows:
- New Anglia Local Enterprise Partnership (NALEP);
 - Suffolk Growth Programme Board;
 - Ipswich Strategic Planning Area Board;
 - Ipswich Central; and
 - Ipswich Vision Board.
- 7.4 More detail on these partnerships and other components of delivery mechanisms are set out within Chapter 10 of the Core Strategy.

Targets

- 7.5 The Council recognises that it is important to have a limited number of measurable targets against which the delivery of the Local Plan can be assessed. These are set out within Chapter 11 of the Core Strategy Review document.

Monitoring and Review

- 7.6 The annual Authority Monitoring Report will review the progress against the targets set out in Chapter 11 of the Core Strategy focused review.
- 7.7 The Local Development Scheme states that the documents will be kept under regular review following their adoption.

Part E

Appendices

Appendix 1

A Summary of the Tests of Soundness

Development plan documents are subject to independent examination by a planning inspector. The inspector considers whether the plan has been prepared in accordance with the Duty to Cooperate and legal and procedural requirements, and whether it is 'sound'.

Soundness is assessed in terms of whether the plan meets the following tests of soundness, as set out in the National Planning Policy Framework (paragraph 35):

1. It should be positively prepared – the plan should be based on a strategy which seeks to meet objectively assessed development and infrastructure requirements, including unmet requirements from neighbouring authorities where it is practical to do so and consistent with achieving sustainable development;
2. It should be justified – the plan should be an appropriate strategy, when considered against the reasonable alternatives, based on proportionate evidence;
3. It should be effective – the plan should be deliverable over its period and based on effective joint working on cross-boundary strategic matters that have been dealt with rather than deferred as evidenced through a statement of common ground; and
4. It should be consistent with national policy – the plan should enable the delivery of sustainable development in accordance with the policies in the Framework.

Appendix 2

A list of Policies Contained in this Document

Chapter 4 Site Allocations

| | |
|------------|---|
| Policy SP1 | The protection of allocated sites |
| Policy SP2 | Land allocated for housing |
| Policy SP3 | Land with planning permission or awaiting a Section 106 |
| Policy SP4 | Opportunity Sites |
| Policy SP5 | Land allocated for employment use |
| Policy SP6 | Land allocated and protected as open space |
| Policy SP7 | Land allocated for leisure uses or community facilities |
| Policy SP8 | Orwell Country Park Extension |
| Policy SP9 | Safeguarding land for transport infrastructure |

Chapter 5 IP-One

| | |
|-------------|--|
| Policy SP10 | Retail Site Allocation |
| Policy SP11 | The Waterfront |
| Policy SP12 | Education Quarter |
| Policy SP13 | Portman Quarter (formerly called Ipswich Village) |
| Policy SP14 | Arts, Culture and Tourism Policy relocated to Part C Chapter 9 of the Core Strategy, as it is primarily a development management policy. |
| Policy SP15 | Improving pedestrian and cycle routes |
| Policy SP16 | Transport Proposals in IP-One |
| Policy SP17 | Town Centre Car Parking |

Chapter 6 Opportunity Areas

This chapter sets out urban design and development principles for the seven Opportunity Areas:

| | |
|---|--|
| A | Island Site |
| B | Merchant Quarter |
| C | Mint Quarter and surrounding area |
| D | Education Quarter and surrounding area |
| E | Westgate |
| F | River Corridor and Princes Street Corridor |
| G | Upper Orwell River and Canalside – this is the area in the vicinity of West End Road |
| H | Holywells |

Appendix 3

Appendix 3A – Site Allocation Details

Please note that development is required to take into account appropriate the constraints identified through the site sheets. Land allocations are made through the policies of the plan.

3B Development constraints for sites identified through policy SP3 where construction is not already underway

| Site reference | Address | Development constraints |
|----------------|---|---|
| IP005 | Former Tooks Bakery, Old Norwich Road | <p>Doctor's surgery to be included.</p> <p>Access constraints and possible contamination.</p> <p>The site is close to the Whitton Conservation Area. Any cumulative impacts on the conservation area with the development of adjacent site IP032 and site IP140 will need to be taken into account.</p> <p>Archaeology - the site lies on high ground above the Gipping Valley. The adjacent site IP032 has been subject to geophysical survey and a desk based assessment has been carried out for both sites. There is potential for remains of multiple periods on the site and trenched evaluation will be required.</p> <p>Water infrastructure and/or treatment upgrades will be required to serve the proposed growth, or diversion of assets may be required.</p> <p>This site falls within the 91.4m height consultation zones surrounding Wattisham airfield.</p> <p>A transport assessment and travel plan will be required.</p> <p>Surface water flooding local to site</p> |
| IP042 | Land between Cliff Quay and Landseer Road | <p>Archaeology - no objection in principle to development but it will require a condition relating to archaeological investigation attached to any planning consent.</p> <p>There are listed buildings within the site of planning permission 12/00700/OUT approved 19/12/13, e.g. the listed Brewery site.</p> <p>Flood risk.</p> <p>TPOs on site and adjacent.</p> |
| IP059b | Arclion House and Elton Park, Hadleigh Road | <p>Archaeology - this site is in a topographically favourable location overlooking the River Gipping, in a general area of prehistoric remains (IPS 104, IPS 034). No objection in principle to development but it will require a</p> |

| | | |
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| | | <p>condition relating to archaeological investigation attached to any planning consent. A desk based assessment would be appropriate in the first instance, to establish impacts of past land use.</p> <p>Need to ensure land is safeguarded to land a pedestrian and cycle bridge to the river path (see Policy SP9). Possible contamination, TPO on site or nearby and adjacent to a flood zone. Need to address drainage issues.</p> <p>This site falls within the 91.4m height consultation zones surrounding Wattisham airfield.</p> <p>A transport assessment and travel plan will be required.</p> |
| IP074 | Church and land at Upper Orwell Street | <p>Archaeology - this site lies in the historic core of the Anglo-Saxon town. No objection in principle to development but it will require a condition relating to archaeological investigation attached to any planning consent. Archaeological costs have the potential to be relatively high</p> <p>Listed building and adjacent to a TPO tree.</p> |
| IP088 | 79 Cauldwell Hall Road | No requirement for archaeology. |
| IP109 | R/O Jupiter Road & Reading Road | No requirement for archaeology. |
| IP116 | St Clement's Hospital Grounds | <p>Sports facilities should be retained or replaced.</p> <p>TPOs on site or nearby and adjacent to a local wildlife site (the golf course). Design and layout should support wildlife corridor functions. Bat and reptile surveys will be required prior to any vegetation clearance, and mitigation where appropriate.</p> <p>Archaeology - This development affects an area of high archaeological potential, as defined by information held by the County Historic Environment Record (HER). The proposed works will cause significant ground disturbance that have the potential to damage any archaeological deposits that exist. There is no objection in principle to development but any permission will require a condition relating to archaeological investigation. Historic buildings should be assessed.</p> <p>Water infrastructure and /or treatment upgrades will be required to serve the proposed growth, or diversion of assets may be required.</p> |
| IP131 | Milton Street | Possible contamination. Surface water flooding local to site. |
| IP142 | Land at Duke | Close to an Air Quality Management Area, possible |

| | | |
|--------|--|---|
| | Street | contamination, and TPOs on site or nearby (an application for Tree Works may be needed). Surface water flooding local to site - will need to be considered at planning application stage. |
| IP150a | Ravenswood S & T (adjacent Fen Bight Circle) | Archaeology - the site lies in the vicinity of Prehistoric and Medieval sites. No objection in principle to development but it will require a condition relating to archaeological investigation attached to any planning consent. Development should link into cycling and pedestrian route networks. |
| IP169 | 23-25 Burrell Road | Archaeology - this site lies on the bank of the Orwell. No objection in principle to development but it will require a condition relating to archaeological investigation attached to any planning consent. Archaeological monitoring of groundworks is likely to be appropriate. Flood risk. |
| IP200 | Griffin Wharf, Bath Street | Archaeology - this is a large site on the edge of the river, close to Medieval sites (IPS 145, IPS 294). No objection in principle to development but it will require a condition relating to archaeological investigation attached to any planning consent. Flood risk. |
| IP205 | Burton's, College Street | Archaeology - this site lies in an area of international archaeological importance, on the Anglo-Saxon and Medieval waterfront of Ipswich (Historic Environment Record IPS 413). This site potentially represents that last surviving section of 'early' waterfront. The site will involve potentially high excavation costs. Measures for archaeology should be addressed at an appropriate stage in the planning process. Early consultation and evaluation is advised so that decisions can be taken on preservation <i>in situ</i> , and/or appropriate investigation strategies designed. Flood risk. |
| IP206 | Cranfields, College Street | Archaeology - this site has been subject to large scale excavation but post-excavation work was stalled by the development situation and there is still an outstanding obligation for assessment, conservation, analysis and publication, particularly of important waterlogged |

| | | |
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| | | <p>wooden structures from the Anglo-Saxon waterfront. Preservation by record has not been met, and the condition on IP/04/00313 remains outstanding. Any new applications would require a condition or agreement to secure post-excavation work. Any additional proposed groundworks should be subject to consultation.</p> <p>Flood risk.</p> |
| IP211 | Regatta Quay, Key Street (subject to further discussions with agent) | <p>Archaeology - this site has largely been excavated. Applications involving further groundworks should be subject to a condition relating to archaeology, where appropriate.</p> <p>Flood risk.</p> |
| IP214 | 300 Old Foundry Road | <p>The site is in the Central Conservation Area and an Air Quality Management. This site lies within the historic core of Ipswich and within the Area of Archaeological Importance (IPS413), close to the town defences. Detailed early pre-application discussions with Suffolk County Council Archaeological Service would be required to agree the scope of required assessment and to inform design.</p> <p>Surface water flooding local to site.</p> |
| IP245 | 12-12a Arcade Street | <p>The site is in the Central Conservation Area and next to a listed building. The site is also within the Area of Archaeological Importance (IPS413). Detailed early pre-application discussions with Suffolk County Council Archaeological Service would be required to agree the scope of required assessment and to inform design (e.g. to allow for preservation in-situ of deposits or appropriate programmes of work). Archaeological remains may be complex and important and mitigation could involve significant costs and timescales.</p> <p>Surface water flooding local to site - will need to be considered at planning application stage. There is a Flood Incident Report for this site.</p> |
| IP256 | Artificial Hockey Pitch, Ipswich Sports Club | <p>Development needs to accord with Core Strategy policy DM5.</p> <p>TPO along the eastern boundary.</p> <p>Possible drainage constraint. Surface water flooding local to site - will need to be considered at planning application stage.</p> <p>This site lies in the vicinity of Iron Age and Roman sites. Whilst it remains an area of archaeological potential,</p> |

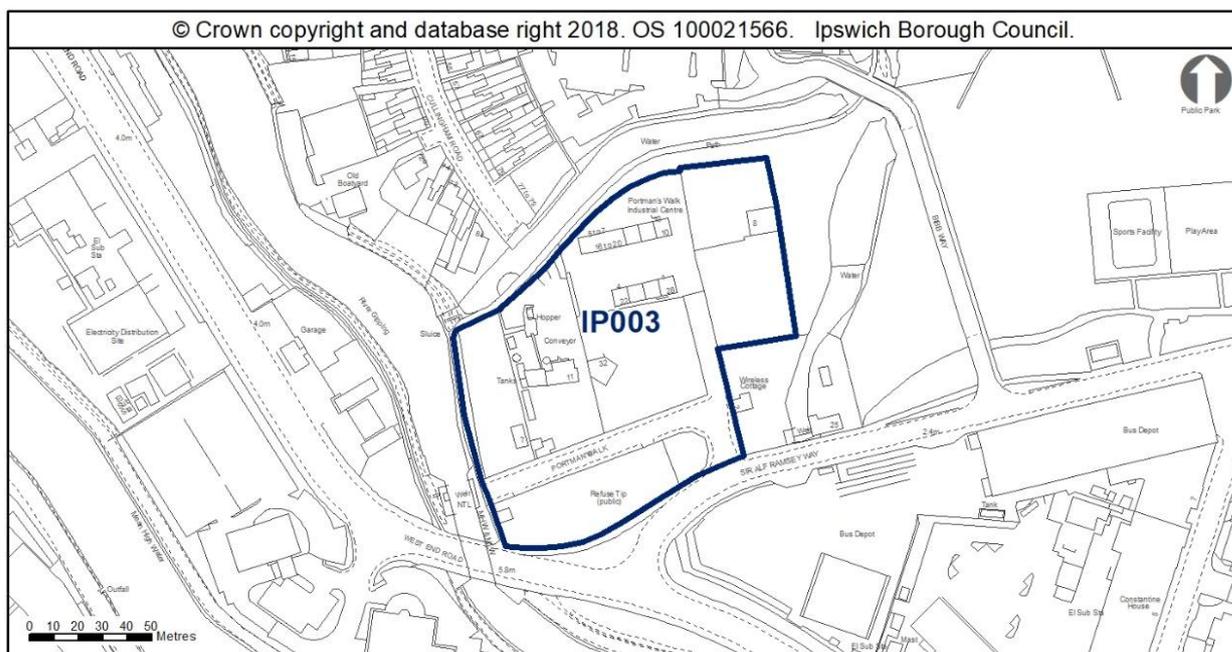
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| | | given the impacts of previous landscaping there would be no requirement for an archaeological condition or work on this site on the basis that it looks heavily truncated. |
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Appendix 3

Appendix 3B – Site Sheets (Part one)

Site ref: IP003 Waste tip and employment area north of Sir Alf Ramsey Way

Site area: 1.41ha



Allocation Policy SP2

| Use(s) | | Indicative capacity |
|-----------|----------------------------|---------------------------------|
| Primary | Residential | 114 (90dph* on 90% of the site) |
| Secondary | Small Scale retail/leisure | 200sqm |
| | Offices B1(a) | 800sqm |

* see Core Strategy policy DM23 for minimum and average densities

Adopted Plan 2017

N/A – new site but was previously considered for 100% housing through IP-One Area Action Plan Preferred Options 2007.

Current use

Waste recycling centre, concrete batching plant & employment premises

Development constraints/issues

Development principles for the Upper Orwell River and Canalside within which the site is located, are set out in Chapter 6 of the Site Allocations and Policies Plan (See 'Opportunity Area G').

Possible contaminated land, an assessment would be required. The site currently holds an environmental permit which will need to be surrendered when the area is redeveloped. If controlled waste is to be removed, the operator must ensure a registered carrier is used to convey the waste to a suitably permitted facility. All documentation to be kept in accordance with regulations. Further guidance can be found on the Environment Agency's Waste pages.

In terms of archaeology, the Development and Archaeology SPD 2018, states that in this part of the town, the archaeological questions and impacts of development are different from those within the core. There are likely to be deep reclamation deposits. Where major excavations are undertaken, recording may be necessary to record preserved layers and structural remains. Generally, however, geotechnical modelling would be fundamental in the first instance. Palaeoenvironmental modelling may be required to capture information relating to the river before it is destroyed, but deposits are not anticipated to be complex in the urban sense, although there is potential for waterlogged remains. Generally, it would be anticipated that archaeological matters could be dealt with through a condition on consent, depending on details. Questions exist around the character and nature through time of the river, manmade channels, and the marsh.

Parts of the site are covered by facility retention policies of the Suffolk County Council Minerals and Waste Local Plan. Alternative sites will need to be agreed with the County Council and the site operators for the relocation of the Concrete Batching Plant and Household Waste Recycling Centre before the sites can be made available.

Water infrastructure and/or treatment upgrades may be required to serve the proposed growth, or diversion of assets may be required.

Surface water flooding local to site - will need to be considered at planning application stage. See Appendix 1 of the Ipswich SFRA.

Development must take account of the River Corridor Buffer (10m) where no development should take place.

A transport assessment and travel plan will be required.

County Wildlife Site and Local Nature Reserve adjacent to the site. The site may have wildlife interest an ecological, bat and reptile survey will be needed. Development of this site should ensure that the Alderman Canal corridor and its associated habitats are buffered and enhanced. Any future green space should be sited adjacent to the

canal, to complement it. The design and layout would need to support the wildlife corridor function. The recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate. . Footpath 44 (River Path) exists along the site's western and northern edge.

New development on this site should be of high quality and have regard for the context of the site, and thus should address the Alderman Road Recreation Ground, river walk, River Gipping and Sir Alf Ramsey Way, providing active, engaging frontages to all of these outlooks.

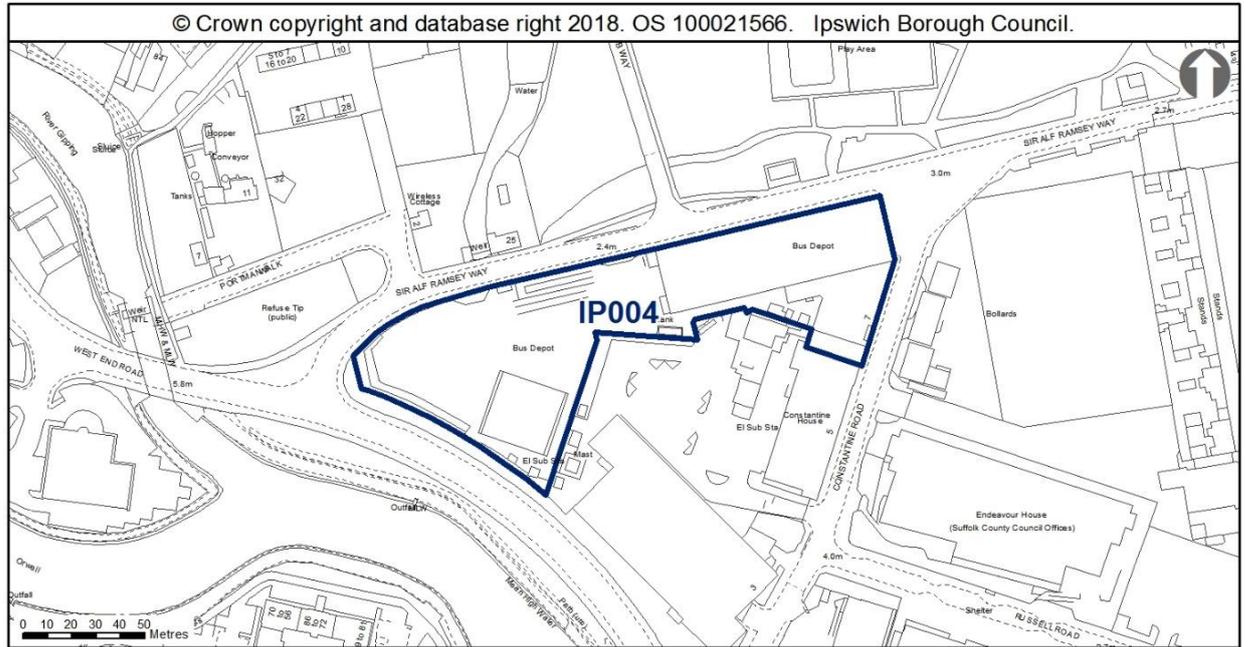
Attention is drawn to the Valley Ipswich Urban Characterisation Study SPD adopted in 2015, which states, 'The Alderman Canal is a historic feature which has been managed as natural riverside habitat accessed by wooden walkways. This is an approach which should be developed wherever possible in the Valley character area. A wide range of building types and designs is possible in this setting. The area lacks good visual connectivity to the town centre, however (commercial buildings along Civic Drive have created a wall-like barrier). Master planning for vacant sites in the Portman Road area should take into account the need for a legible new street network which links where possible with the inner urban area'.

Pedestrian access to Cullingham Road could be provided to link London Road and Portman's Walk via a less vehicle dominated route.

Opportunities for soft landscaping within the allocation site should also be explored, as a way of extending the green landscape of Alderman Park into the allocation area once developed.

Site ref: IP004 (UC004) Bus Depot, Sir Alf Ramsey Way

Site area: 1.07ha



Allocation Policies SP2 & SP5

| Use(s) | | Indicative capacity |
|---------|---------------|----------------------------|
| Primary | Residential | 48 (90dph on 50% of site*) |
| | Offices (B1a) | 5,000sqm |

* see Core Strategy policy DM23 for minimum and average densities

Adopted Plan 2017

48 dwellings at 90dph on 50% of the site

Offices (B1a) 5,000sqm

Current use

Ipswich Buses bus depot

Development constraints / issues

Development principles for the Upper Orwell River and Canalside within which the site is located, are set out in Chapter 6 of the Site Allocations and Policies Plan (See 'Opportunity Area G').

Need to relocate the bus station first.

Development is required to retain the tram shed with the expectation that it would be converted for office use. The tram shed is included on the Local List of buildings of townscape interest.

In terms of archaeology, this site is close to prehistoric remains (IPS 004, 148 and 150). There is no archaeological objection in principle to development but any permission will require a condition relating to archaeological investigation. Historic buildings on the site would require assessment.

Flood risk and possible contamination are further constraints. Surface water flooding local to site - will need to be considered at planning application stage. See Appendix 1 of the Ipswich SFRA.

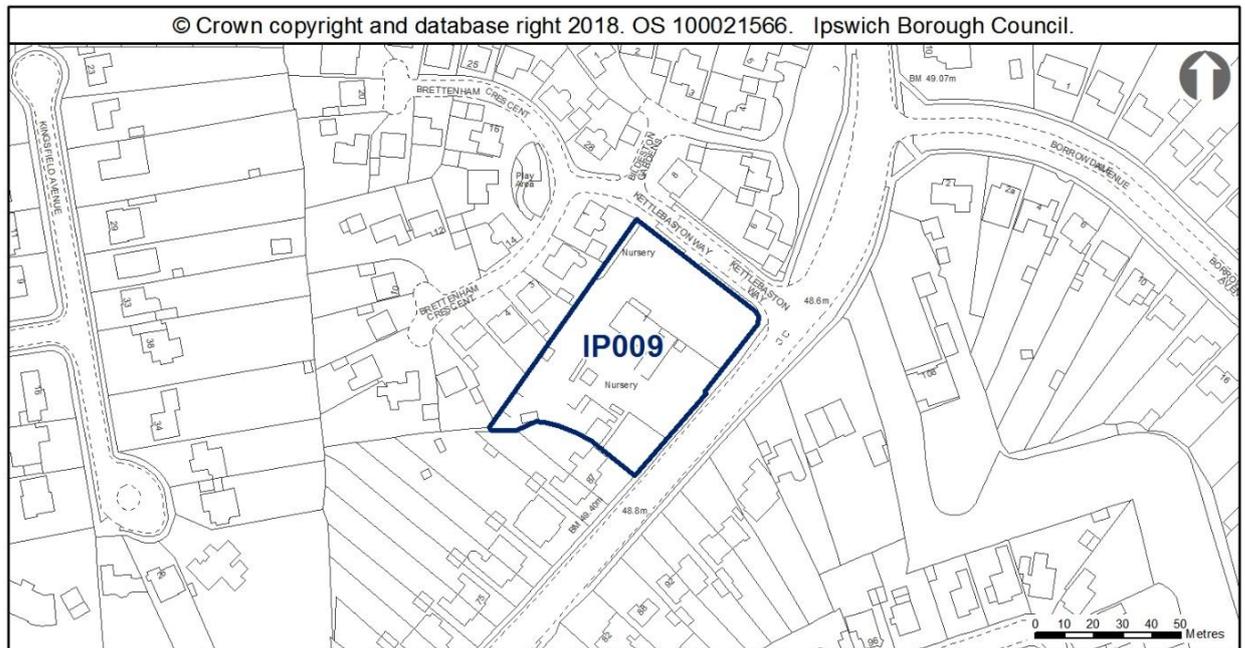
A transport assessment will be required.

The site is close to uses which are safeguarded through the Suffolk Waste Core Strategy, and is also in close proximity to existing minerals uses.

New development on this site would need to have regard for the heritage asset within the allocation area, the early 20th century bus shed which is included on the Buildings of Local and Townscape Interest SPD. It is expected that the bus shed will be retained and converted as part of the development of this site. A conversion scheme would need to respect the architectural language and detail of the existing building, maintaining the existing openings and respecting the rhythm of the fenestration scheme.

The open bus parking area to the west of the bus depot could accommodate a contemporary 3-4 storey building, using the junction of West End Road/Portman's Walk as the principal design focus to create a landmark building. Development in this location should have an elevation design which would address both West End Road and Portman Walk, acknowledging both roads with principal frontages. Some spacing/stepping down would be required to address the change in scale of the former bus depot to ensure the locally listed bus shed is not dominated by the new development.

Site ref: IP009 (UC009) Victoria Nurseries, Westerfield Road
Site area: 0.39ha



Allocation Policy SP2

| Use(s) | Indicative capacity |
|-------------|---------------------|
| Residential | 12 (30dph*) |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Plan 2017

100% housing at low density – 12 dwellings

Current use

Plant nursery and small convenience shop

Development constraints / issues

Possible contamination and a TPO on site or adjacent. Surface water flooding local to site - will need to be considered at planning application stage. See Appendix 1 of the Ipswich SFRA.

This site is close to a known Prehistoric site (IPS 246). There is no objection in principle to development but any permission will require a condition relating to archaeological investigation.

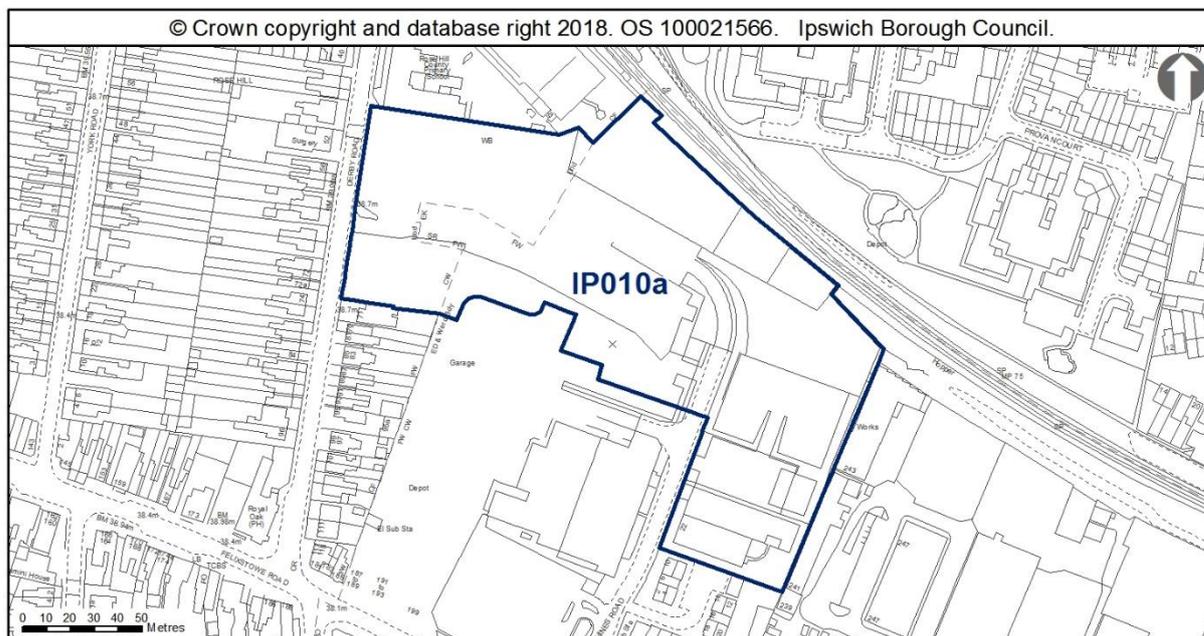
Development of this site would need to address both Westerfield Road and Kettlebaston Way, providing active, engaging elevations to both frontages. A development scheme would need to respect the prevailing domestic scale of

surrounding development of detached and pairs of semi-detached dwellinghouses adjacent. Continuation of the established building line of Westerfield Road would be encouraged which would also allow for domestic landscaping to the front of the site, which would contribute to the suburban character of the area, as noted in the Parks Ipswich Urban Characterisation Study, adopted in 2015.

A bespoke approach to design would be encouraged to address the varied architectural character of properties along Westerfield Road as the road transitions from the more historic 19th buildings to the south into the 1930s suburban expansion of the town to the north.

Site ref: IP010a (UC010) Co-Op Depot Felixstowe Road

Site area: 2.22ha



Allocation Policies SP2, SP7 & SP9

| Use(s) | | Indicative capacity |
|-----------|---|----------------------------|
| Primary | Residential | 75 (45dph on 75% of site*) |
| Secondary | School extension (approximately 25%) | 0.5ha |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Plan 2017

As above.

Current use

Vacant or part used yards, and employment premises.

Development constraints / issues

Expansion needed at Rose Hill School.

Possible contamination, TPO on site or nearby, noise from the railway. Design and layout would need to support the wildlife corridor function of the railway. An ecological survey (including flora, reptiles, bats and badgers) will be needed prior to any vegetation clearance and mitigation where appropriate. To achieve biodiversity net gain, the recommendations of the Ipswich Wildlife Audit 2019 could be

incorporated into future development, unless other means of biodiversity enhancement are appropriate.

In terms of archaeology, this site lies close to prehistoric and Palaeolithic remains (IPS 056). Depending on the nature of ground works, a condition may be recommended for archaeological works.

A transport assessment and travel plan will be required. Land should be reserved as part of the development of either IP010a or IP010b to facilitate development of a cycle and pedestrian bridge to link the District Centre with the housing to the north of the railway.

This allocation site is accessed off Derby Road and Felixstowe Road and wraps around the north of the Rosehill Centre adjacent to the railway line. The future development of this site should, if feasible, not prohibit the adjacent allocation at IP010b from being accessed from Hines Road.

The proposed extension to Rose Hill Primary School should reflect the distinctive character of the existing school buildings onto Derby Road, a building with origins in the early 20th Century, which experienced remodelling and extensions in the middle of the century. It features various textured brickwork bonding, canted bays with moderne and art deco influences in curved elements and wide windows with a strong horizontal emphasis. The school extension should seek to respond to these architectural influences in the design and appearance of the extension, whilst also being read independently of the existing range to act as a landmark building to signify the gateway to the new development of the allocation site.

The residential development of this allocation site should respect the established grid layout of the Rosehill area, and follow the perimeter block form with active frontages facing the streets, an established characteristic of the area, as identified in the California Urban Characterisation Study SPD. Existing dwellinghouses in California are principally red brick terraces and pairs of semi-detached houses, with often a prevailing architectural feature which characterises a particular road or area, such as the position of the front door, the pattern of fenestration, the use of bay windows, which has led to some distinctive areas of development.

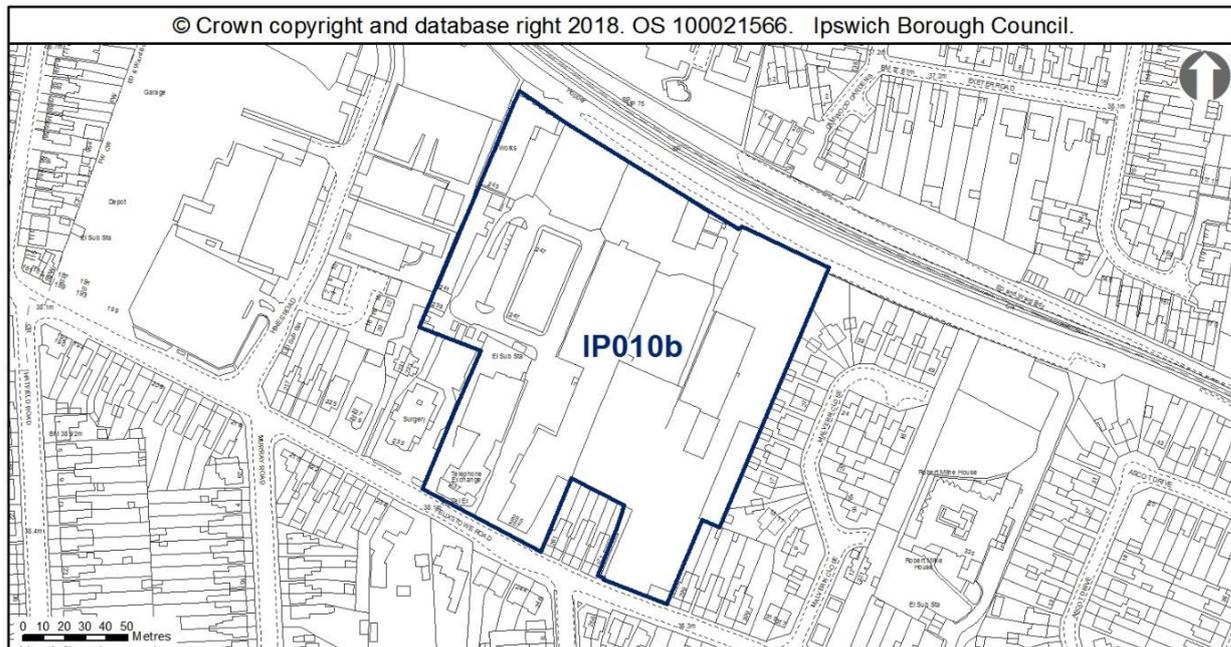
This varied approach to employing architectural details to create pockets of distinguishable housing should be incorporated into the development of the allocation site to ensure the design of the new development is high quality and distinctive.

Parking should be incorporated into the design proposals to encourage the public realm to contribute positively to the character and experience of the development at the allocation site.

Development of the site should consider the enhancement of pedestrian links to the school avoiding main roads in the interests of highway safety.

Site ref: IP010b (UC010) Felixstowe Road

Site area: 2.79a



Allocation Policy SP2 & SP9

| Use(s) | | Indicative capacity |
|-----------|--|----------------------------|
| Primary | Residential | 62 (45dph* on 50% of site) |
| Secondary | Current employment uses (land not available for development) | - |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Plan 2017

60% housing at medium density – 75 dwellings

40% Current employment uses

Current use

Various employment uses and multiple occupiers

Development constraints / issues

Redevelopment is dependent on existing uses being relocated.

Possible contamination, TPO on site or nearby, noise from the railway. Design and layout would need to support the wildlife corridor function of the railway. An ecological survey (including flora, reptiles, bats and badgers) will be needed prior to any vegetation clearance and mitigation where appropriate. To achieve biodiversity

net gain, the recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate.

In terms of archaeology, this site lies close to prehistoric and Palaeolithic remains on Foxhall Road (IPS 056). Depending on the nature of ground works, a condition may be recommended for archaeological works, with a desk-based assessment in the first instance.

A transport assessment and travel plan will be required. Land should be reserved as part of the development of either IP010a or IP010b to facilitate development of a cycle and pedestrian bridge to link the District Centre with the housing to the north of the railway.

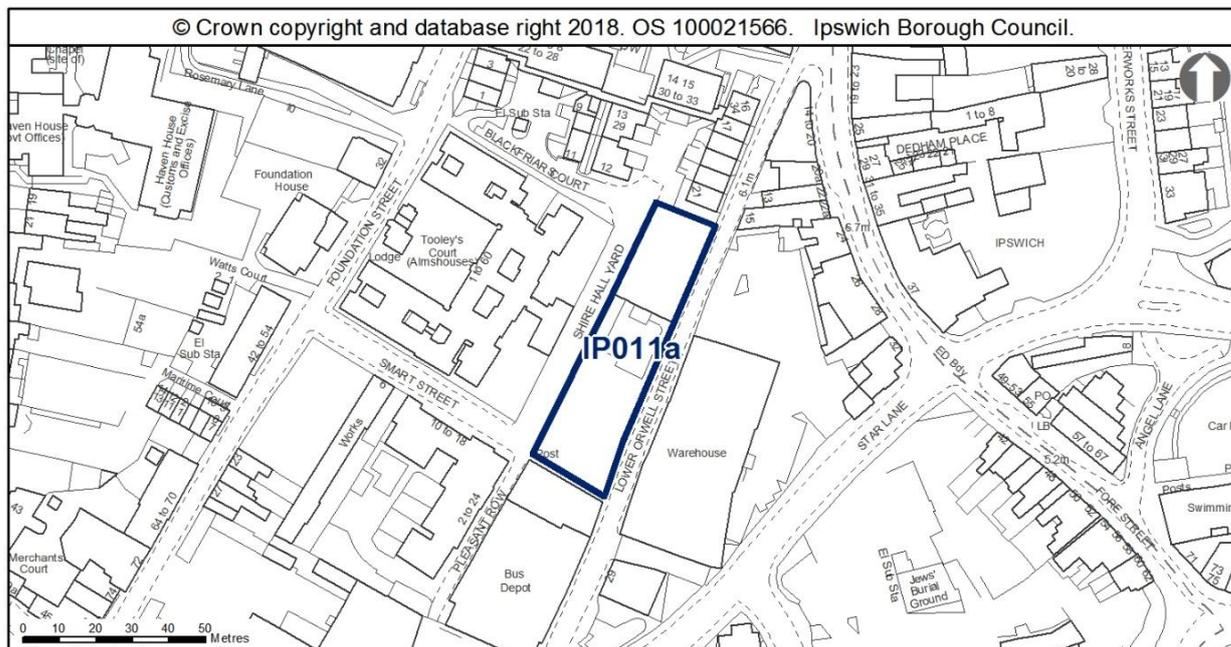
It is envisaged that residential development is likely to come forward on the eastern and western sides of the site, because there has been no indication from the users in the centre that they may relocate.

The residential development of this allocation site should respect the established grid layout of the Rosehill area, and follow the perimeter block form with active frontages facing the streets, an established characteristic of the area, as identified in the California Urban Characterisation Study SPD. Existing dwellinghouses in California are principally red brick terraces and pairs of semi-detached houses, with often a prevailing architectural feature which characterises a particular road or area, such as the position of the front door, the pattern of fenestration, the use of bay windows, which has led to some distinctive areas of development.

This varied approach to employing architectural details to create pockets of distinguishable housing should be incorporated into the development of the allocation site to ensure the design of the new development is high quality and distinctive.

Parking should be incorporated into the design proposals to encourage the public realm to contribute positively to the character and experience of the development at the allocation site.

**Site ref: IP011a (UC011) Lower Orwell Street former Gym & Trim
(formerly Smart Street/ Foundation Street)
Site area: 0.15 ha**



Allocation Policy SP2

| Use(s) | | Indicative capacity* |
|---------|-------------|-----------------------------|
| Primary | Residential | 18 (110dph on 100% of site) |
| | | |

* see Core Strategy policy DM23 for minimum and average densities

Adopted Plan 2017

100% housing at high density – 14 dwellings

Current use

Disused gym building and car park

Development constraints / issues

It is close to an Air Quality Management Area (Star Lane) and just outside the Flood Zone and contains trees protected through a TPO (an application for tree works may be needed).

The site is adjacent to the Central Conservation Area, close to the grade II listed Tooley's Court and Smart's Almshouses, contains a scheduled monument and lies within an area of archaeological importance. This allocation is located within the

setting of several listed buildings and locally listed buildings, including the remains of the Blackfriars Priory, numerous historic buildings along Fore Street and the former Smart Street School. The site is therefore highly sensitive with regard to the possible impacts on the historic environment.

Development principles for the Merchant Quarter, within which this site is located, are set out in Chapter 6 of the Site Allocations and Policies plan (see 'Opportunity Area B').

This site affects an area of archaeological importance within the area of the Anglo-Saxon and medieval town. The site lies over the line of the Anglo-Saxon and medieval town defences, and the 14th century Friary wall. Much of the site is a scheduled monument (List entry no: 1005985). Scheduled Monument Consent (SMC) is a legal requirement for any development which might affect a monument either above or below ground level. Historic England administers the SMC application process on behalf of the Secretary of State for Digital, Culture, Media and Sport and should be consulted at the earliest opportunity to discuss the nature of the development. SMC is a separate process from the planning system.

There is a high potential for archaeological remains of national significance and detailed early pre-application discussions with Suffolk County Council Archaeological Service and Historic England would be required. Archaeology may be a major consideration for project costs and timescales. Proposals would need to be supported by programmes of pre-determination archaeological works which may include desk-based assessments, survey works and archaeological evaluation. Complex archaeological mitigation is likely to be required which could include watching briefs, full excavation and / or design scheme changes to allow for preservation in situ. Design questions would relate to the surviving defences and structures in particular, which were largely left in situ in earlier excavations. Post-excavation analysis, assessment and reporting would also be necessary.

Planning permission was granted in 2007 for the erection of 3-4 storey buildings to provide 40 no. residential units on this site. This permission has lapsed, however refreshed policy and guidance with regard to the protection and enhancement of the historic environment has been issued since the 2007 application was approved, including the NPPF, Ipswich Local List (Buildings of Townscape Interest) SPD adopted 2013, Ipswich Development and Archaeology Supplementary Planning Document SPD adopted 2018, Historic England's The Setting of Heritage Assets: Historic Environment Good Practice Advice in Planning Note 3, and Conservation Principles Policies and Guidance.

Therefore, the design, scale, massing, appearance and materials of the proposed building would need careful consideration to ensure that a building on this site does not harm the significance of nearby listed buildings, locally listed buildings, or the

Conservation Area, through inappropriate development within the setting of these heritage assets.

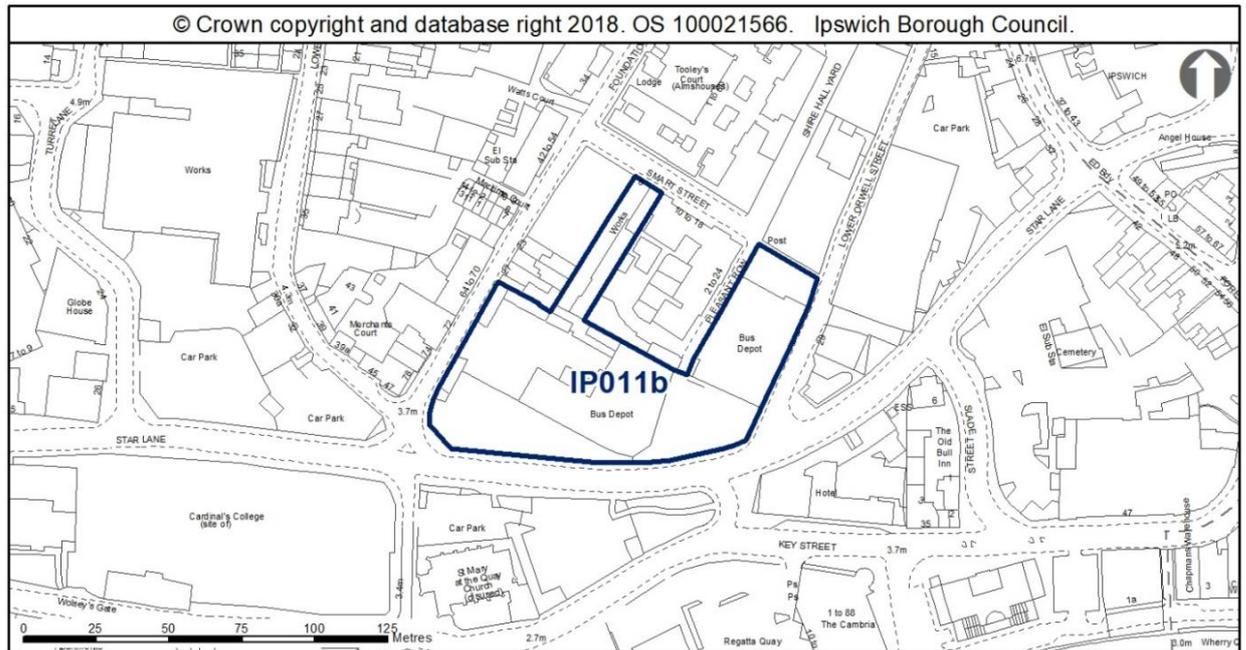
The design should have regard for the sensitive historic setting of the application site, whilst taking a contemporary design approach to make a positive design statement which respects the transition between waterfront and town centre.

The site is expected to require improvements to the existing water supply and foul sewerage networks to enable development.

To achieve biodiversity net gain, the recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate.

Site ref: IP011b (UC011) Smart Street / Foundation Street (South)

Site area: 0.62ha



Allocation Policy SP2

| Use(s) | Indicative capacity | |
|---------|---------------------|------------|
| Primary | Residential | 56 (90dph) |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Plan 2017

80% housing at high density – 50 dwellings – on a site which included IP011c to the north.

Current use

First bus depot, and employment works.

Development constraints / issues

Bus depot needs to be relocated before development could commence.

Air quality, flood risk, possible contamination, possible access constraints, TPO on site or nearby (an application for Tree Works may be needed). The site lies between the Central and Wet Dock Conservation Areas, close to the grade II* St Mary at Quay church, contains two scheduled monuments and lies within an area of archaeological importance.

Development principles for the Merchant Quarter, within which the site is located, are set out in Chapter 6 of the Site Allocations and Policies plan (see 'Opportunity Area B'). The site provides important linkage for the redevelopment of Merchant Quarter and would provide improved frontage along Star Lane, which in turn would provide visual enhancement to the Central and Wet Dock Conservation Areas. Where possible, the site layout should allow for improvements to the Star Lane frontage such as footway and cycleway provision or widening, and tree planting.

This site, within the Anglo-Saxon core and the Area of Archaeological Importance (IPS 413), includes two separate scheduled monuments relating to the Middle and Late Saxon town (IPS 211 and 213 List Entry numbers 1005986 and 1005985). Scheduled Monument Consent (SMC) is a legal requirement for any development which might affect a monument either above or below ground level. Historic England administers the SMC application process on behalf of the Secretary of State for Digital, Culture, Media and Sport and should be consulted at the earliest opportunity to discuss the nature of the development. SMC is a separate process from the planning system.

There is also a potential for nationally important archaeological remains outside of the scheduled areas. Detailed early pre-application discussions with Suffolk County Council Archaeological Service and Historic England would be required to agree the scope of required assessment, the principle of development and to inform design (e.g. to allow for preservation in-situ of deposits or appropriate programmes of work). Where development is accepted in principle archaeological remains will be complex, important and mitigation could involve significant costs and timescales.

A transport assessment and travel plan will be required.

Surface water flooding local to site - will need to be considered at planning application stage. See Appendix 1 of the Ipswich SFRA.

Development principles must have regard for the sensitive historic setting of this allocation site, and ensure proposals preserve or enhance the settings of nearby designated heritage assets, including the grade II* St Mary at the Quay Church, Tooley's Almshouses and Central and Wet Dock Conservation Areas.

Reinstating a building line along Star Lane which turns the corner of Foundation Street and Lower Orwell Street would be welcome, with the opportunity for development on this site to have a landmark quality to mark the transition between the waterfront and the town centre. The design of the development would be expected to be of high quality to make a positive architectural statement.

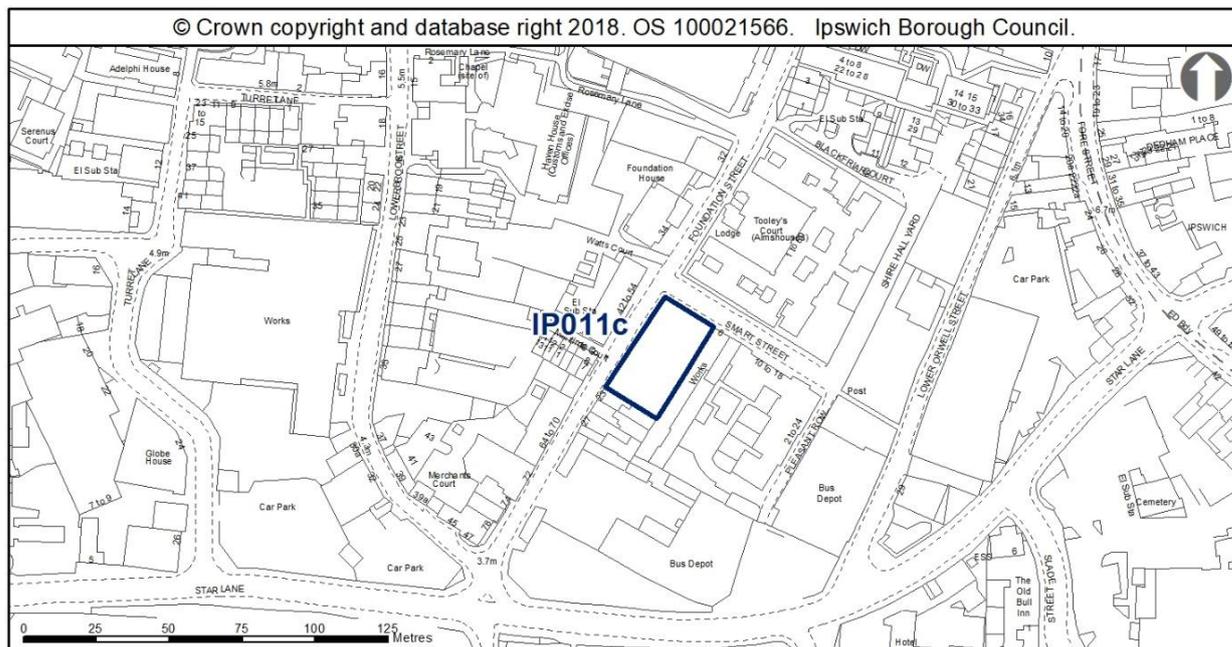
The allocation site wraps around the former Smart Street School, which is included on the Local List (Buildings of Townscape Interest) SPD. Development of the allocation site should use a varied approach to massing and layout to avoid enclosing

this locally listed building. Pleasant Row is a particularly narrow lane, the redevelopment of this allocation site should look to move the building line away from the edge of Pleasant Row to allow the former school more breathing space to improve the surroundings in which the building is experienced.

Although this site is currently of low wildlife value, the brick and pantile building could support bats and consequently further surveys are recommended. To achieve biodiversity net gain, the recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means for biodiversity enhancement are appropriate.

Site ref: IP011c Smart Street / Foundation Street (North)

Site area: 0.08ha



Allocation Policy SP2

| Use(s) | | Indicative capacity |
|---------|-------------|---------------------|
| Primary | Residential | 7 (90dph) |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Plan 2017

Previously part of IP011b – Sites have been subdivided to reflect ownerships.

Current use

Car park

Development constraints / issues

Air quality, flood risk, possible contamination, possible access constraints, TPO on site or nearby (an application for Tree Works may be needed). The site lies adjacent to the Central and Wet Dock Conservation Areas, close to the grade II* St Mary at Quay church, contains two scheduled monuments and lies within an area of archaeological importance.

Development principles for the Merchant Quarter, within which the site is located, are set out in Chapter 6 of the Site Allocations and Policies plan (see ‘Opportunity Area B’). Where possible, the site layout should allow for improvements to the Star

Lane frontage such as footway and cycleway provision or widening, and tree planting.

This site lies within the Anglo-Saxon core and the Area of Archaeological Importance (IPS 413). The adjacent IP011 sites include two separate scheduled monuments relating to the Middle and Late Saxon town (IPS 211 and 213 List Entry numbers 1005986 and 1005985). The allocation was de-scheduled as a monument in 2016 as a large portion of it has been excavated (IPS 212). The excavated area can be considered archaeologically sterile, although it gives an indication of what might survive on the rest of the site.

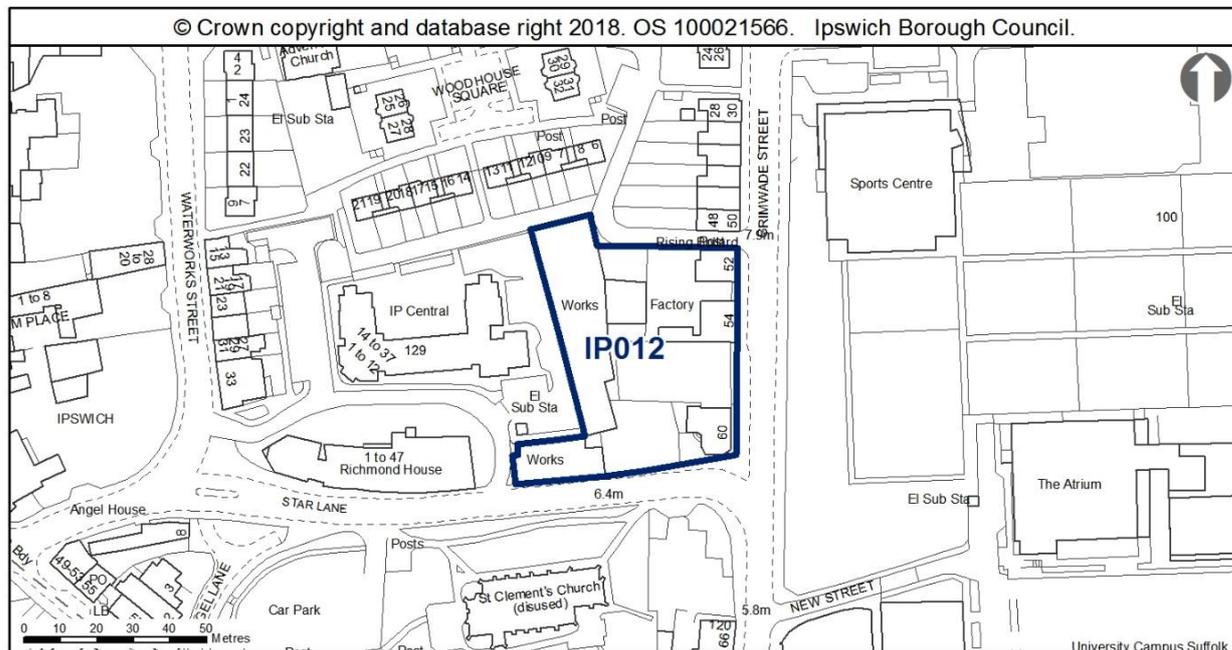
There is also limited potential for nationally important archaeological remains outside of the scheduled areas. This is because much of the site has already been excavated in the past. Detailed early pre-application discussions with Suffolk County Council Archaeological Service and Historic England would be required to agree the scope of required assessment, the principle of development and to inform design.

Surface water flooding local to site - will need to be considered at planning application stage. See Appendix 1 of the Ipswich SFRA.

Development of this site must have regard for the highly sensitive setting of this allocation site, with particular regard to the setting of the Central Conservation Area, Smart Street School and Tooley's Almshouses. Development proposals should be of a high quality design, respecting the existing 2 storey scale of development along Foundation Street. The scheme should address both Foundation Street and Smart Street with active, engaging frontages. A contemporary design approach in this location would be welcomed, but should ensure that the design of the proposal has regard for the sensitive historic setting of the site, using the adjacent architectural features (such as varied approaches to textures and bonding of brickwork) to influence the appearance of the allocation site.

Site ref: IP012 (UC012) Peter's Ice Cream, Grimwade Street

Site area: 0.32ha



Allocation Policy SP2

| Use(s) | Indicative capacity |
|-------------|---------------------|
| Residential | 35 (110dph*) |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Plan 2017

As above

Current use

Former ice cream factory and engineering works.

Development constraints / issues

Planning permission granted for student accommodation subject to a planning agreement (08/00978), however it is considered unlikely to come forward for this use.

Possible access constraints, close to an Air Quality Management Area and there is possible contamination.

This site is within the Anglo-Saxon and medieval core and Area of Archaeological Importance (IPS413). Necessary measures for archaeology should be addressed at an appropriate stage in the planning process. Detailed early pre-application

discussions with Suffolk County Council Archaeological Service would be required to agree the scope of required assessment and to inform design (e.g. to allow for preservation in-situ of deposits or appropriate programmes of work). Archaeological remains may be complex and important and mitigation could involve significant costs and timescales.

The site is adjacent to the Central Conservation Area and the grade II* listed St Clement's Church.

Surface water flooding local to site - will need to be considered at planning application stage. See Appendix 1 of the Ipswich SFRA.

Planning permission was granted in 2008 for the erection of a 5-6 storey building to provide in excess of 500 units of student accommodation.

The 2008 scheme was a particularly large scheme given the sensitive historic setting of the site. This permission has lapsed, however refreshed policy and guidance with regard to the protection and enhancement of the historic environment has been issued since the 2007 application was approved, including the 2018 NPPF, Ipswich Local List (Buildings of Townscape Interest) SPD adopted 2013, Ipswich Development and Archaeology Supplementary Planning Document SPD adopted 2018, Historic England's The Setting of Heritage Assets: Historic Environment Good Practice Advice in Planning Note 3, and Conservation Principles Policies and Guidance.

Development of this site should ensure that the design of the proposals address both Star Lane and Grimwade Street, and should look to reinstate an active street scene. The development of this site will inevitably read as the backdrop to the grade II* St Clements Church and thus requires a high quality, bespoke approach to design.

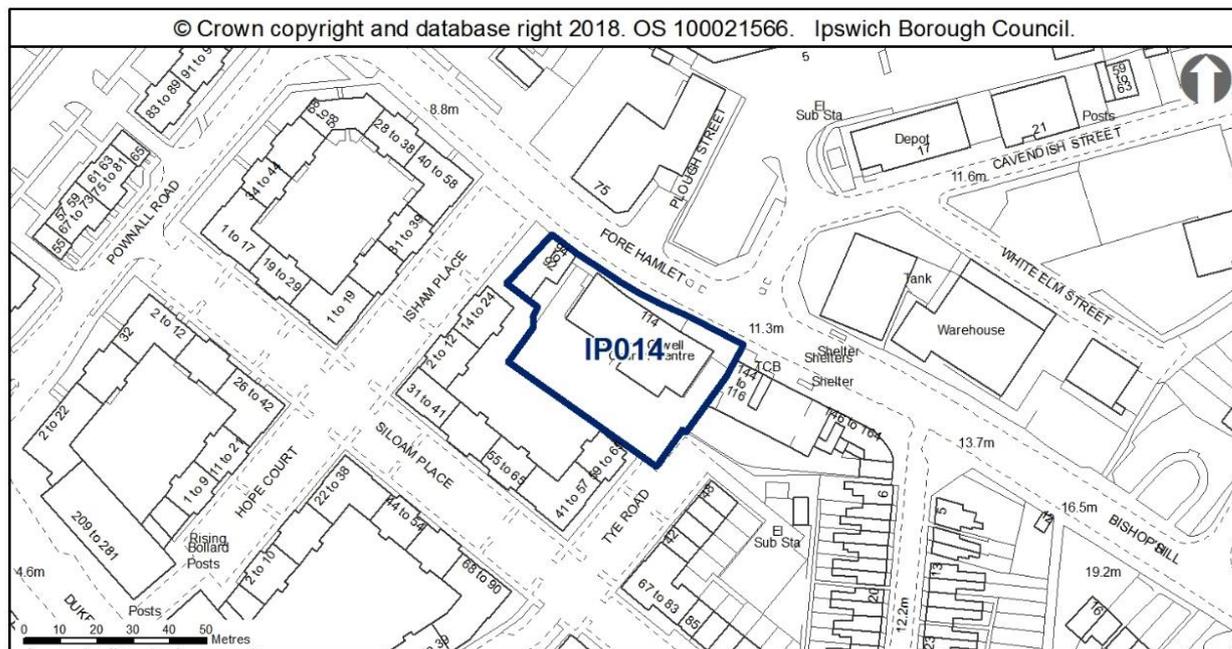
The reuse of the early 20th century brick buildings along Star Lane would be encouraged, as these buildings have group value with Richmond House from the former industrial use of the site as a lawn mower factory at the beginning of the 20th century, and contribute to the setting of this more industrial part of the Conservation Area around the waterfront.

Opportunities should also be taken to improve the public realm around the allocation site, such as widening footpaths along Star Lane, introducing planting and street furniture to Grimwade Street to improve the experience of the area for pedestrians, and create a high quality public realm which would complement the new development at the site.

Although this site is currently of low wildlife value some of the buildings in the could support bats and consequently further surveys are recommended. To achieve biodiversity net gain, the recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate.

Site ref: IP014 Hope Church, Fore Hamlet

Site area: 0.21ha



Allocation Policy SP2

| Use(s) | Indicative capacity |
|-------------|---------------------|
| Residential | 23 (110dph*) |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Plan 2017

N/A – new site but was previously considered for 100% housing through IP-One Area Action Plan Preferred Options 2007.

Current use

Church with planning permission (18/00316/FUL) to relocate.

Development constraints / issues

Surface water-flooding local to site - will need to be considered at planning application stage. See Appendix 1 of the Ipswich SFRA.

Potential contaminated land. Therefore an assessment would be required.

Within Air Quality Management Area.

This allocation site is occupied by a mid-20th century two storey flat roofed warehouse building located on Fore Hamlet, used as a church, and a 19th century painted brick dwellinghouse, now in commercial use. The site backs onto a modern

residential development of three and four storey blocks orientated around landscaped courtyards.

In the 1800s, Fore Hamlet was characterised by residential development, with rows of terraces occupying much of the area between Fore Hamlet and Duke Street, with land gradually being cleared throughout the 20th century to provide expanses of land for industrial activities. The pattern of the fenestration and quoining indicates that the building was formerly two residential units, with the eastern unit being the end of the terrace, and the blank gable of the western elevation suggesting that the building formed part of a longer row of terraces to the west. As this is the last remaining relic of 19th century residential expansion of the town along Fore Hamlet, it is recommended that the retention of the 19th century dwellinghouse should be incorporated into the design proposals for this allocation site. Reinstatement of sash windows and removal of the modern shop front at ground floor would be welcome in this regard.

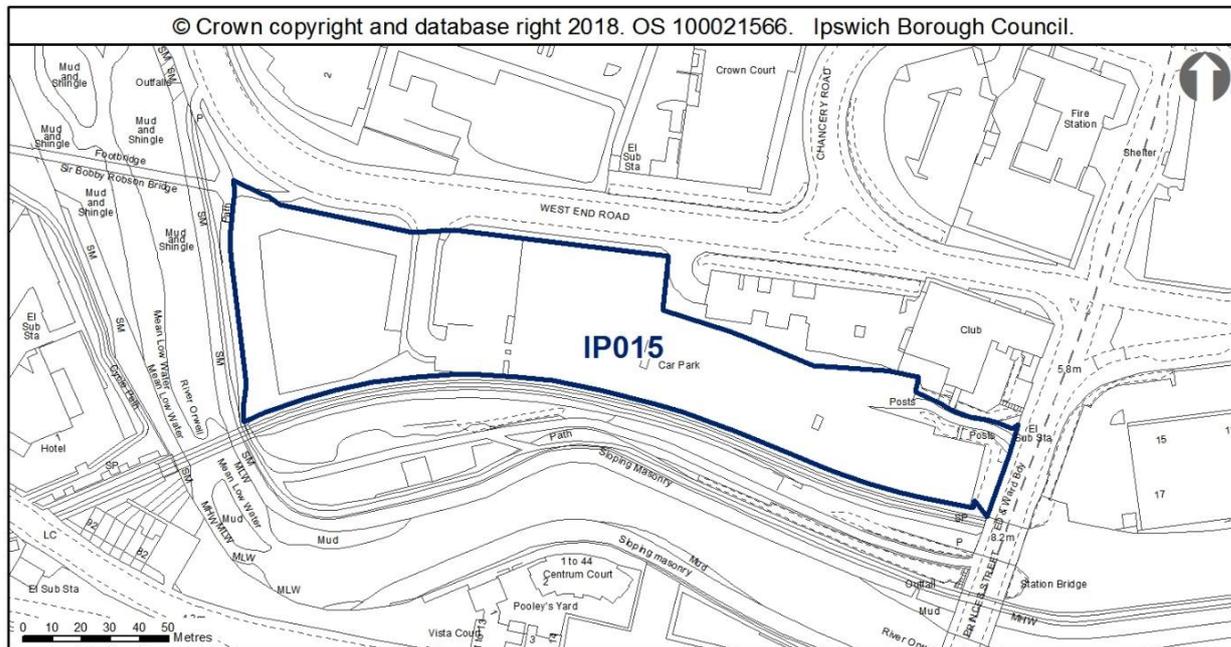
The development of the remainder of this allocation site should look to take architectural cues from the adjacent development to the south, taking a modern, contemporary design approach. The scale and massing of the proposed development should address the change in topography as Fore Hamlet transitions into Bishop Hill, and step down from the adjacent 5 storey building at 116-144 Fore Hamlet.

The layout of the scheme should mimic the existing development to 'complete' the courtyard block of Isham Place and Siloam Place and include details for the soft landscaping of the courtyard.

Although this site is currently of low wildlife value the buildings in the site could support bats and consequently further surveys are recommended. To achieve biodiversity net gain, the recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate.

Site ref: IP015 (UC015) West End Road Surface Car Park

Site area: 1.22ha



Allocation Policies SP2 & SP17

| Use(s) | | Indicative capacity |
|-----------|--------------------|--------------------------------|
| Primary | Long stay car park | Multi storey car park |
| Secondary | Residential | 67 (100dph on 55% of the site) |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Plan 2017

20% Housing at high density – 22 dwellings

Long stay car park

Offices – 1000sqm

Current use

Surface level long stay car parking.

Development constraints / issues

Existing car parking needs to be replaced on site and potentially accommodate that from Portman Road (IP051) also. The site should be master planned with the adjacent site to the south, fronting the river (IP083). Flood risk, possible contamination and adjacent to a listed building. Development principles for the

River Corridor and Princes Street Corridor, within which the site is located, are set out in Chapter 6 of the Site Allocations and Policies plan (see 'Opportunity Area F').

In terms of archaeology, depending on the nature of any ground works, a condition may be attached to any grant of permission relating to archaeological investigation.

The site is close to a County Wildlife Site (the River). Prior to any vegetation clearance, a reptile survey should be undertaken in the western section of the site, with particular attention paid to the vegetated banks. Footpath 61 (River Path) is recorded along the western edge of the site.

Development must take account of the River Corridor Buffer (10m) where no development should take place.

This allocation site is in a strategic position as a gateway site to people entering the town from the south, and in particular arriving from the train station. The development of this site therefore needs to be to a high quality design and make a positive architectural statement adjacent to an important entrance to the town.

The north east corner adjacent to the site is the grade II listed Princes Street Maltings, now being converted into office use. The Maltings is a prominent red brick 19th century building with a long two storey range, decorative Dutch gable to Princes Street, with the tall pyramidal slate roof of the kiln projecting proudly above the main range. The building is thus highly visible in the street scape, with key views of the building being along Burrell Road, Princes Street bridge, Commercial Road, West End Road and indeed further afield owing to the prominent nature of the kiln.

This allocation site is currently a surface car park which allows for long ranging views of the grade II listed maltings across the site, although is of low aesthetic quality, and the redevelopment of this site is welcome. However given the existing undeveloped character of the allocation site, the introduction of development to this car park does have the potential to harm the significance of The Maltings through the introduction of development within the setting of the listed building. The design, massing, scale and layout of the proposed development therefore requires particularly careful consideration to respect the sensitive historic context of the allocation site.

The nature of the scale, massing, design and appearance a multi-storey car park is a key consideration regarding this allocation, with multi storey car parks typically being a single volume building, with the potential to be a particularly tall building. Owing to the massing of multi storey car parks, this building thus has the potential to be extremely prominent, and depending on its proximity to the listed building and position in key views, the car park therefore could be harmful to the significance on the grade II listed maltings, which the Council has a legal duty to protect. This

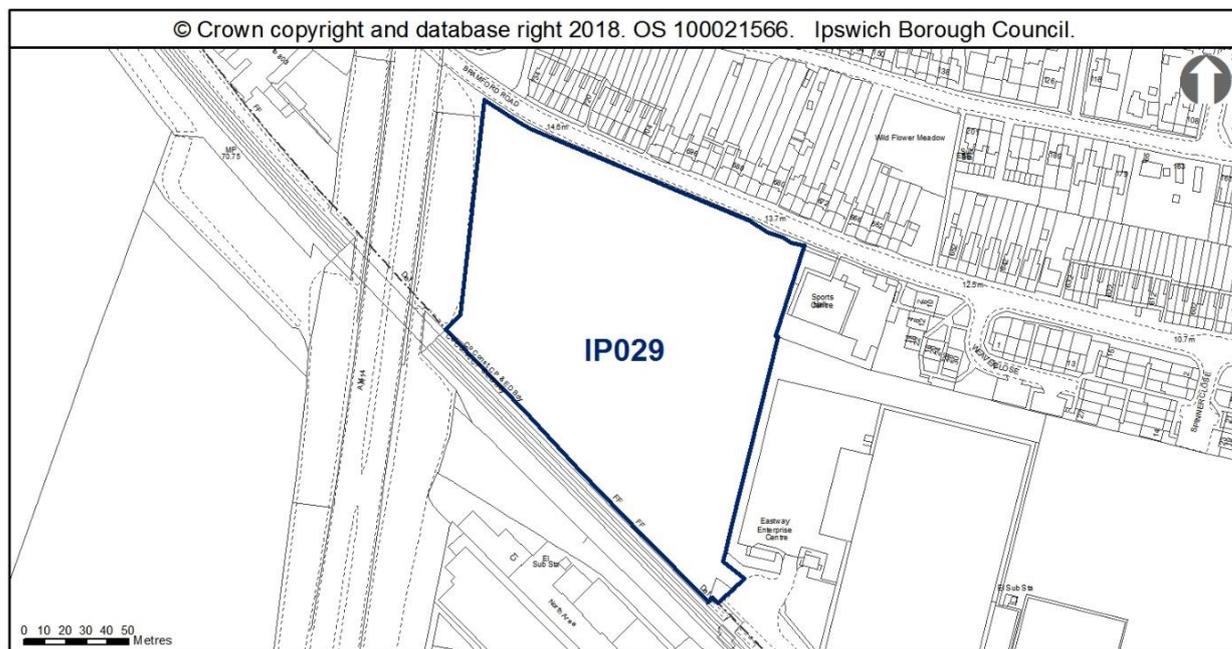
highlights the importance of preserving this key heritage asset when designing proposals for a multi-storey car park.

The secondary land use allocation for this site is to provide 43 residential units. Residential as a development in this location has far greater flexibility in the design, scale and massing than with the single volume multi-storey car park. The layout of the residential units could be laid out in a way to offer key views of The Maltings, with the more human scale of domestic architecture contrasting against the dominating height of the kiln. A well designed residential development in this area therefore has the potential to enhance the significance of The Maltings to provide positive contrast in scale to reveal the prominence of the kiln.

The design of the residential accommodation should have active and engaging frontages to West End Road and views across the river, whilst also create a positive architectural statement which will act as a landmark development signalling the entrance to Ipswich. A contemporary design approach would be encouraged, continuing the varied architectural expressions and use of modern materials around the Portman Quarter and Sir Bobby Robson Bridge to positively contrast the industrial architecture of The Maltings.

Site ref: IP029 (UC030) Land Opposite 674-734 Bramford Road

Site area: 2.26ha



Allocation Policies SP5, SP6 & SP9

| Use(s) | | Indicative capacity |
|-----------|--------------------------------------|-------------------------|
| Primary | Employment | 45% of site (Circa 1ha) |
| Secondary | Amenity green space (on 55% of site) | 1.26ha |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Plan 2017

70% Housing at medium density – 71 dwellings

Current use

Vacant land used informally for recreation. Part recently used in connection with rail chord works.

Development constraints / issues

Loss of informal open space, although it was not identified as recreational open space in the Open Space, Sport and Recreation Study 2009 or the 1997 Ipswich Local Plan. Access constraints, possible contamination, noise from the A14 and railway.

The site has potential wildlife interest Design and layout would need to support the wildlife corridor function of the railway and A14. This site is of at least a medium

biodiversity value and detailed surveys could reveal that it has higher ecological significance. Prior to any vegetation clearance, further surveys should continue/be undertaken to assess the wildlife interest, particularly botanical, reptiles, bats, badgers and breeding birds and mitigation implemented as appropriate. Consideration should also be given to the likely impact of vegetation clearance upon the local hedgehog population. New development should retain as much of the existing habitat as possible and integrate it within a landscaping scheme, in particular the hedgerows along the boundaries. The recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate.

The site offers the potential to provide a link road between Bramford Road and Europa Way. The impacts of such a link are currently being investigated by the Highway Authority. Should the site come forward for development in advance of the outcome being known, the layout should not prejudice the provision of the road. Bridleway 12 is recorded along the site's western edge. Bridleway links are required at the route's northern end to a) connect to the urban footpath leading to Morgan Drive; and b) eastwards to link to Bramford Lane.

Potential Noise from the railway and road network therefore design and layout will need to address this.

In terms of archaeology, this site lies in the vicinity of Roman (IPS 242, IPS 233) and Prehistoric (IPS 018) sites. No objection in principle to development but any permission will require a condition relating to archaeological investigation.

This site falls within the 91.4m height consultation zones surrounding Wattisham airfield, therefore any proposed structures which may exceed this height would need to be reviewed by the Ministry of Defence.

A transport assessment and travel plan will be required.

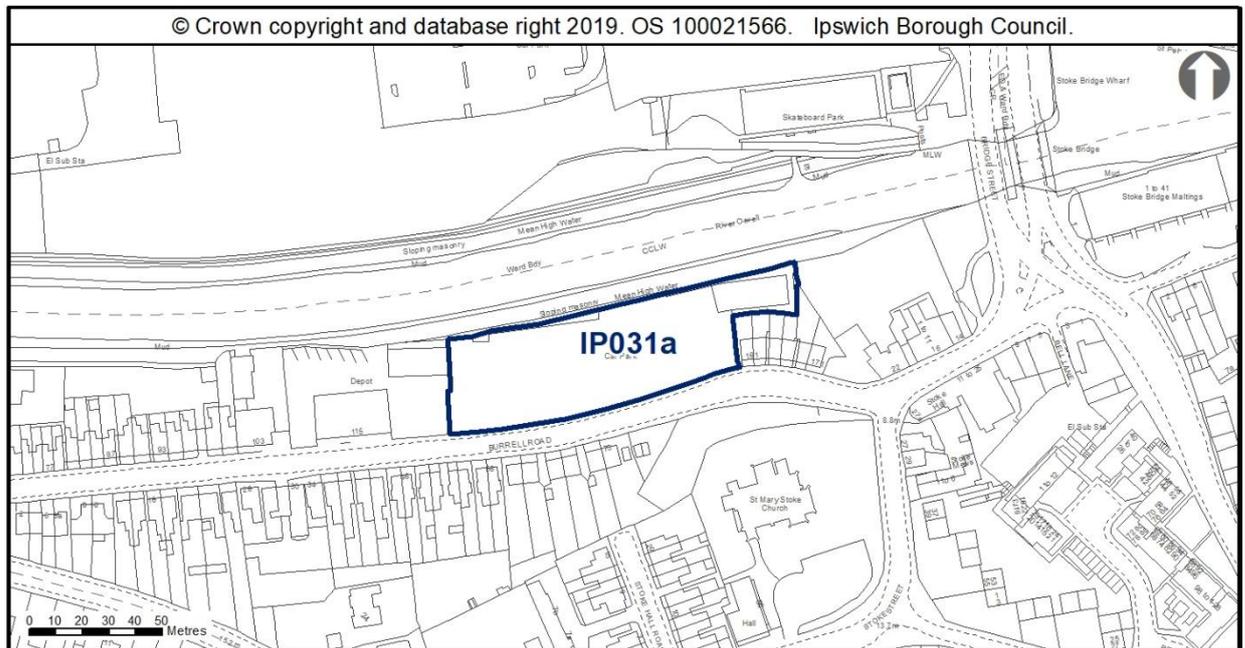
Opposite the allocation site are rows of interwar terraced housing which feature mansard roofs, with shared dormers and chimneys which penetrate the elongated roof slopes. To the east of the site is a hall used as a place of worship. The hall is a fairly utilitarian structure, however features a prominent, steeply pitched entrance porch which contrasts the shallow pitch of the main range.

A contemporary approach to design utilising distinctive roofs forms would be encouraged, with development proposals taking design cues from adjacent architecture, particularly with regard to the unusual and contrasting roof forms which characterise this western end of Bramford Road.

The layout of the proposal should reflect the existing grain of development of linear streets and grids of housing, and should address the retained amenity space as well as Bramford Road.

Soft landscaping and screening could be installed along the southern and eastern boundary to screen this industrial development and enhance the amenity and biodiversity value of the retained open space.

Site ref: IP031a (UC032) Car Park, Burrell Road
Site area: 0.44ha



Allocation Policy SP2

| Use(s) | Indicative capacity |
|-------------|---------------------|
| Residential | 20 (45dph*) |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Plan 2017

100% housing at medium density – 20 dwellings – on a reduced site.

Current use

Car park, rough ground, roadside landscaping.

Development constraints / issues

In a flood zone, close to an AQMA and possible contamination (former petrol station and car workshop). Development would need to support the wildlife corridor function of the river which is a County Wildlife Site at this point.

This site is within the Area of Archaeological Importance (IPS 413). Desk-based Assessment and consideration of geotechnical data would be advised in the first instance. Depending on proposed groundworks, conditions on consent informing these assessments may be appropriate, to inform archaeological investigation and mitigation, including palaeo-environmental investigation. This site lies along the

bank of the Orwell, adjacent to the Stoke Conservation Area and the grade I listed Church of St Mary at Stoke.

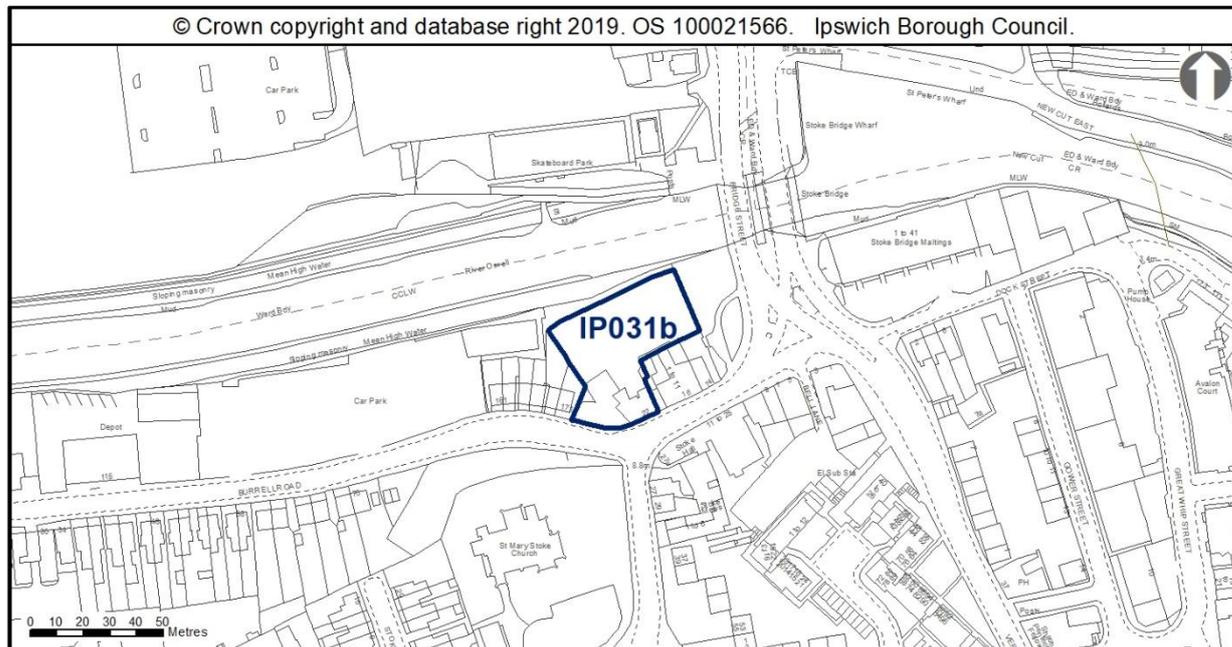
Stoke Conservation Area covers the historic core of Stoke from when the area was a modest hamlet before the widespread expansion of the area in the 19th century. Development of this allocation site should therefore have regard to the domestic scale of existing architecture within the Stoke Conservation Area, and should take its architectural influences in terms of height, massing and design from the hamlet of Stoke, rather than seeking to continue the more industrial scale of development found to the north east along the waterfront.

Development must take account of the River Corridor Buffer (10m) where no development should take place.

Development proposals need to address the change in topography to the north of the site as land drops away towards the river to avoid the need to build up site levels which would increase the prominence of buildings on the site, and in turn, affect views into and out of the Conservation Area.

Site ref: IP031b (UC032) 22 Stoke Street

Site area: 0.18ha



Allocation Policy SP2

| Use(s) | Indicative capacity |
|-------------|--|
| Residential | 18 (100dph(reflective of highly accessible location – not higher because of heritage constraints.) |

DM23Adopted Plan 2017

100% housing at medium density – 20 dwellings – on a reduced site.

Current use

Car park, rough ground, roadside landscaping.

Development constraints / issues

In a flood zone, close to an AQMA and possible contamination (former petrol station and car workshop). Development would need to support the wildlife corridor function of the river which is a County Wildlife Site at this point. A reptile survey should be undertaken and mitigation for this group undertaken as required. This site is located adjacent to the River Orwell wildlife corridor. Any lighting scheme should be designed to prevent light spillage into this area, or the scrub habitat along the river banks. The recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate.

This site is within the Area of Archaeological Importance (IPS 413). Desk-based Assessment and consideration of geotechnical data would be advised in the first instance. Depending on proposed groundworks, conditions on consent informing these assessments may be appropriate, to inform archaeological investigation and mitigation, including palaeo-environmental investigation. This site lies along the bank of the Orwell, adjacent to the Stoke Conservation Area and the grade I listed Church of St Mary at Stoke.

Stoke Conservation Area covers the historic core of Stoke from when the area was a modest hamlet before the widespread expansion of the area in the 19th century. Development of this allocation site should therefore have regard to the domestic scale of existing architecture within the Stoke Conservation Area, and should take its architectural influences in terms of height, massing and design from the hamlet of Stoke, rather than seeking to continue the more industrial scale of development found to the north east along the waterfront.

The allocation site would be prominently positioned adjacent to Stoke Bridge, and would thus be visible in views into and out of the Stoke Conservation Area, and also views out of the Central and Wet Dock Conservation Areas and thus in a highly visible location.

The design of a development scheme for this site should therefore have active frontages which address the river to the north, and both Bridge Street and Burrell Road to ensure that new development engages with existing architecture and enhances the street scene when viewed from the many vantage points from where the site can be seen.

The very eastern section of the allocation site is presently open space, which provides pleasant views of the prominent Stoke Hall when approaching Stoke Conservation Area from Stoke Bridge. Stoke Hall is a particularly decorative building, and so development across the existing open space should be carried out sensitively to prevent obscuring views into the Stoke Conservation Area, particularly with regard to Stoke Hall.

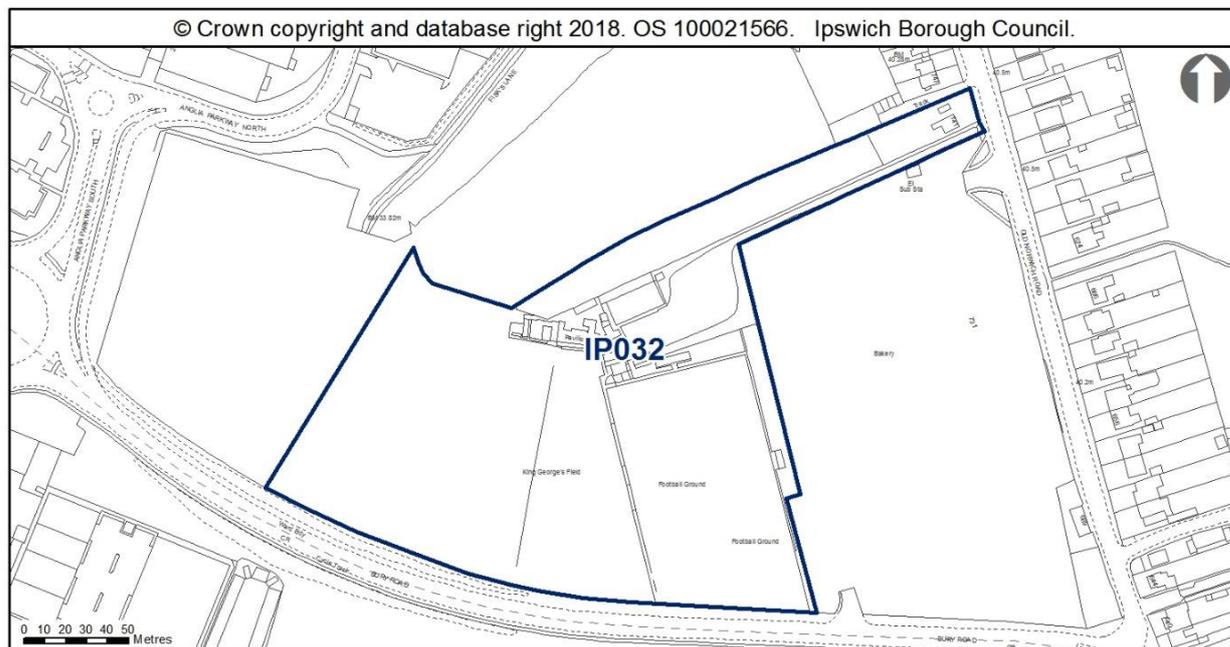
Development proposals need to address the change in topography to the north of the site as land drops away towards the river, to avoid the need to build up site levels which would increase the prominence of buildings on the site, and in turn, affect views into and out of the Conservation Area.

Particular thought needs to be given to the development of the eastern parcel of development which wraps around existing 19th century buildings along Burrell Road. A bespoke design approach is required to prevent the new development from reading as awkward backland development, and needs to integrate well with existing buildings on Burrell Road.

Development must take account of the River Corridor Buffer (10m) where no development should take place.

Site ref: IP032 (UC033) King George V Field, Old Norwich Road

Site area: 3.7ha



Allocation Policies SP2 & SP6

| Use(s) | | Indicative capacity |
|-----------|--------------------------------------|----------------------------|
| Primary | Residential | 99 (35dph on 80% of site*) |
| Secondary | Amenity green space (on 20% of site) | 0.71ha |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Plan 2017

As above

Current use

Playing fields

Development constraints / issues

Need prior provision of a replacement pitch and ancillary facilities such as changing rooms and spectator accommodation of equivalent or better quality and quantity in the locality and subject to equivalent or better accessibility and management arrangements. Currently there is planning permission in place for replacement pitches and changing facilities to be provided within Mid Suffolk District (reference 0254/13) north of Whitton Sports Centre. However more (80%) of the site can now

be redeveloped than previously thought (50%) because the open space to the north of the site is being retained.

Possible contamination. Trees on southern boundary protected by a TPO. Footpath 32 is recorded to the west of, but outside, the site. The development should support the diversion of FP32 off the football pitches and fund an upgrade of the route to bridleway or restricted bridleway status to provide for cycling connectivity to Fisk's Lane (Restricted Byway 75).

In terms of archaeology, the site lies on high ground above the Gipping Valley. It has been subject to geophysical survey, and a desk based assessment has been carried out for both this site and the adjacent site IP005. There is potential for remains of multiple periods on the site and trenched evaluation will be required. There is no objection in principle to development but any permission will require a condition relating to archaeological investigation.

The site is close to the Whitton Conservation Area. The Core Strategy and the published development brief for this site and the adjacent IP005 Tooks Bakery require the Conservation Area to be taken into account. Any cumulative impacts on the conservation area with the development of adjacent site IP005 and site IP140 will need to be taken into account.

In respect of water supply and waste water treatment, infrastructure and / or treatment upgrades will be required to serve the proposed growth, or diversion of assets may be required.

The Council has published a development brief for this site and the adjacent former Tooks bakery site (reference IP005).

Whitton includes numerous listed buildings along Old Norwich Road, and also benefits from Conservation Area designation. To the north of Whitton is the borough boundary, as fields open up to the neighbouring authority of rural Mid Suffolk.

Planning permission has recently been granted at the adjacent Tooks Bakery Site IP0005, which includes an access to this allocation site through the proposed residential development.

The design of the residential development at the allocation site should respond to the architectural approach taken at the Tooks Bakery allocation site IP0005, producing contemporary units utilising textured brick bonds and asymmetric use of cladding to add visual interest and contrast to the scheme.

Whilst acoustic mitigation measures may be required along Bury Road, a continuous timber fence or boundary wall has the potential to deaden the street scene, and would prevent the development site making a positive architectural statement when

first entering the borough. This boundary should therefore be well detailed, perhaps utilising a textured brick bond to integrate with the new development, and include soft landscaping at the periphery of the site.

This site falls within the 91.4m height consultation zones surrounding Wattisham airfield, therefore any proposed structures which may exceed this height would need to be reviewed by the Ministry of Defence.

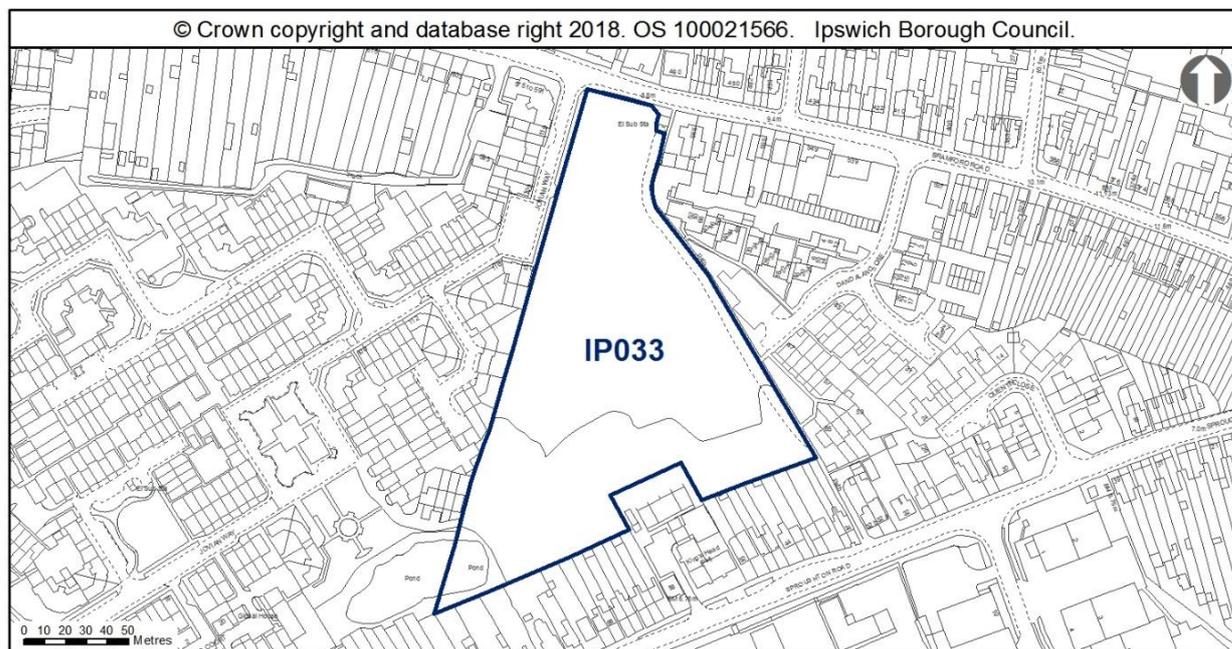
A transport assessment and travel plan will be required. The traffic impact of access from Bury Road will need to be considered. The Old Norwich Road junction has received Section 106 money via a recent appeal to fund a mitigation scheme. Further contributions may be required to mitigate the impact from this site.

Surface water flooding local to site - will need to be considered at planning application stage. See Appendix 1 of the Ipswich SFRA.

Due to the presence of rough grassland around the margins a reptile survey should be carried out prior to any removal of vegetation. New development should retain as much of the higher value existing habitat as possible, for example the hedgerows, and integrate it within a landscaping scheme, to deliver locally accessible natural greenspace. The recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate.

Site ref: IP033 (UC034) Land at Bramford Road (Stocks site)

Site area: 2.03ha



Allocation Policies SP2 & SP6

| Use(s) | | Indicative capacity |
|---------|--------------------------------------|----------------------------|
| Primary | Residential | 55 (55dph on 50% of site*) |
| | Amenity green space (on 50% of site) | 1.02ha |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Local Plan 2017

As above.

Current use

Open land (fenced).

Development constraints / issues

Possible access constraints, possible contamination (former landfill) to the south of the site which should form the public open space area with the housing element forming the northern area, local wildlife site. There are substantial changes in level which will need to be addressed and the design should ensure that there are links from the site to the existing footpath links bounding the site. An ecological (including breeding birds, great crested newt, bats and badgers) and reptile survey will be needed prior to any vegetation clearance and mitigation where appropriate.

Development should retain a thick, scrubby buffer around the pond. To achieve biodiversity net gain, the recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate.

In terms of archaeology, there were gravel pits across part of the site. Bronze Age and Neolithic finds were recovered (IP018), and Saxon remains were recorded to the south (IPS 499). Evaluation is needed to identify the impact of past land use. There are Saxon sites between this one and the river (IPS 395). There is no objection in principle to development but any permission will require a condition relating to archaeological investigation attached to any planning consent. Early evaluation is advisable.

This site falls within the 91.4m height consultation zones surrounding Wattisham airfield, therefore any proposed structures which may exceed this height would need to be reviewed by the Ministry of Defence.

A transport assessment and travel plan will be required. Access visibility and junction spacing along Bramford Road will need to be considered.

Surface water flooding local to site - will need to be considered at planning application stage. See Appendix 1 of the Ipswich SFRA.

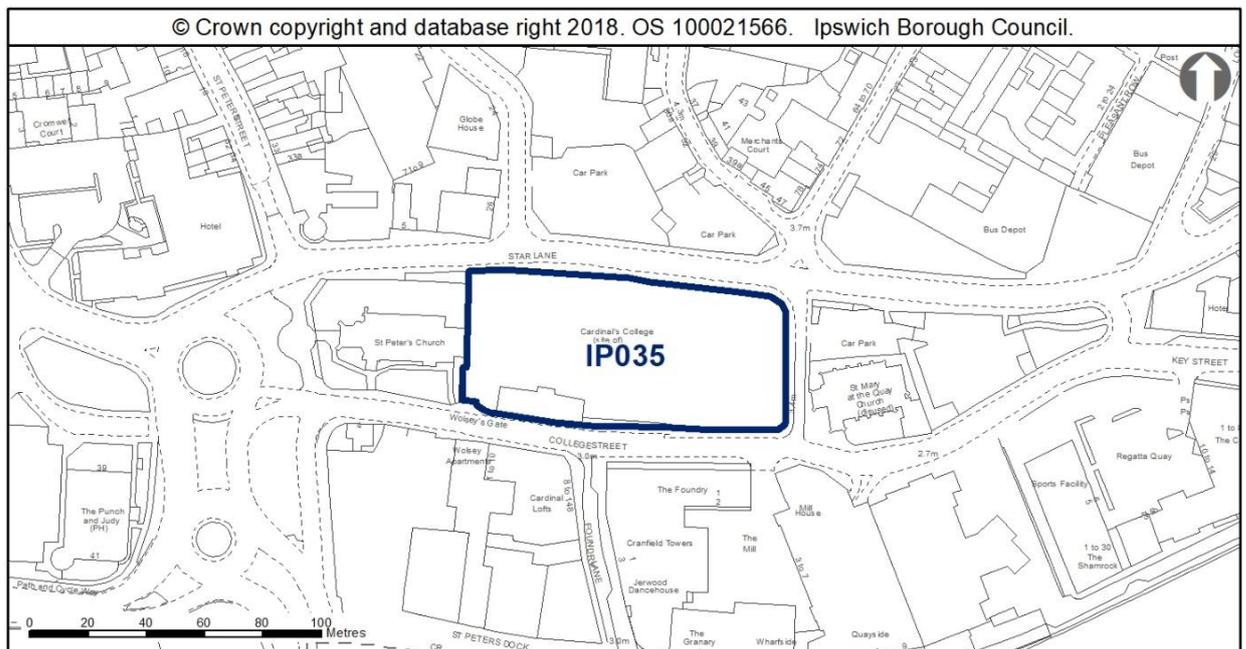
This allocation site is a former gravel and sand pit, now vacant and covered by vegetation. The allocation site is a wedge shaped piece of land, enclosed by residential development on its three main boundaries. The surrounding residential development is varied, with houses along Sproughton Road to the south being earliest, principally dating from the 1930s, the development off Dandalan Close to the east dating from the 1970s, and housing to the west being more recent off Jovian Way.

The allocation site is thus bounded by varied 20th century approaches to domestic architecture, although the existing access off Jovian Way would appear to be the principal access to the site, and so a design which would relate to the more contemporary development would probably be most appropriate, but perhaps with a more distinctive appearance than the development off Jovian Way.

Proposals should look to provide active and engaging frontages onto Bramford Road and Jovian Way, with a layout and design bespoke to the shape of the site.

Site ref: IP035 (UC036) Key Street / Star Lane / Burtons (St Peter's Port)

Site area: 0.54ha



Allocation Policy SP2

| Use(s) | | Indicative capacity |
|-----------|---|---|
| Primary | Residential | 86 (200dph on 80% of site) |
| Secondary | Office, Leisure or Retail (small scale) | Some individual units up to 200sqm – subject to Core Strategy policy DM32 |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Plan 2017

Allocation under SP5 for Employment (B1 uses such as offices/Hotel/Leisure/Small scale Retail and car parking) – 2000sqm

Current use

Vacant land apart from the grade II listed 1-5 College Street on south side.

Development constraints / issues

Access constraints, within an Air Quality Management Area, possible contamination, in a flood zone and listed buildings on or adjacent to the site. The site contains the grade II listed 1-5 College Street, adjoins the grade 1 listed and scheduled monument of Wolsey's Gate and lies within an area of archaeological importance. It is located

between two conservation areas (Central and Wet Dock) and two grade II* listed churches (St Peter's and St Mary at the Quay).

Development principles for the Merchant Quarter, within which this site is located, are set out in Chapter 6 of the Site Allocations and Policies plan (see 'Opportunity Area B').

This site, the former Cardinal Works, is one of the most important remaining sites in Ipswich with the potential to address major research questions about the origin and development of the town. The site lies on the remains of St Peter's Priory, within the historic core of Ipswich and close to the Anglo-Saxon and medieval waterfront. Evidence for the Priory and preceding Anglo-Saxon occupation was revealed during evaluation (IPS 455). There is high potential for archaeological remains of possible national significance, such as important waterlogged remains and evidence of the later Wolsey's College, and a Quaker burial ground.

Detailed early pre-application discussions with Suffolk County Council Archaeological Service and Historic England would be required in order to agree the scope of required assessment, the principle of development and to inform designs (e.g. to allow for preservation in-situ of deposits or appropriate programmes of work). Where appropriate to development impacts, total archaeological excavation of any development footprint prior to development will be required. Where development is accepted in principle, archaeological remains will be complex and important, and mitigation could involve significant costs and timescales.

This very sensitive site forms part of the transition area from the town centre to the waterfront where development needs to reflect this transition in terms of design and scale etc. Wolsey's Gate is a scheduled monument (List Entry No. 1006071). Proposals impacting upon its setting would require detailed pre-application discussions. The site presents opportunities for enhancing the setting of this scheduled monument. Scheduled Monument Consent (SMC) is a legal requirement for any development which might affect a monument either above or below ground level. Historic England administers the SMC application process on behalf of the Secretary of State for Digital, Culture, Media and Sport and should be consulted at the earliest opportunity to discuss the nature of the development. SMC is a separate process from the planning system.

Development proposals should look to find a long term use for 1-5 College Street, which has been on the borough's Buildings at Risk Register since 1995. If development is approved on the wider site, a Section 106 Agreement would be expected to require the repairs to 1-5 College Street to facilitate a use.

Proposed development around 1-5 College Street should look to enhance the setting of the listed building and the surroundings in which the building is enjoyed, and should not sit in the shadow of new, over scaled development.

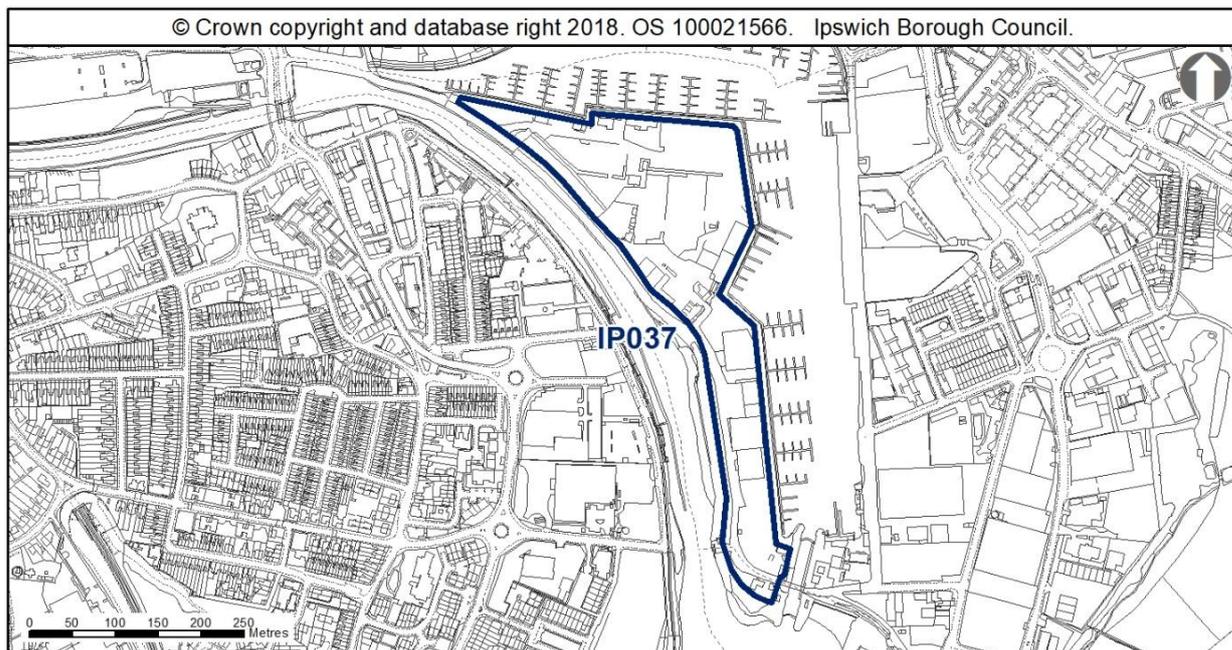
More generally, the scheme should look to introduce active frontages to all four of its boundaries to provide an engaging scheme which contributes to the street scene. A contemporary design approach would be welcomed, to positively contrast the historic buildings on and around the allocation site. Development should look to retain intervisibility between St Mary at the Quay and St Peters Churches, whose settings and relationship with one another are important contributors to the significance of these grade II* listed buildings.

Owing to the strategic importance of the site, development should look to improve public access through the site and encourage pedestrian movement across Star Lane and College Street, between the town centre and waterfront. This should also incorporate an area to the north of Wolsey's Gate for formal landscaping to improve access to this nationally important heritage asset.

Further detailed bat surveys will be required on the building as well as potentially detailed invertebrate surveys. The recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate.

Site ref: IP037 (UC038) Island Site

Site area: 6.02ha



Allocation Policies SP2, SP6, SP15 & SP16

| Use(s) | | Indicative capacity |
|-----------|---|------------------------------|
| Primary | Residential | 421 (100dph on 70% of site*) |
| Secondary | Amenity green space (minimum of 15%) Employment and leisure relating to Ipswich Haven Marina (as existing and additional as may be proposed) and enterprise zone. Restaurant and small retail (as existing) Early years education. | 0.90ha |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Plan 2017

50% Housing at high density – 271 dwellings. Open Space at 0.90ha.

Current use

Mix of uses – boat building, fitting and servicing, pub/restaurant, industrial uses.

Development constraints / issues

Redevelopment will be dependent on the intentions of existing businesses. The aim would be to retain and incorporate the existing boat-related uses and leisure uses. Development would require the provision of additional vehicular and pedestrian/cycle access (see Policy SP9), designed to be in keeping with the Conservation Area, including the provision of access for emergency vehicles as a priority. Any additional access would need to be risk assessed. RB36 and BY38/RB38 (New Cut West) are recorded on either side of the Island. Part of the Island Site now has Enterprise Zone status.

It is close to an AQMA, is in flood zones 2 and 3, and close to the Orwell Estuary Special Protection Area. The site is in close proximity to the Stour and Orwell SPA/Ramsar site. Information to inform a project level HRA will be required to demonstrate that urbanisation impacts on the site are prevented.. The recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate.

Development principles for the Island Site are set out in Chapter 6 of the Site Allocations and Policies plan (see 'Opportunity Area A'). They include, for example, retaining and refurbishing historic structures such as the local keeper's cottages and harbour master's house. The site will need to be master planned given its importance, and the master plan will need to address the development principles set out in the plan.

This site is within the Area of Archaeological Importance (IPS 413). There may be archaeological issues in relation to industrial heritage. Historic buildings should be assessed. Detailed early pre-application discussions with Suffolk County Council Archaeological Service would be required to agree the scope of required assessment. The Island site and channel were partly reclaimed from marsh. Previous assessment has shown that the island comprises up to 4.8m of made ground that either seals intact river deposits or lies directly onto river terrace gravels where the channel was previously dredged. Generally, these depths are too deep for conventional mitigation and generally sensitive remains will stay well protected, but historic and waterlogged deposits may survive, including remains of boats and structures. The Island area has palaeo-environmental potential. Made ground includes residual material from other parts of Ipswich.

Development of this site should be of a scale, massing and design which respects the character and appearance of the Wet Dock Conservation Area, and avoid creating a visual barrier which could cut off the buildings to the west of the waterfront to those on the east.

The masterplan should consider 'zones' for the various uses proposed at the allocation site, perhaps focussing the employment uses to the south of the site, and residential development to the north. The amenity space could be located around the Harbour Master's House and Lock Keepers Cottages to enhance the settings of these buildings, possibly utilising these buildings for a public use. A high quality public realm should complement the development of this allocation site, and look for opportunities to reinstate the promenade which once occupied the south of the island with a tree lined pedestrian boulevard.

The site forms a large part of the Wet Dock Conservation Area.

In respect of water supply and waste water treatment, infrastructure and / or treatment upgrades will be required to serve the proposed growth, or diversion of assets may be required.

The scale of residential development expected at the Island site would be sufficient to require early years provision. Its location would need to accord with the Council's Flood Risk and Development SPD.

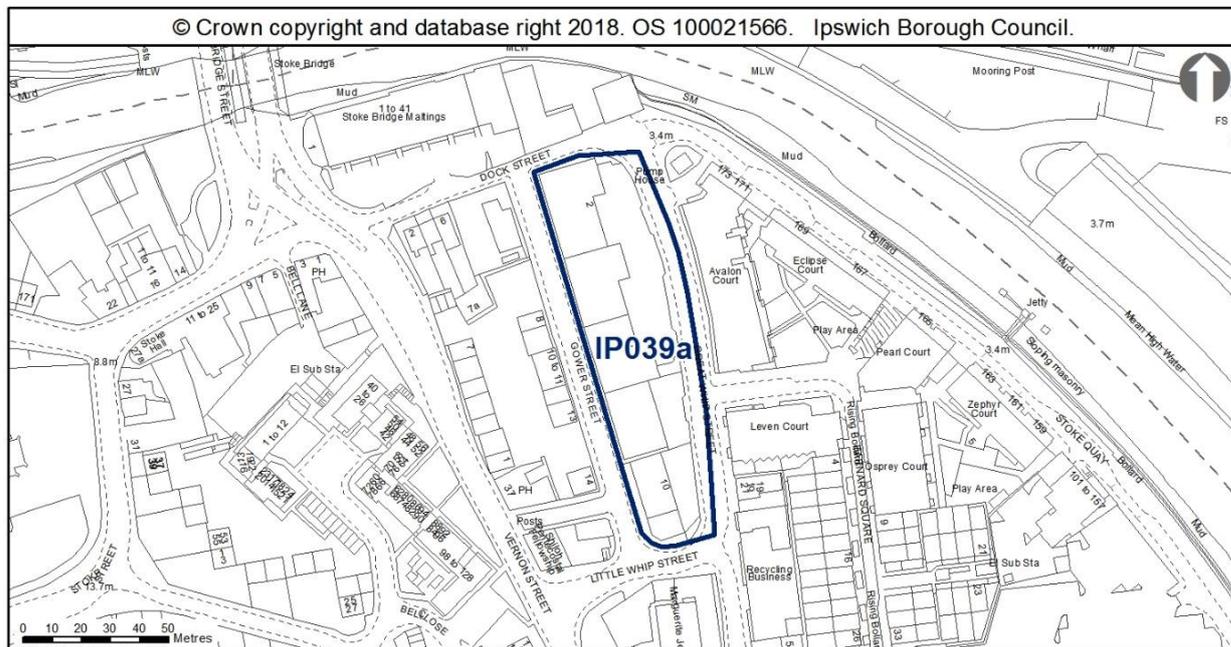
A transport assessment and travel plan will be required. Traffic impact especially on Bridge Street will need to be considered.

The site is close to uses which are safeguarded through the Suffolk Minerals Core Strategy. The relationship with the safeguarded minerals facility (within 250m) would need to be considered as part of the masterplanning and design.

Potential contaminated land. Therefore an assessment would be required. Potential for indirect impacts from water contamination, particularly during construction.

Development in this location should seek to enable greater recreational, leisure and sports use of the River Gipping and River Orwell, for example through the provision of Upper River Orwell (tidal) slipway or pontoon access and facilities including boat storage facilities, where practicable. Development must take account of the River Corridor Buffer (10m) where no development should take place, wherever practicable.

Site ref: IP039a (UC040 part) Land between Gower Street & Great Whip Street
Site area: 0.48ha



Allocation Policy SP2

| Use(s) | Indicative capacity |
|-------------|---------------------|
| Residential | 45 (95dph*) |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Plan 2017

As above

Current use

Part vacant industrial buildings.

Development constraints / issues

In a flood zone, close to an Air Quality Management Area, and possible contamination. Development principles for the Island Site Opportunity Area, within which the site is located, are set out in Chapter 6 of the Site Allocations and Policies plan (see 'Opportunity Area A').

The site is immediately adjacent to a large area of Anglo-Saxon and Medieval occupation at Stoke Quay core (IPS 683) and the Area of Archaeological Importance (IPS 413). There is high potential for Middle Saxon archaeology. Detailed early pre-application discussions with Suffolk County Council Archaeological Service would be

required to agree the scope of required assessment and to inform design (e.g. to allow for preservation in-situ of deposits or appropriate programmes of work). Archaeological remains may be complex and important and mitigation could involve significant costs and timescales.

This allocation site is located near to the waterfront to the south of the town centre, adjacent to both the Stoke and Wet Dock Conservation Areas, and is found immediately south of the grade II listed Stoke Bridge Maltings. The allocation site is currently occupied by utilitarian warehousing and bounded by metal security fencing. The site therefore makes little contribution to the street scene, nor setting of adjacent Conservation Areas.

Stoke was once a small hamlet independent of Ipswich, occupied by only a few buildings. The area around Stoke has been developed throughout the 19th century as the town grew and prospered, yet the area to the west is principally a domestic scale. Development to the east is more modern, and reflects the more industrial scale of maltings which once occupied the site. The allocation site is visible in key views out of the Stoke Conservation Area towards the waterfront, and indeed from the Wet Dock Conservation Area towards Stoke. The development of this allocation site therefore needs to respect the transition between the more built up character of the quay to the east, and more human, domestic scale of architecture to the east. Development should therefore balance the provision of housing units without being of a scale which would cause harm to the surrounding historic environment.

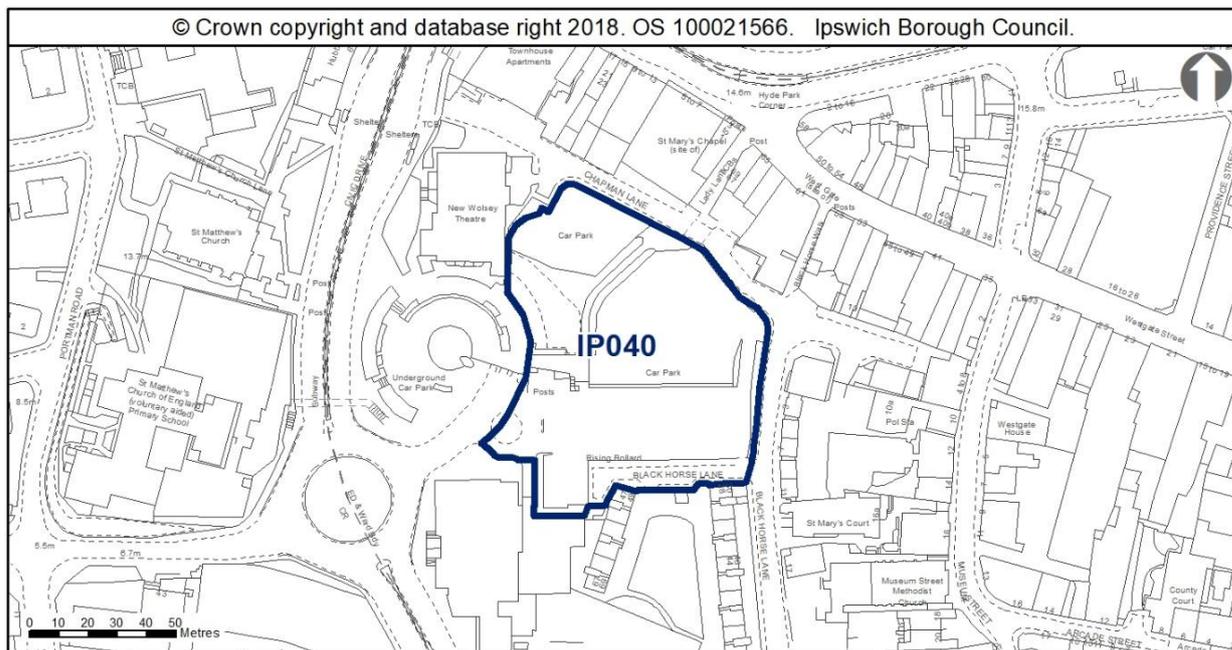
Development should look to adopt a contemporary architectural approach, and address all four of its boundaries with active and engaging frontages, utilising varied materials and pattern of fenestration to provide visual interest, whilst breaking up the massing of proposed development.

Surface water flooding local to site - will need to be considered at planning application stage. See Appendix 1 of the Ipswich SFRA.

To achieve biodiversity net gain, the recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate.

Site ref: IP040 (UC041) Former Civic Centre, Civic Drive (Westgate)

Site area: 0.73ha



Allocation Policies SP2 & SP10 – IP040

| Use(s) | | Indicative capacity |
|-----------|--|---------------------------|
| Primary | Residential | 59 (90dph on 90% of site) |
| Secondary | Retail A1 Restaurant (A3) and Theatre-related uses | 2,050 sq m (10%) |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Local Plan

Allocated with IP041 and existing housing to the south for retail-led mixed use.

10-15,000sqm Retail

20% housing at 100dph – 32 dwellings

Current use

Vacant development site, former Civic Centre, currently used as temporary car park.

Development constraints / issues

All the previous non-residential uses have now been relocated from this site. There is possible contamination. Residential use could be located above or adjacent to retail uses. The residential element of the development should be designed and located so as not to be adversely affected by noise from the Wolsey Theatre.

The site is adjacent to the Central Conservation Area and the Burlington Road Conservation Area lies a little further away to the west. grade II* St Matthews Church also lies to the west. Development principles for Westgate Opportunity Area, within which the site is located, are set out in Chapter 6 of the Site Allocations and Policies plan (see 'Opportunity Area E').

Development on this site should seek to introduce development which addresses Chapman Lane to improve this street scene. Improvements to the public realm along Chapman Lane should also be incorporated to make this a more useable and welcoming street, which is currently dominated by the rears of the 20th century buildings along St Matthews Street. Development along Chapman Lane would also be visible from Westgate Street through Lady Lane, a now much shortened passageway but historically significant as a site of pilgrimage to the shrine of Our Lady of Grace. The views towards the allocation site through Lady Lane should seek to make a positive architectural statement and encourage pedestrian passage along Lady Lane.

The allocation site includes an increase in level to the north of the site. The design of development needs to address this change in topography, in order to prevent the development to the north of the site becoming too dominant, or overshadowing that to the south.

Development should also seek to respond to the remaining pattern of development along Black Horse Lane and reinstate a continuous frontage to the east of the site, the scale of which should respond positively to the existing housing along Black Horse Lane.

The allocation site wraps around the edge of the Spiral Car Park to the west, which has a more contemporary character than the eastern boundary. A contemporary design approach which responds to the curve of the adjacent Spiral Car Park and improves the public realm around the site would be encouraged.

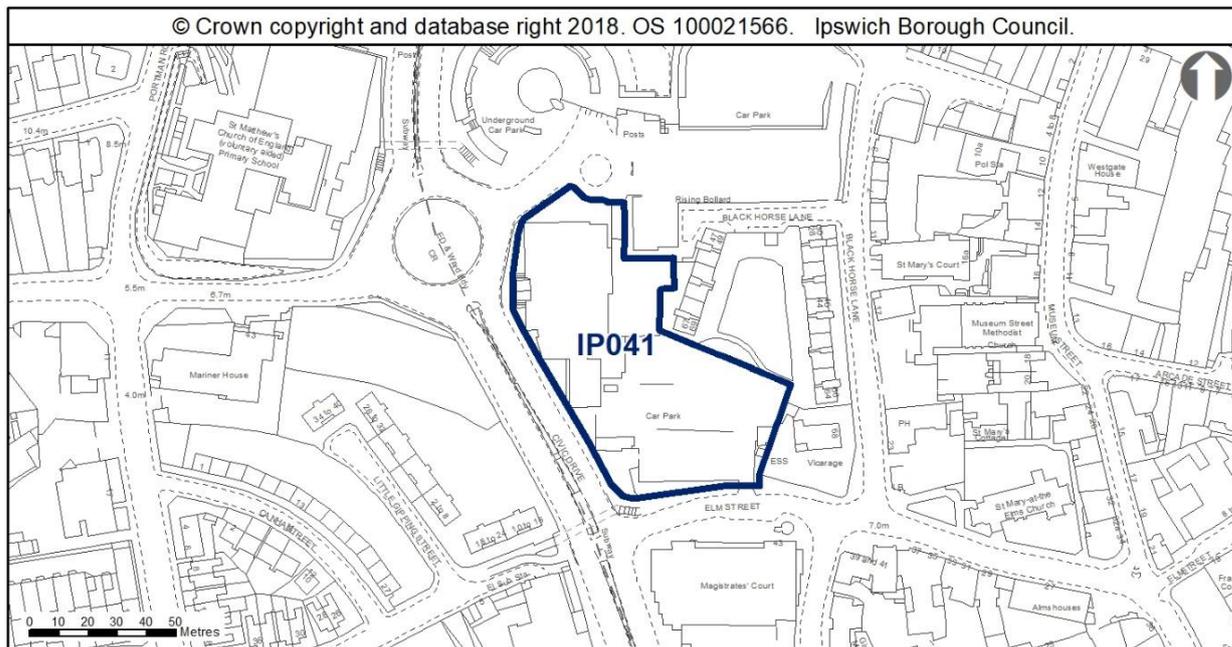
This site is within the Anglo-Saxon and medieval core and the Area of Archaeological Importance (IPS 413). Detailed early pre-application discussions with Suffolk County Council Archaeological Service would be required to agree the scope of required assessment and to inform design (e.g. to allow for preservation in-situ of deposits or appropriate programmes of work). Archaeological remains may be complex and important and mitigation could involve significant costs and timescales.

A transport assessment and travel plan will be required.

Surface water flooding local to site - will need to be considered at planning application stage. See Appendix 1 of the Ipswich SFRA.

The recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate.

Site ref: IP041 (UC042) Former Police Station, Civic Drive
Site area: 0.52ha



Allocation Policy SP2

| Use(s) | | Indicative capacity |
|---------|-------------|---------------------|
| Primary | Residential | 58 (110dph) |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Plan 2017

Allocated with IP040 and adjacent housing to the east for retail-led mixed use.

10-15,000sqm Retail

20% housing at 100dph – 32 dwellings

Current use

Vacant site currently used as temporary car park, former county court.

Development constraints / issues

All the previous non-residential uses have now been relocated from this site. There is possible contamination. Residential development should be designed so as not to be adversely affected by noise from the Wolsey Theatre.

The site is adjacent to the Central Conservation Area and the Burlington Road Conservation Area lies a little further away to the west. grade II* St Matthews Church also lies to the west. Development principles for Westgate Opportunity

Area, within which the site is located, are set out in Chapter 6 of the Site Allocations and Policies plan (see 'Opportunity Area E').

The existing building on site has interesting architectural aspects, including the glazed entrance and is illustrative of approaches to civic architecture in the mid 20th century. In this regard, the former County Court is important to the more recent history of the town, illustrating the perception of civic improvements to towns and cities in the 1960s and 70s, focusing on car users and providing new municipal centres. Civic Drive illustrates the road widening associated with this movement, whilst the former court represents the architectural approach to civic building at this time. This building could be reused and converted into residential use to provide some of the housing capacity suggested at this site, enhancing the building's architectural features.

The southern part of the site was occupied by the former Suffolk Constabulary Offices, now demolished. Development along Elm Street should therefore look to reinstate a building line to the south, although set back so that development would not harm views into the Central Conservation Area of St Mary at the Elms Church.

Improvements to the public realm which bound the site should also be incorporated into the development of this allocation site, to enhance the surroundings in which this area can be experienced, and indeed enhance the entrance to the Central Conservation Area and historic town centre to the east.

This site is within the Anglo-Saxon and medieval core and the Area of Archaeological Importance (IPS 413). Detailed early pre-application discussions with Suffolk County Council Archaeological Service would be required to agree the scope of required assessment and to inform design (e.g. to allow for preservation in-situ of deposits or appropriate programmes of work). Archaeological remains may be complex and important and mitigation could involve significant costs and timescales.

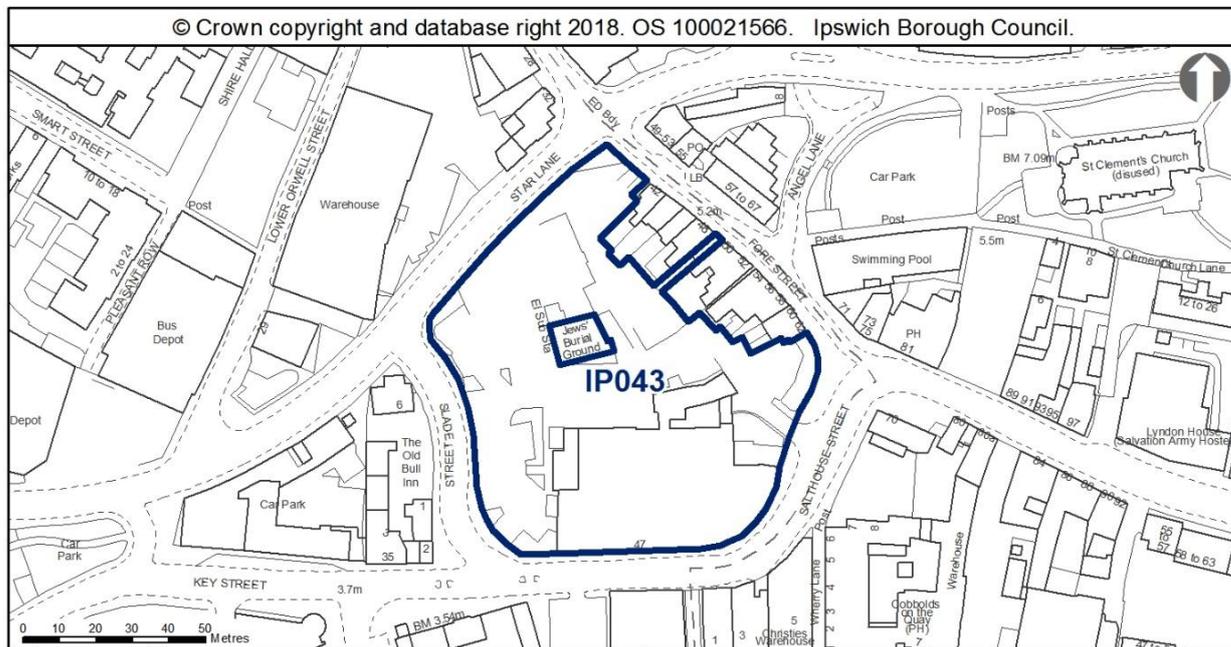
A transport assessment and travel plan will be required.

Surface water flooding local to site - will need to be considered at planning application stage. See Appendix 1 of the Ipswich SFRA.

The recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate.

Site ref: IP043 (UC044) Commercial Buildings, Star Lane

Site area: 0.70ha



Allocation Policies SP2 & SP5

| Use(s) | | Indicative capacity |
|-----------|---|----------------------------|
| Primary | Residential | 50 (90dph on 80% of site*) |
| Secondary | Employment (B1 uses such as offices, research & development, light industrial uses appropriate in a residential area) | 1,000sqm |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Plan 2017

As above

Current use

Vacant site, Jewish Burial Ground

Development constraints / issues

Access constraints, within an Air Quality Management Area and a flood risk area.

This site is located partly within the Central Conservation Area; it contains grade II listed buildings and adjoins others, including the grade II* 54-58 Fore Street to the north-east, the grade II* Old Custom House to the south-west and the complex of

highly graded buildings at Isaac Lord to the south-east. The site lies within an area of archaeological importance. The site presents opportunities for enhancing the setting of the Jewish burial ground which needs to be carefully respected by any development proposal. Any development proposal needs to ensure that adequate arrangements to access the Jewish Burial Ground are retained.

It is considered unlikely to come forward for student accommodation as per an extant permission for the site (11/00267/FUL), hence its inclusion in policy. Development principles for the Merchant Quarter, within which the site is located, are set out in Chapter 6 of the Site Allocations and Policies plan (see 'Opportunity Area B').

This allocation site is therefore in a highly sensitive historic setting, includes numerous heritage assets, and falls within the setting of a high volume of listed buildings. Development on this site therefore requires careful consideration to avoid causing harm to the historic environment through the introduction of development within their setting, whilst also considering the present intervisibility between heritage assets, key views of listed buildings into and out of the Conservation Area.

The development approved under IP/11/00267/FUL for a student village at this site has lapsed. However, refreshed policy and guidance with regard to the protection and enhancement of the historic environment has been issued since the 2011 application was approved, including the 2018 NPPF, Ipswich Development and Archaeology Supplementary Planning Document SPD adopted 2018, Historic England's The Setting of Heritage Assets: Historic Environment Good Practice Advice in Planning Note 3.

Owing to the highly sensitive historic environment which this allocation site has the potential to affect, a bespoke, master planned scheme should be prepared for this site so that the heritage constraints can be positively enhanced and/or preserved by the development of this site.

Development should retain the 1930s building along Key Street/Salthouse Street, possibly utilising this for the secondary employment use.

New development should look to establish building lines along Slade Street and Star Lane, providing active frontages to enhance these routes. The scale of new development should respect the human scale of historic buildings along Fore Street, taking a contemporary approach to design, whilst using the influences of adjacent architecture to respond to the historic context of the allocation site.

Public access through the site to improve permeability and pedestrian routes between the town centre and waterfront should also be provided. This should also include a landscaped courtyard in the centre of the site around the listed walls of the Jewish burial ground to provide a setting in which the listed walls can be

experienced, possibly incorporating some heritage interpretation and/or public art relevant to the Jewish history of Ipswich.

This site is within the urban core and area of archaeological importance (IPS 413) and previous archaeological evaluation and limited excavation has revealed Anglo-Saxon and Medieval remains in particular (IPS 639, 371,372, 358). There is outstanding post-excavation work under IP/11/00267} and further work would be needed across the site. There is potential for archaeological remains of possible national significance. Detailed early pre-application discussions with Suffolk County Council Archaeological Service and Historic England would be required in order to agree the scope of required assessment, the principle of development and to inform design. Where development is accepted in principle, archaeological remains will be complex and important, and mitigation could involve significant costs and timescales.

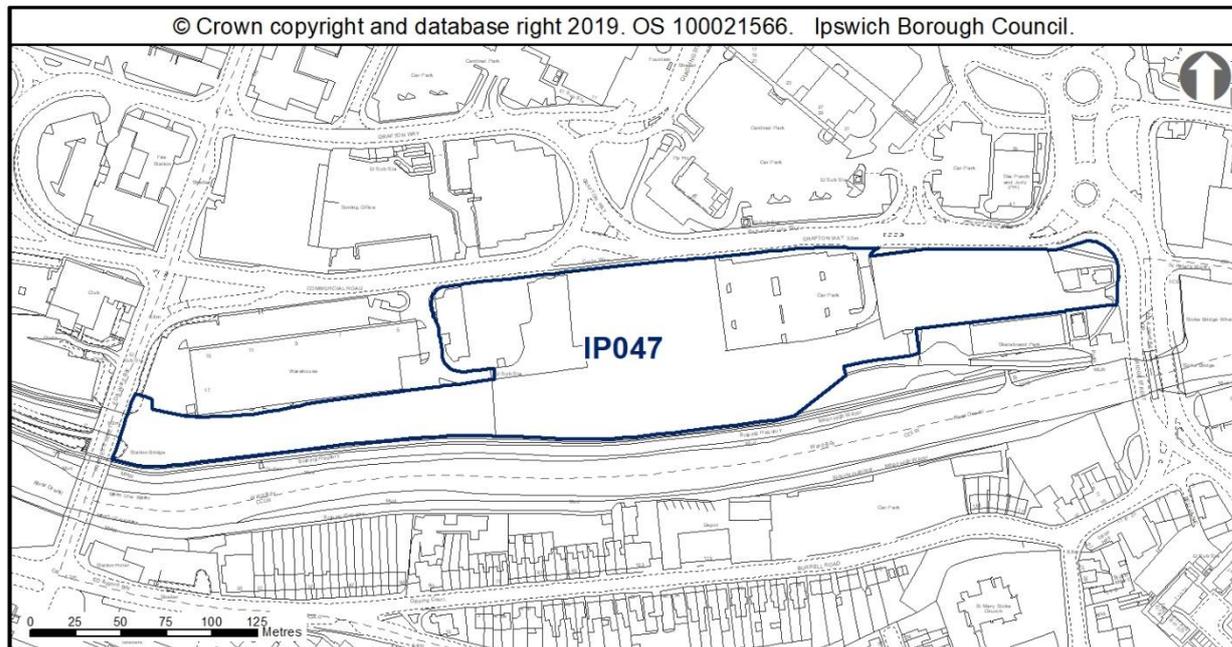
A transport assessment and travel plan will be required.

Surface water flooding local to site - will need to be considered at planning application stage. See Appendix 1 of the Ipswich SFRA.

Although this site is currently of low wildlife value, the older buildings could support bats and consequently further surveys are recommended. To achieve biodiversity net gain, the recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate.

Site Ref: IP047 Land at Commercial Road

Site area: 3.11ha



Allocated Policies SP2 & SP6

| Use(s) | | Indicative capacity |
|-----------|--|---|
| Primary | Residential | 173 (55dph on 80% of site based on pending application 19/00148/OUT) |
| Secondary | Hotel/ Leisure/ Retail Public open space and enhanced river path on the southern boundary | Min 15% of the site to form public open space and enhanced river path – 0.5ha |

Adopted Plan 2017

As above

Current use

Temporary car park, vacant land

Development constraints/issues

Adjacent to Area of Archaeological Importance.

Previous planning permissions (e.g. 08/00953/FUL) have had a condition attached requiring archaeological investigation, which could include archaeological monitoring and recording of initial groundworks with contingency for fuller archaeological recording if deemed necessary; recording the remaining railway features; checking whether any trace of the dock tramway survives; and a palaeoenvironmental sampling strategy.

In a Flood Zone and adjacent to an AQMA. Possible contamination.

Contains trees protected by a TPO (an application for tree works may be required).

Adjacent to the river which forms a continuous green corridor and ecological network and is a County Wildlife Site. Public open space provision should be sited on the southern boundary to enhance the existing wildlife corridor. A survey for reptiles is recommended prior to any vegetation clearance and if present they should ideally be retained within existing habitat on site, or on enhanced habitat adjacent to the site. The recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate.

FP61 (River Path) is recorded abutting most of the southern edge of the site. The developer should ensure there are high-quality cycle and pedestrian links through the site which help connect the Princes Street Bridge area to the Waterfront area.

Development principles for the River and Princes Street Corridor, within which this site is located, are set out in Chapter 6 of the Site Allocations and Policies plan (see 'Opportunity Area F').

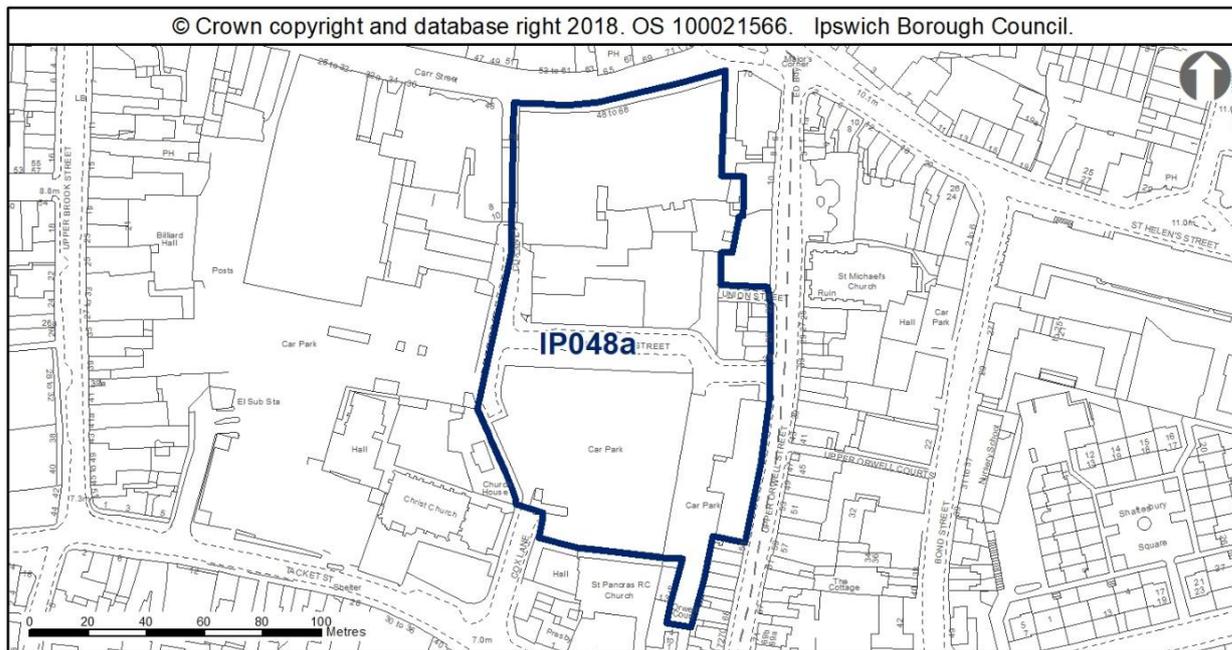
A transport assessment will be needed for this site due to its scale, location and the proposed uses, and it may identify the need to contribute towards significant off-site highway mitigation, depending on the detail of the scheme.

The site is expected to require improvements to the existing water supply and foul sewerage networks to enable development.

Development in this location should seek to enable greater recreational, leisure and sports use of the River Gipping and River Orwell, for example through the provision of Upper River Orwell (tidal) slipway or pontoon access and facilities including boat storage facilities, where practicable.

Development must take account of the River Corridor Buffer (10m) where no development should take place, wherever practicable.

Site ref: IP048a (UC051) Mint Quarter / Cox Lane East Regeneration Area
Site area: 1.33ha



Allocation Policies SP2, SP6, SP7 & SP17

| Use(s) | | Indicative capacity |
|-----------|--|--------------------------------|
| Primary | Primary School Amenity green space & short stay multi-storey car parking 40% | - |
| Secondary | Residential 40% | 53 (100dph on 40% of the site) |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Plan 2017

60% Residential – 72 dwellings

Amenity green space & short stay multi-storey car parking 40%

Current use

Surface level short stay car parking, vacant shops

Development constraints / issues

Short stay parking for shoppers needed and the provision of public open space within the site, however part of these could span Cox Lane into the west part of the

site also. A development brief for the whole site (east and west of Cox Lane) will be prepared but development may come forward incrementally.

The identification of the north part of this site for a primary school has replaced the adopted Local Plan allocation IP258 as part of the Education Quarter.

Close to an Air Quality Management Area, possible contamination and TPOs on site or nearby (an application for tree works may be needed). The façade to Carr Street is locally listed. The site lies within an area of archaeological importance, contains a large scheduled monument and is adjacent to the Central Conservation Area, two grade II listed churches (Christ Church and St Pancras) and other listed buildings to the south.

Development principles for the Mint Quarter / Cox Lane regeneration area, within which the site is located, are set out in Chapter 6 of the Site Allocations and Policies development plan document (see 'Opportunity Area C'). FP89 and FP241 are recorded along the western edge of site IP048a. Opportunities should be explored to see if a north-south cycling link can be provided between Carr Street and Tacket Street.

Much of this site is surface level car park which does not contribute to the appearance or general amenity of the area, nor does it encourage pedestrian footfall through the site. The sensitive development of this is therefore welcomed, and has the potential to enhance the setting of the Conservation Area. However as above, the site does fall within the setting of numerous listed buildings, with the surface level car parks reading as open space and so the introduction of development on this allocation has the potential to harm the historic environment, if not designed carefully and with regard to the surrounding heritage.

The buildings along Carr Street are heritage assets included on the Local List SPD, and should be retained and incorporated into development proposals if feasible. These buildings form an important gateway to the town centre, represent the growth and prominence of the Co-operative movement in Ipswich, as well as illustrate early 20th century architectural approaches to commercial development, being formed of four distinctive phases and designs. The removal of the heavy fascia across the front of these buildings would be encouraged, as would the sensitive replacement of the late 20th ground floor shop fronts.

The redevelopment of the site should look to improve views along Cox Lane and create a more welcoming pedestrian route from north to south. Removing the stepped pedestrian footbridge which overhangs Cox Lane between 46 and 48 Cox Lane would assist with this, as would a reduction in the scale of the footbridge between the later phases of the buildings, to open up views to the south. The mosaic to Cox Lane should be retained, or reused in the redevelopment of the site, if it can be removed without causing damage.

Proposals should look to establish a legible layout which will encourage pedestrian access through the site, continuing Cox Lane through the site to reinstate this historic route. Development principles should also look to provide active building frontages which engage with Upper Orwell Street and Tacket Street. The scale of development should respect the existing scale of commercial and residential buildings around the allocation site, perhaps taking a stepped approach to address the transition from town centre to periphery shopping areas of lower scale.

Development should look to reinstate the building line along Upper Orwell Street, respecting the existing scale of development. The existing terrace at 22-28 Upper Orwell Street should be retained and incorporated into the development of the site, taking opportunities to reinstate timber windows and provide shop fronts more sympathetic to the age and character of these buildings.

The site lies within the area of archaeological importance (IPS 413) and much of it lies within a scheduled monument relating to the Middle and Late Saxon town, preserved under current car parks (List entry No 1005983). Scheduled Monument Consent (SMC) is a legal requirement for any development which might affect a monument either above or below ground level. Historic England administers the SMC application process on behalf of the Secretary of State for Digital, Culture, Media and Sport and should be consulted at the earliest opportunity to discuss the nature of the development. SMC is a separate process from the planning system.

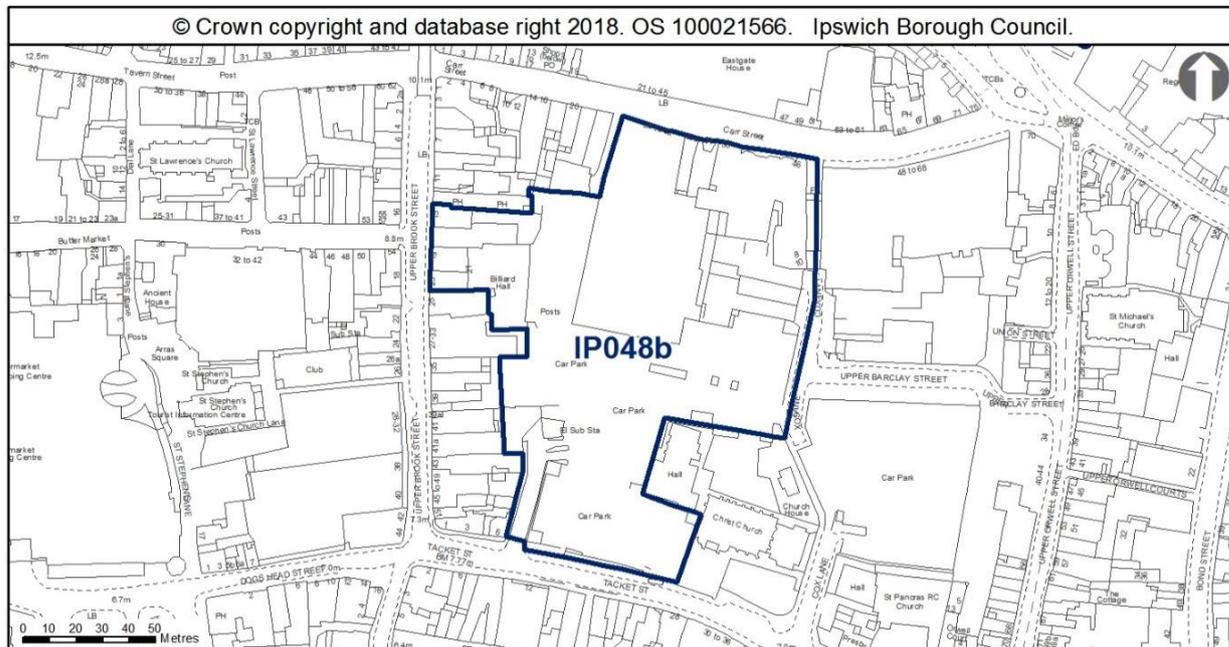
There is also potential for nationally important archaeological remains outside the scheduled areas. Detailed early pre-application discussions with Suffolk County Council Archaeological Service and Historic England would be required in order to agree the principle of development and inform designs (e.g. to allow for preservation in-situ of deposits or appropriate programmes of work). Excavations and interventions have taken place in parts of the site and revealed evidence for occupation and activity from the Middle Saxon period onwards - the rest of the site is undisturbed from modern development under car parks and is anticipated to contain rich and well preserved archaeological remains. Development would require full assessment prior to the granting of consent to any proposals - desk based assessment, building survey and field evaluation. Where development is accepted in principle, archaeological remains will be complex and important, and mitigation could involve significant costs and timescales.

A transport assessment and travel plan will be required.

Surface water flooding local to site - will need to be considered at planning application stage. See Appendix 1 of the Ipswich SFRA.

To achieve biodiversity net gain, the recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate.

Site ref: IP048b Mint Quarter / Cox Lane West Regeneration Area
Site area: 1.34ha



Allocation Policies SP2, SP6, SP10 & SP17

| Use(s) | | Indicative capacity |
|-----------|---------------------|--------------------------------|
| Primary | Retail | 4,800sqm |
| | Short Stay Car Park | |
| | Open Space | |
| Secondary | Residential | 36 (90dph on 30% of the site)* |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Plan 2017

Secondary Shopping Frontage Zone within the Central Shopping Area.

Current use

Surface level short stay car parking, vacant and occupied shops

Development constraints / issues

Short stay parking for shoppers needed and the provision of public open space within the site, however part of these could span Cox Lane into the east part of the site also. A development brief for the whole site (east and west of Cox Lane) will be prepared but development may come forward incrementally.

Close to an Air Quality Management Area, possible contamination and TPOs on site or nearby (an application for tree works may be needed). The site lies within an area of archaeological importance, contains a large scheduled monument and is adjacent to the Central Conservation Area, two grade II listed churches (Christ Church and St Pancras) and other listed buildings to the south.

Development principles for the Mint Quarter / Cox Lane regeneration area, within which the site is located, are set out in Chapter 6 of the Site Allocations and Policies development plan document (see 'Opportunity Area C'). FP89 and FP241 are recorded along the western edge of site IP048a. Opportunities should be explored to see if a north-south cycling link can be provided between Carr Street and Tacket Street.

Development should look to improve pedestrian permeability through the site, providing a north/south route through the site, whilst also enhancing the pedestrian thoroughfare from Upper Brook Street. In this regard, development should look to provide active and engaging frontages to the pedestrian routes across the site, addressing existing development whilst also being contemporary in appearance, with thoughtful design influenced by the setting of the allocation site.

Proposals should look to respect the scale of existing development of 2-3 storey development along Upper Brook Street and Tacket Street, with the possibility of increasing the scale of development towards the centre of the site around the existing late C20th Co-op building. If the late C20th Co-op building is proposed for retention, then improvements to the external appearance of this building should be included in the redevelopment scheme.

The open space should be incorporated into development proposals, and should look to improve the setting of Christ Church and its associated hall and vicarage. The design of this space could be informed by the archaeological excavations which would be required prior to the development of this site, perhaps including interpretation and/or public art to illustrate the archaeological significance of the area.

The site lies within the area of archaeological importance (IPS 413) and much of it lies within a scheduled monument relating to the Middle and Late Saxon town, preserved under current car parks (List entry No 1005983). Scheduled Monument Consent (SMC) is a legal requirement for any development which might affect a monument either above or below ground level. Historic England administers the SMC application process on behalf of the Secretary of State for Digital, Culture, Media and Sport and should be consulted at the earliest opportunity to discuss the nature of the development. SMC is a separate process from the planning system.

There is also potential for nationally important archaeological remains outside the scheduled areas. Detailed early pre-application discussions with Suffolk County

Council Archaeological Service and Historic England would be required in order to agree the principle of development and inform designs (e.g. to allow for preservation in-situ of deposits or appropriate programmes of work). Excavations and interventions have taken place in parts of the site and revealed evidence for occupation and activity from the Middle Saxon period onwards - the rest of the site is undisturbed from modern development under car parks and is anticipated to contain rich and well preserved archaeological remains. Development would require full assessment prior to the granting of consent to any proposals - desk based assessment, building survey and field evaluation. Where development is accepted in principle, archaeological remains will be complex and important, and mitigation could involve significant costs and timescales.

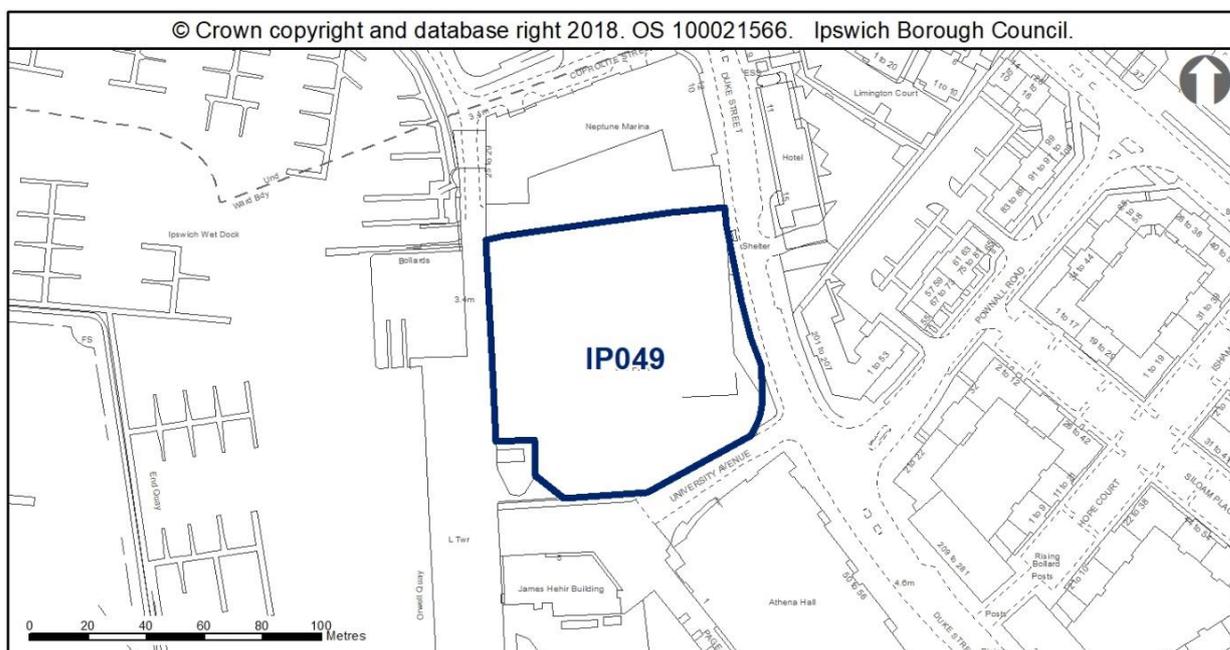
A transport assessment and travel plan will be required.

Surface water flooding local to site - will need to be considered at planning application stage. See Appendix 1 of the Ipswich SFRA.

To achieve biodiversity net gain, the recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate.

Site ref: IP049 (UC052) No. 8 Shed, Orwell Quay

Site area: 0.76ha



Allocation Policy SP12 and SP17

| Use(s) | | Indicative capacity |
|-----------|--|---------------------|
| Primary | Education / Waterfront uses | - |
| | Public long stay car parking | - |
| Secondary | Ancillary uses to university (e.g. GP surgery) | - |

Adopted Plan 2017

As above

Current use

Temporary surface level long stay car parking.

Development constraints / issues

Within a flood zone, close to an Air Quality Management Area and there may be contamination. It is adjacent to the Wet Dock Conservation Area. RB36 is recorded along the site's western boundary.

Although not located within the Conservation Area boundary, the site does contribute to the setting of the Conservation Area, with views of the waterfront across the site being obtained from Dock Street. Development of this site therefore has the potential to affect the character and special interest of the Wet Dock Conservation Area, and therefore requires careful consideration.

Development should look to make a positive, bold architectural statement, continuing the contemporary design influences of the nearby University of Suffolk building, to create an interesting, intriguing, landmark building which would draw pedestrian movement around the waterfront.

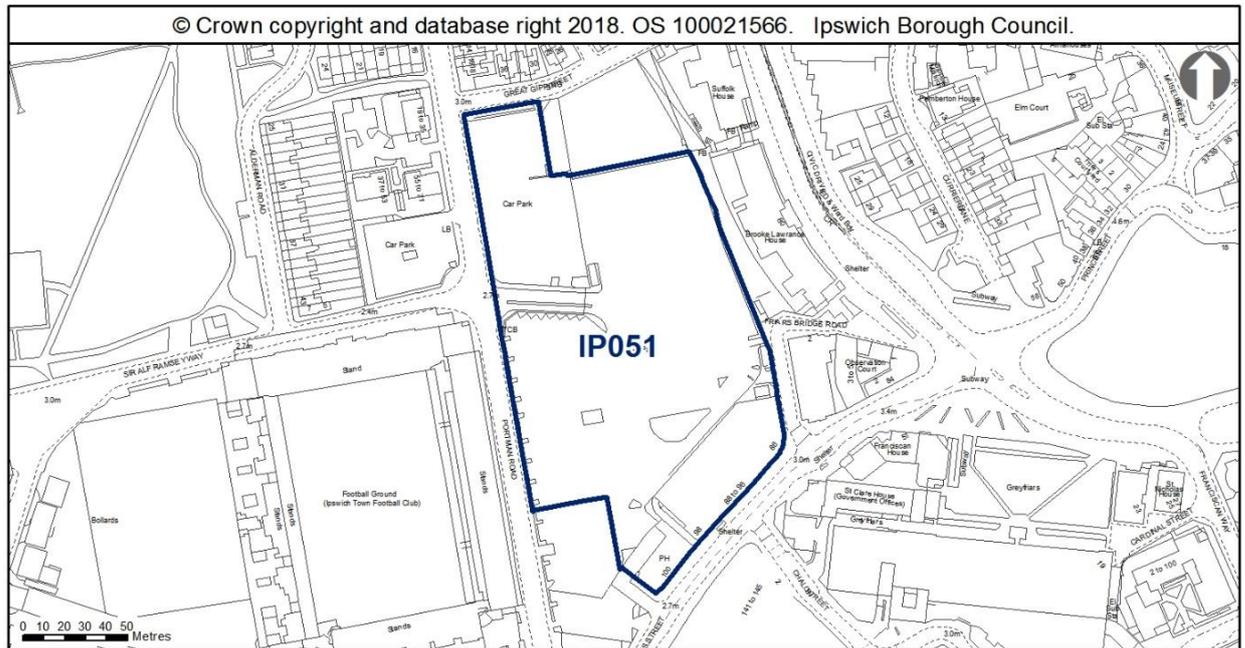
The development of this site should look to respect the existing scale of development, with taller building elements onto the Waterfront, and then stepping down towards Duke Street. Elevations should be sure to address Neptune Quay, University Avenue and Duke Street, providing active frontages which engage positively with the street scene. The public realm surrounding the allocation site should be high quality and encourage pedestrian movement between the waterfront and Duke Street.

A multi-storey car park in this location could be acceptable, however would need to be of a high quality design, finished with quality materials so that the structure is a feature building which contributes positively to the character of the area, and of a height which would not dominate the scale of existing development.

The site is within the Area of Archaeological Importance (IPS413). This site has potential for evidence relating to exploitation of the foreshore from the Middle Saxon period onwards, as well as potential for remains relating to medieval and post-medieval shipyards. Buildings on the site should be assessed. Detailed early pre-application discussions with Suffolk County Council Archaeological Service would be required to agree the scope of required assessment and to ensure that archaeological mitigation can be factored in to project designs.

Development must take account of the River Corridor Buffer (10m) where no development should take place, wherever practicable.

Site ref: IP051 (UC054) Old Cattle Market, Portman Road - South
Site area: 2.21ha



Allocation Policy SP5 & SP17

| Use(s) | | Indicative capacity |
|-----------|--|---------------------|
| Primary | Offices (B1a) | 20,000sqm |
| Secondary | Hotel / Leisure (and possible long stay car parking) | - |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Local Plan

As above.

Current use

Surface level long stay car parking, vacant former car showroom, vehicle repair workshop.

Development constraints / issues

There may be a need to provide replacement car parking on site before the site can be redeveloped and/or this may be accommodated at the site on West End Road (IP015). Possible contamination and in a flood zone. Development principles for River Corridor and Princes Street Corridor Opportunity Area, within which the site is located, are set out in Chapter 6 of the Site Allocations and Policies plan (see

'Opportunity Area F'). They include creating a new townscape of well defined blocks east of Portman Road.

This allocation site is principally surface level car parking located to the east of Portman Road, and to the rear of buildings along Civic Drive. The site is in a transitional area between the town centre and river valley, located adjacent to Ipswich Town Football Club. There is an increase in topography to the north of the site, with land to the north being covered by the Burlington Road Conservation Area, views of which can be seen from Portman Road, in particular the rears of domestic properties along Dalton Road, and the prominent spire of the International Church on Barrack Corner.

New development should look to continue the existing building line along Princes Street, providing buildings of a high quality design and appearance which would contribute to the character of Princes Street corridor, and make a positive architectural statement to welcome visitors to the town centre when approaching from the train station.

Improvements to the public realm should also be incorporated into development proposals, and should also look to improve pedestrian permeability east/west through the site, perhaps providing a pedestrian link across the site between Sir Alf Ramsey Way and Friars Bridge Road.

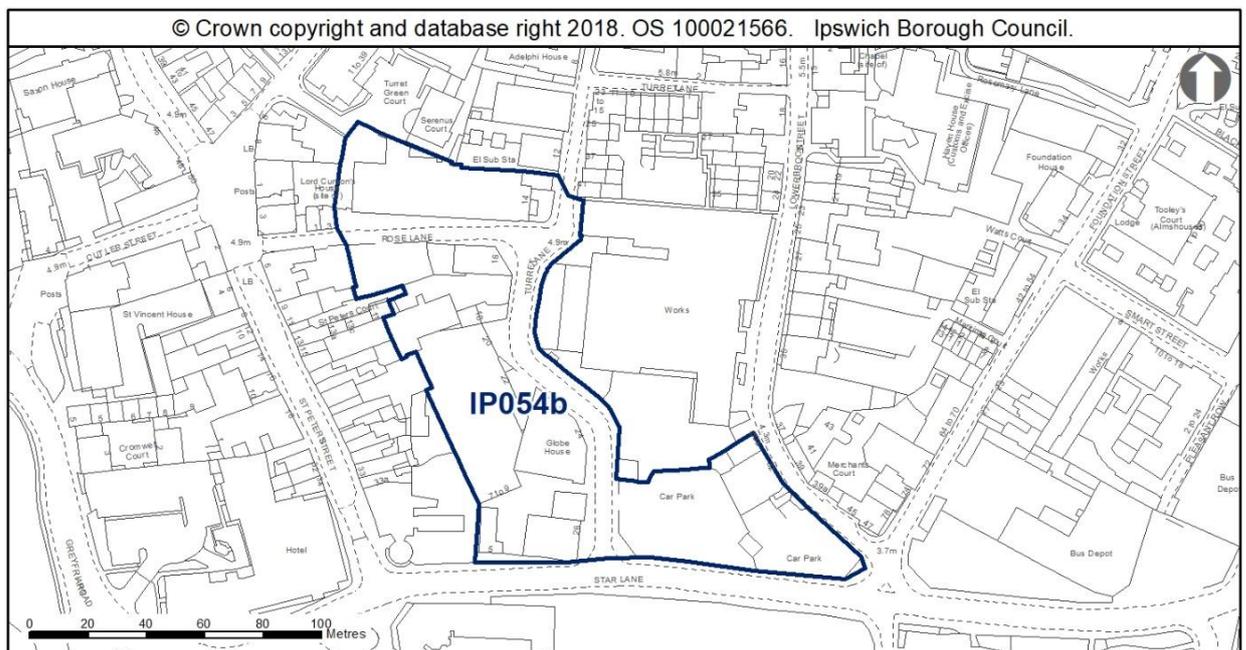
The Bobby Robson Statue is located in this allocation site, and development would need to carefully consider the relocation of this statue, with regard to the football ground. The scheme could perhaps incorporate a public space or plaza along the Portman Road frontage which would provide the statue with a planned setting, which would improve the surroundings in which the statue can be experienced.

Ancillary to the B1 use of this allocation site, secondary uses such as bars and eateries could be accommodated, possibly around the public space at ground floor, providing residential/office accommodation above.

In terms of archaeology, this site has potential for palaeo-environmental and waterlogged remains. There may be potential for waterlogged remains relating to the waterfront, and Friar's Bridge (IP260). A desk-based assessment should be undertaken, with an appropriate level of field evaluation that is informed by its results.

Site ref: IP054b (UC057) Land between Old Cattle Market & Star Lane

Site area: 1.08ha



Allocation Policies SP2

| Use(s) | | Indicative capacity |
|-----------|---|---|
| Primary | Housing | 40 (60dph on 60% of site) |
| Secondary | Small scale retail Electricity sub-station | Some individual units up to 200sqm – subject to Core Strategy policy DM32 |

* lower than policy minimum assumed due to locational characteristics

Adopted Plan 2017

Larger site allocated incorporating the former print works. As above but residential 28 dwellings and possible public car parking included.

Current use

To the west of Turret Lane, various employment uses, car parking. To the east of Turret Lane, car parking.

Development constraints / issues

The site is within or close to an Air Quality Management Area and part within a flood zone at the southern end of the site. There are TPOs on site or nearby (an

application for Tree Works may be needed). The site is also within an area of archaeological importance, partly within the Central Conservation Area, contains a scheduled monument and two grade II listed buildings (30A and 32 Lower Brook Street). The site is flanked by the rest of the Central conservation area and several listed buildings, with two grade II* churches to the south.

Whilst much of this area is not located in the Conservation Area, the allocation site nonetheless is located in the setting of the Central Conservation Area, responds to several historic ranges and outbuildings along St Peters Street, and located on land of archaeological significance. The application site is thus in a historically significant location, contributing to the setting of heritage assets, as well as being in a prominent position in the historic north/south route onto Turret Lane.

Development should look to respect the domestic scale of existing architecture along St Peters Street, and should allow breathing space between these listed buildings and new development. The winding nature of Turret Lane should be incorporated into development proposals, and should reveal the design of the scheme as users turn the corner of Turret Lane, and be of a high quality design which would help link the town centre and the waterfront.

The corner of Star Lane and Turret Lane should look to introduce a landmark building to provide interest to the street scene. The adjacent cylindrical building at the junction of St Peters Street and Star Lane serves as an example that a landmark building need not necessarily be tall or overscaled.

Development should introduce a frontage to Star Lane, although must have regard for the highly graded listed buildings to the south, and incorporate the trees which are covered by TPOs in the eastern corner of the allocation site at Star Lane. This area could perhaps be a pocket of open/amenity space, which would allow for a break in new development and the listed buildings on Lower Brook Street.

If retail units are to be incorporated into the development of this allocation site, then perhaps focussing these along Rose Lane would be most appropriate, to continue the commercial character of St Peter's Street.

Redevelopment will be dependent on the intentions of existing businesses.

Development principles for the Merchant Quarter, within which the site is located, are set out in Chapter 6 of the Site Allocations and Policies development plan document (see 'Opportunity Area B'). They include, for example, a layout to relate to the historic street pattern and enhanced pedestrian linkage between the town centre and the Waterfront. It is a key principle for the development of this site that connectivity is built in to ensure linkages with surrounding development.

There is a need to protect land for an extension to the electricity sub-station or new provision within the site. The route of Turret Lane should be protected in development proposals.

This site lies within the area of archaeological importance (IPS 413) and contains a scheduled monument (split over two separate areas) relating to the Anglo-Saxon and medieval town of Ipswich (List Entry No 1005987). Parts of the area have been investigated (IPS 214), IPS 369, and IPS 574). The latter found a wood-lined well with an assemblage of boar tusks, demonstrating good potential for the survival of wet and well preserved organic deposits. Scheduled Monument Consent (SMC) is a legal requirement for any development which might affect a monument either above or below ground level. Historic England administers the SMC application process on behalf of the Secretary of State for Digital, Culture, Media and Sport and should be consulted at the earliest opportunity to discuss the nature of the development. SMC is a separate process from the planning system.

There is also a potential for nationally important archaeological remains outside the scheduled areas. Detailed pre-application discussions with Suffolk County Council Archaeological Service and Historic England would be required in order to agree the principle of development and inform design (e.g. to allow preservation in-situ of deposits or appropriate programmes of work). Where development is accepted in principle, archaeological remains will be complex and important, and mitigation could involve significant costs and timescales.

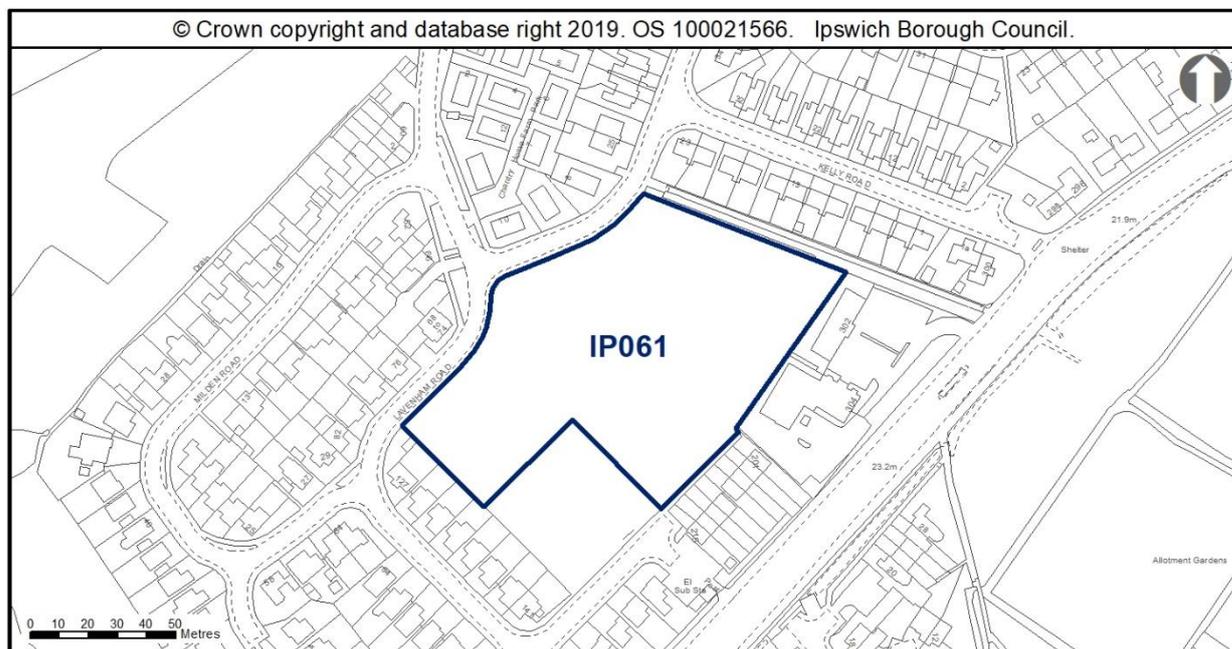
A transport assessment and travel plan will be required.

Surface water flooding local to site - will need to be considered at planning application stage. See Appendix 1 of the Ipswich SFRA.

Although this site is currently of low wildlife value, there is a potential risk that buildings could support bats and consequently an internal inspection by a suitably qualified ecologist is recommended, which will also encompass nesting birds. To achieve biodiversity net gain, the recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate.

Site ref: IP061 (UC064) Former School Site, Lavenham Road

Site area: 0.9ha



Allocation Policies SP2 & SP6

| Use(s) | | Indicative capacity |
|-----------|--------------------------------------|----------------------------|
| Primary | Residential | 23 (40dph on 60% of site*) |
| Secondary | Amenity green space (on 40% of site) | 0.32ha |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Plan 2017

Larger site area (1.08ha) with residential capacity of 30 dwellings (40dph on 70% of site) and 0.32ha of amenity green space on (on 30% of site).

Current use

Open space.

Development constraints / issues

The site is no longer required for a new school. The site was identified through the Open Space Sport and Recreation Study (2009) as park and garden and is used for informal recreation. It was identified as a school site through the 1997 Local Plan. The proposal is that 70% of the site is used for housing to improve the remaining open space. The type of open space provision would need to be agreed with the Council’s Parks and Open Spaces Service.

Planning permission (18/00991/FPC) was granted and works have commenced on part of the site (0.18ha) for the development of four general needs housing units and four respite care units with staff and communal areas. Development of the site allocation (0.9ha) will need to ensure that it is compatible with this adjacent permission.

In terms of archaeology, this site is in the vicinity of a Bronze Age cremation (IPS017), and Roman and Iron Age finds (IPS 034, IPS 185). It has been subject to geophysical survey and some follow up test pits which identified areas of recent overburden but did not reveal major archaeological features. Trial trenching of this site should be carried out in order to further characterise archaeological remains. Evaluation should be undertaken early in the project management to allow mitigation and investigation strategies to be developed. Sparse remains might be anticipated.

It is also adjacent to the listed building Crane Hall – development will need to have regard to the setting of the listed building.

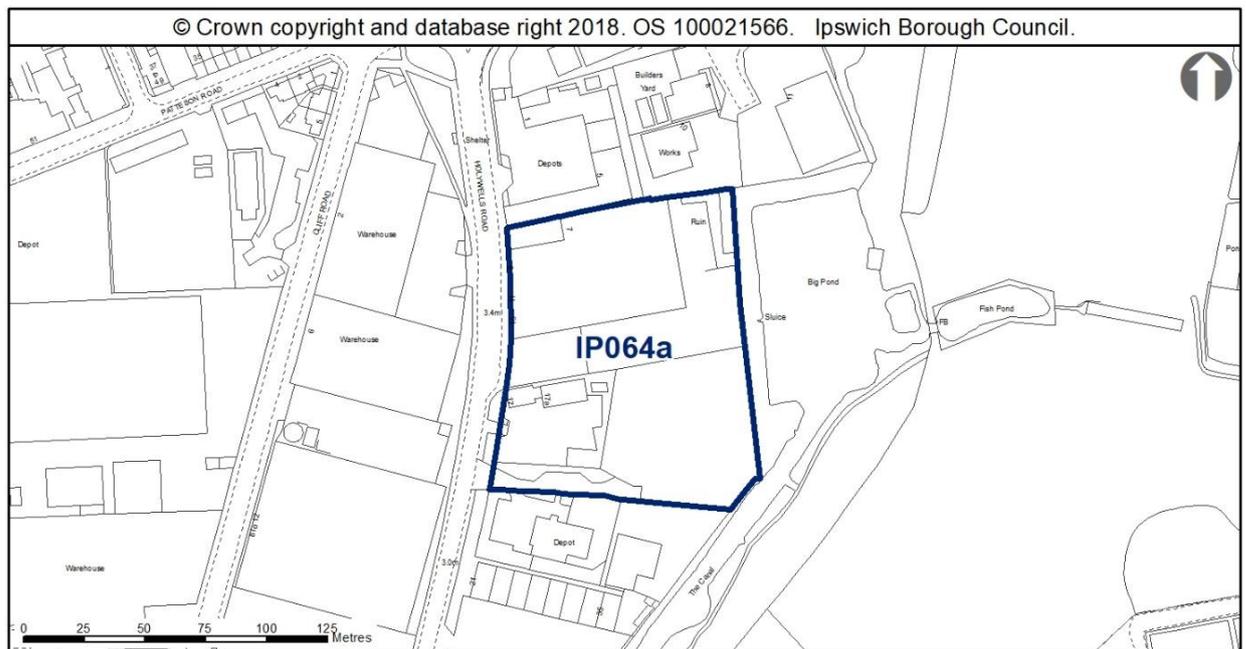
The proposed development should respect the scale and layout of existing residential development around Lavenham Road, and address the topographical changes across the allocation site to provide a distinctive development.

This site falls within the 91.4m height consultation zones surrounding Wattisham airfield, therefore any proposed structures which may exceed this height would need to be reviewed by the Ministry of Defence.

The site provides a valuable stepping-stone habitat between Chantry Park County Wildlife Site and Gippeswyk Park in combination with London Road Allotments. With sensitive landscaping there is the opportunity to improve the quality of this stepping-stone habitat through enhancement on the remaining on-site habitat. New development should retain as much of the existing habitat as possible and integrate it within a landscaping scheme, particularly the hedgerow and mature tree along the northern boundary. To achieve biodiversity net gain, the recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate.

Site ref: IP064a (UC064) Land between Holywells Road and Holywells Park

Site area: 1.2ha



Allocation Policies SP2

| Use(s) | | Indicative capacity |
|---------|-------------|-----------------------------|
| Primary | Residential | 66* (45dph on 100% of site) |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Plan 2017

N/A – New site (2007 preferred options UC067 was a larger site put forward for 50% residential and 50% employment uses)

Current use

Offices B1(a) and B1(c) Light industry including vehicle workshop and car sales, ancillary retail.

Development constraints / issues

The existing non-residential uses are not opposed to relocating.

The site is within an existing Employment allocation (E11) and is among the mid-performing Employment areas within the borough. There are industrial uses immediately to the west of the employment area also proposed for residential use, with vacant land to the east. There are therefore no incompatible uses.

The allocation is within 250m of the access to the safeguarded minerals facility, rather than the facility itself. The relationship with the safeguarded facility would need to be considered as part of the masterplanning and design of the allocation.

Half of E11 is potentially contaminated and therefore a contaminated land assessment would need to be carried out as part of any development. Potential for indirect impacts from water contamination, particularly during construction. The area is within a County Wildlife Site.

Approximately 50% of the site is within Flood Zone 3, although there is potential to mitigate its impact.

This area covers the former site of Fountain Cottage, a fountain, and two ponds, as shown on historic maps, which also show earlier buildings. Documentary evidence suggests that there was a water mill on the site from at least 1580, and possibly earlier (county Historic Environment Record IPS 497). It would appear that the ponds were a continuation of a water management system now within Holywells Park. Historically, the site falls within the manor of Wyx Bishop. The history of the site and the relationship it has to Holywell's Park and the earlier medieval and 16th-18th century landscape should be understood. The impacts of more modern change and use are not known. Setting impacts on Holywells Park should be considered. Desk-based assessment should be undertaken ahead of any planning application, which will inform the nature and timing of any field assessment. Conservation of historic features may be appropriate.

Redevelopment to explore scope for additional access into Holywells Park.

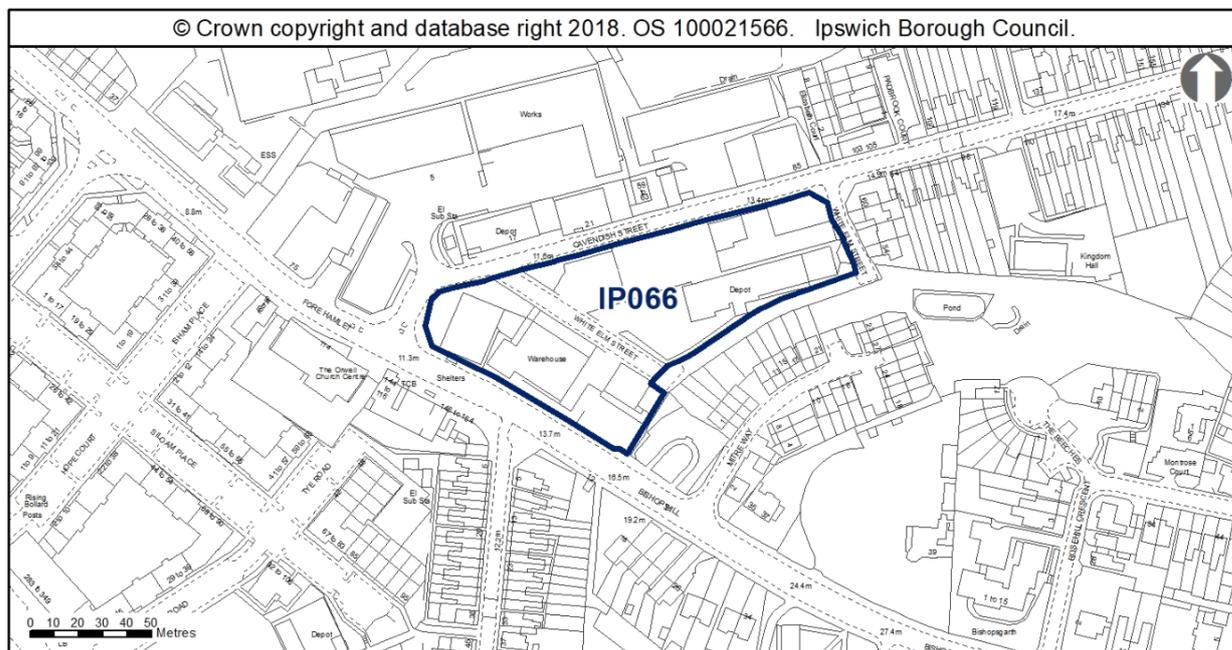
This allocation site is an area of 20th warehousing located to the immediate west of Holywells Park Conservation Area, which bounds the site with a dense covering of mature vegetation to the east. The existing units and use of the site have little street scene or townscape interest, and the redevelopment of this site with a planned development would be welcomed.

Development should be of a domestic scale so as not to be visible in views out of the Holywells Park Conservation Area to the west. The development of the site should introduce an active and engaging frontage to Holywells Road, and should take a contemporary architectural approach in design and should look to borrow architectural influences from successful nearby residential schemes, such as the Patteson Road/John Street development to the northwest. A traditional grid layout would likely be more appropriate than a cul-de-sac.

The development of the site could look to provide a new pedestrian access to Holywells Park for members of the public entering from the west. This route would need to be integral to the design of the redevelopment scheme using hard and soft landscaping to direct users through the site and to the access.

As the site is located adjacent to Holywells Park CWS the eastern boundary should be buffered from any development. There is an opportunity to strengthen the local ecological network by siting any new greenspace adjacent to the Park. Due to the proximity of the park, the lighting scheme should be designed to prevent light spillage into this area. Bats are particularly sensitive to increased light levels, so it is important to maintain dark corridors to support local ecological networks. To achieve biodiversity net gain, the recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate. The site is in close proximity to the Stour and Orwell SPA/ Ramsar site. Information to inform a project level HRA will be required to demonstrate that urbanisation impacts on the site are prevented.

Site ref: IP066 (UC069) JJ Wilson, White Elm Street
Site area: 0.85ha (includes land adjacent to the east not previously included)



Allocation Policy SP2

| Use(s) | Indicative capacity |
|-------------|---------------------|
| Residential | 55 (65dph) |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Plan 2017

100% housing at medium density – 18 dwellings on portion fronting Bishop's Hill only, land to the rear at Cavendish Street is a new site.

Current use

Warehousing, car park.

Development constraints / issues

Need to relocate the existing businesses first.

Possible contamination and close to an AQMA. Surface water flooding local to site - will need to be considered at planning application stage. See Appendix 1 of the Ipswich SFRA. Any greenspace provision should be located at the north-eastern end of the site as this is closest the area of offsite, private seminatural habitat. To achieve biodiversity net gain, the recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of

biodiversity enhancement are appropriate. It should be noted that Japanese Knotweed has been recorded on this site and this should be addressed as a matter of urgency to avoid any worsening of this and prevent spreading to other sites.

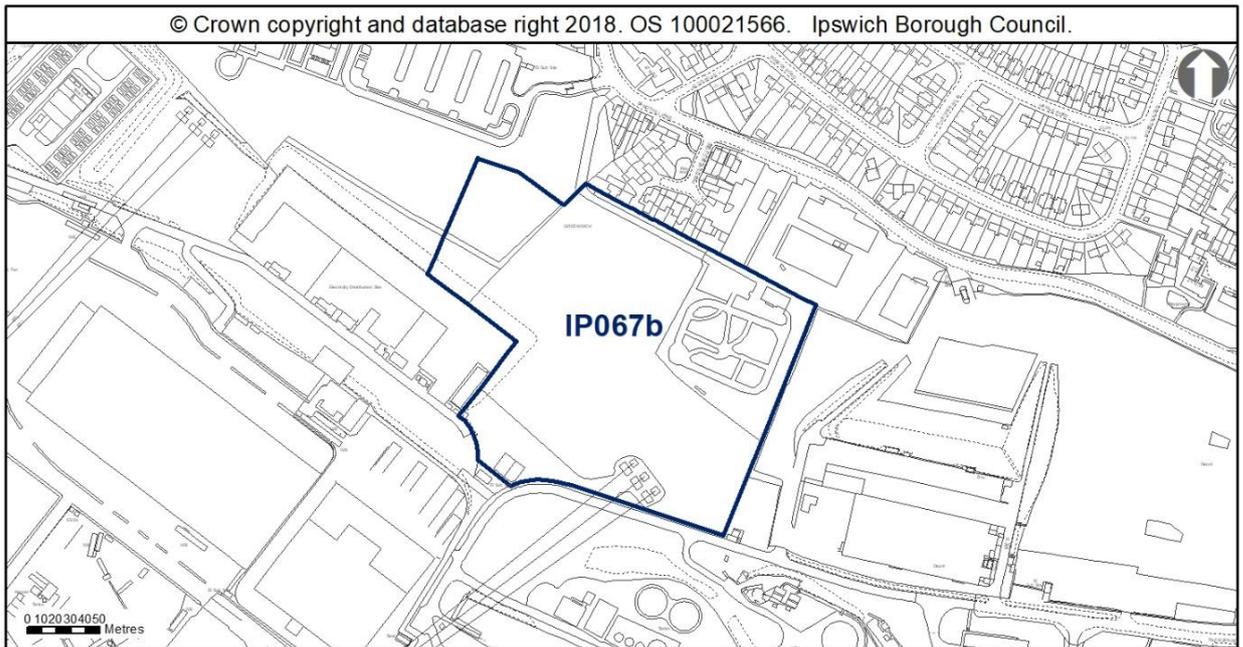
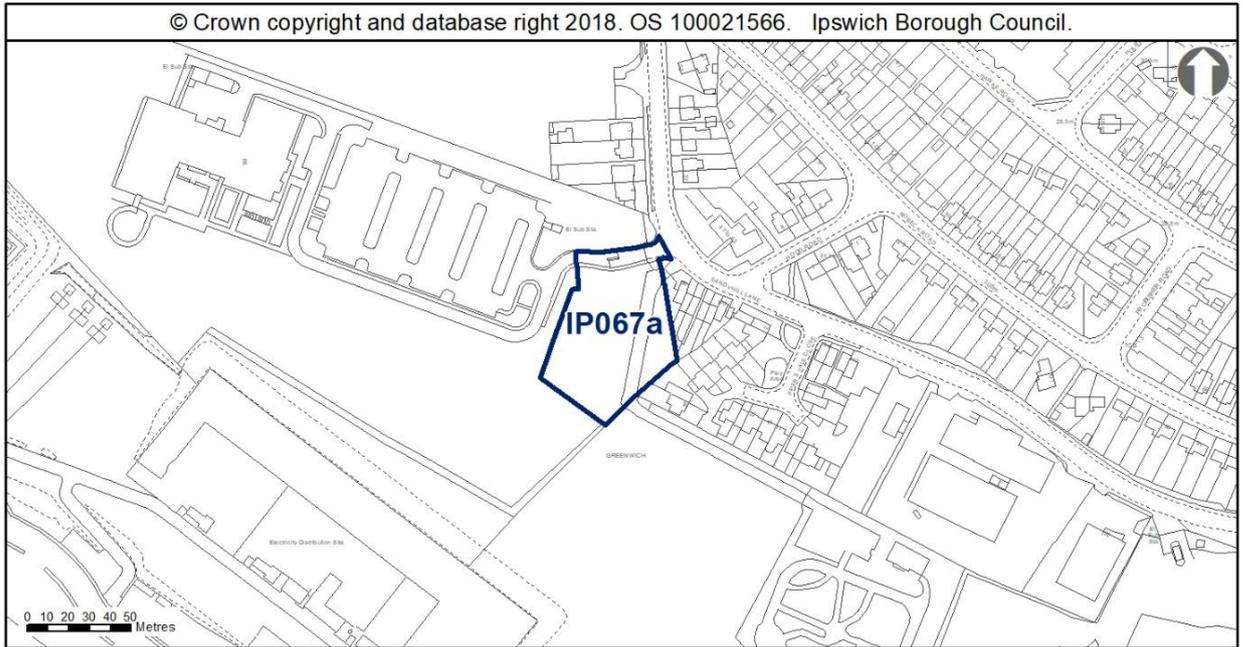
This allocation site comprises 20th century warehousing located to the east of the town centre, and bounded by dense, mature vegetation to the east, historically associated with Uplands Gate, a large listed dwellinghouse on Bishop's Hill.

Before developed for warehousing, this allocation site was occupied by terrace housing along Cavendish Street, Bishop's Hill and around White Elm Street, which once formed a grid. Reinstating housing along the historic street pattern would be encouraged in the redevelopment of this site.

Adjacent dwellinghouses are principally laid out in terraces, and so rows of terraces would be recommended, or perhaps shorter rows of 4 units if continuous terracing could not be accommodated. A contemporary design approach would be encouraged, whilst still respecting the character of 19th century terracing which borders the site on Cavendish Street. In this regard, a red brick construction would be encouraged, however the masonry bond could be experimented with to provide textured brickwork to contrast the more traditional terrace design.

Proposals would need to consider the change in topography over the site, particularly along Bishop's Hill to the south east. Stepping the height of the units would help illustrate this topographical change.

Site ref: IP067a & b (UC070) Former British Energy Site, Cliff Quay
Site area: 0.38ha Residential, 4.18ha Industrial



Allocation Policy SP2 & SP5

| | Use(s) | Indicative capacity |
|----------------|--|----------------------------|
| Primary IP067b | Industrial (B-Class) and appropriate employment-generating sui generis uses as defined through policy DM33 | 20,000sqm |

| | | |
|------------------|-------------|------------|
| Secondary IP067a | Residential | 17 (45dph) |
|------------------|-------------|------------|

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Plan 2017

Allocated for industrial uses, 20,000sqm. Site has now been split

Current use

Vacant former British Energy Site

Development constraints / issues

Access constraints, odour from the sewage works, possible contamination and the site is within a Landfill Consultation Zone. It is adjacent to a County Wildlife Site, contains part of a Local Wildlife Site and forms part of the ecological network. An ecological and reptile survey will be needed prior to any vegetation clearance and mitigation where appropriate. The site is in close proximity to the Stour and Orwell SPA/ Ramsar site. Information to inform a project level HRA will be required to demonstrate that urbanisation impacts on the site are prevented.

In terms of archaeology, this site overlooks the Orwell, in a topographically favourable location in the vicinity of prehistoric sites, particularly of Palaeolithic date. There is no objection in principle to development but any permission will require a condition relating to archaeological investigation. A desk-based assessment is recommended in the first instance.

Water infrastructure and/or treatment upgrades will be required to serve the proposed growth or diversion of assets may be required.

Surface water flooding local to site - will need to be considered at planning application stage. See Appendix 1 of the Ipswich SFRA.

A transport assessment and travel plan will be required. Development of IP067b is likely to require signal control of Landseer road/ Sandyhill Lane as a means of mitigation. Toucan facilities in signals also represent an opportunity to improve sustainable access.

The site may be suitable for housing at the northern most end, adjacent to the existing housing granted in 1999 for the adjacent site on Pipers Vale Close. This is now fully built out.

The Valley Urban Characterisation Study SPD notes that existing industrial development has encroached on areas of former meadow and foreshore, and that it is important that future development improves access and provides open space and connections to the waterside.

IP067a would be more appropriate to residential development, so that the dwellinghouses will read in the context of existing residential units at Pipers Vale Close and Sandyhill Lane, and reflect the semi-detached layout of the surrounding area.

IP067b would be better suited to larger scale industrial uses. Opportunities for robust modern design which responds to the commercial heritage of the area should be encouraged. The provision of development with green roofs could assist in providing habitats for wildlife, whilst also help softening the often utilitarian character of industrial development, and also the outlook towards the river valley.

Opportunities to create sightlines through the site and across the estuary towards the Orwell Bridge should be incorporated into development proposals where possible.

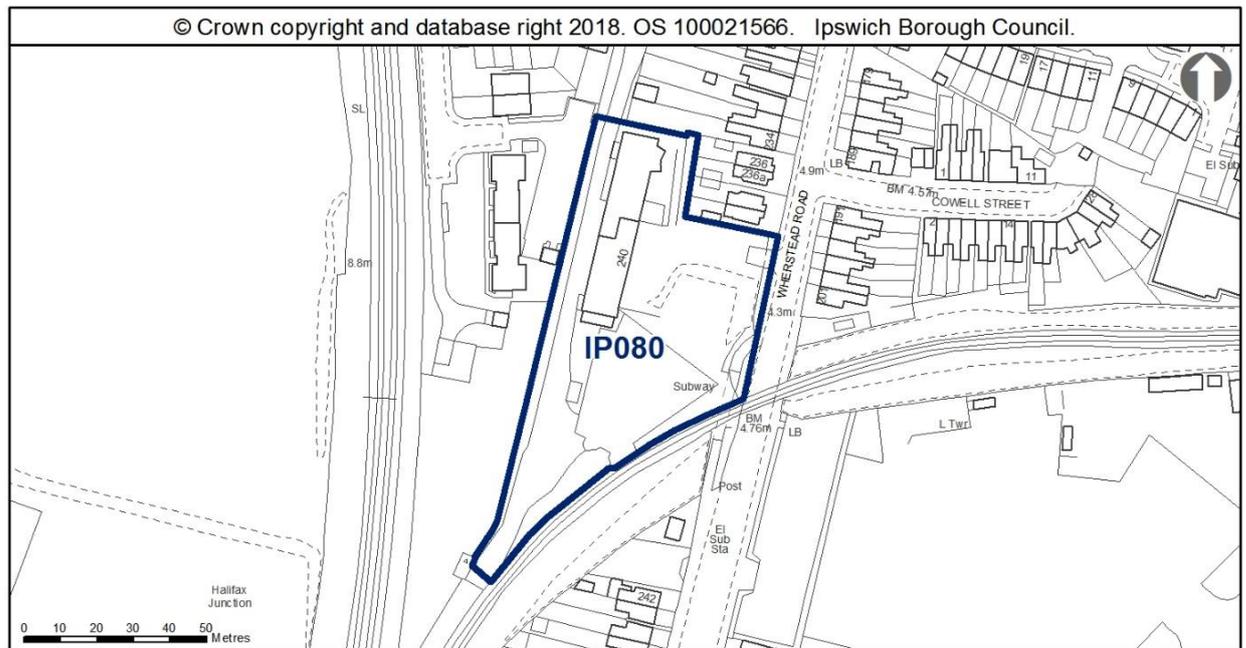
Prior to any removal of vegetation or excavation, a preliminary ecological appraisal of the site, along with any required species specific surveys will need to be undertaken. To achieve biodiversity net gain, the recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate.

Appendix 3

Appendix 3B – Site sheets (Part two)

Site ref: IP080 (UC085) 240 Wherstead Road

Site area: 0.49ha



Allocation Policy SP2

| Use(s) | Indicative capacity |
|-------------|---------------------|
| Residential | 27 (55dph*) |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Plan 2017

As above

Current use

Vacant land and buildings.

Development constraints / issues

The site is in close proximity to the Stour and Orwell SPA/ Ramsar site. Information to inform a project level HRA will be required to demonstrate that urbanisation impacts on the site are prevented.

The layout should support the wildlife corridor function of the railway line. There is an opportunity to strengthen the local ecological network by enhancement of onsite habitats adjacent to this feature. As such any residential lighting scheme should be

designed to prevent light spillage into this area. An ecological appraisal of this site should be undertaken prior to any clearance of vegetation, along with any specific surveys highlighted in the report. These are likely to include, but not be limited to, reptiles, bats and badgers. Given the nature of the site this should also encompass the potential for invasive plant species. To achieve biodiversity net gain, the recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate.

Possible contamination and adjacent to a flood zone. Also noise from the adjacent railway line. Surface water flooding local to site - will need to be considered at planning application stage. See Appendix 1 of the Ipswich SFRA.

There is no archaeological objection in principle to development, but any permission may require an archaeological condition, subject to development details. This site is in the vicinity of Palaeolithic deposits, including the SSSI of the Stoke Bone Bed (IPS 163, IPS 468). It will require desk-based assessment in the first instance.

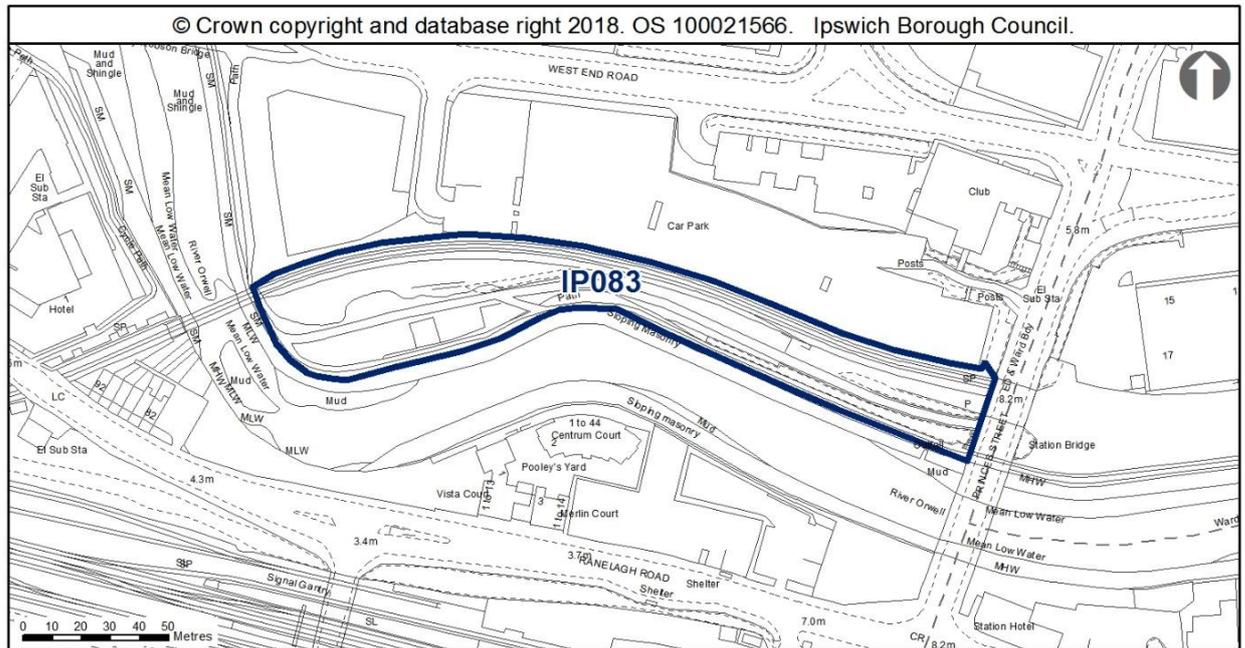
Development to explore the possibility of providing a pedestrian and cycle link through to Wherstead Road from the existing housing to the west.

The site is close to uses which are safeguarded through the Suffolk Minerals Core Strategy. The allocation is within 250m of the access to the safeguarded minerals facility, rather than the facility itself. The relationship with the safeguarded facility would need to be considered as part of the masterplanning and design of the allocation.

The site features a steep rise in topography from Wherstead Road, which would require careful consideration in the development of this site, to avoid the erection of buildings on site levels which would overshadow or dominate the existing development.

Development of this allocation site should look to improve the appearance of the subway, and the area of landscaping which surrounds it.

Site ref: IP083 (UC089) Banks of river upriver from Princes Street
Site area: 0.76ha



Allocation Policy SP6

| Use(s) | | Indicative capacity |
|---------|-------------------|---------------------|
| Primary | Public open space | - |

Adopted Plan 2017

As above.

Current use

River path, overgrown areas, disused railway tracks.

Development constraints / issues

The site is close to a County Wildlife Site (the River). A survey for reptiles is recommended if any suitable habitat is to be impacted by development. There is an opportunity to strengthen the local ecological network by encouraging further growth of scrub and grassland habitat in species poor amenity grassland areas offsite to the west. To achieve biodiversity net gain, the recommendations of the Ipswich Wildlife Audit 2019 could be incorporated, unless other means of biodiversity enhancement are appropriate.

Depending on the nature of groundworks, there may be a requirement for a condition relating to archaeological works on this land on the edge of the river.

The design will need to avoid light spillage in the river corridor.

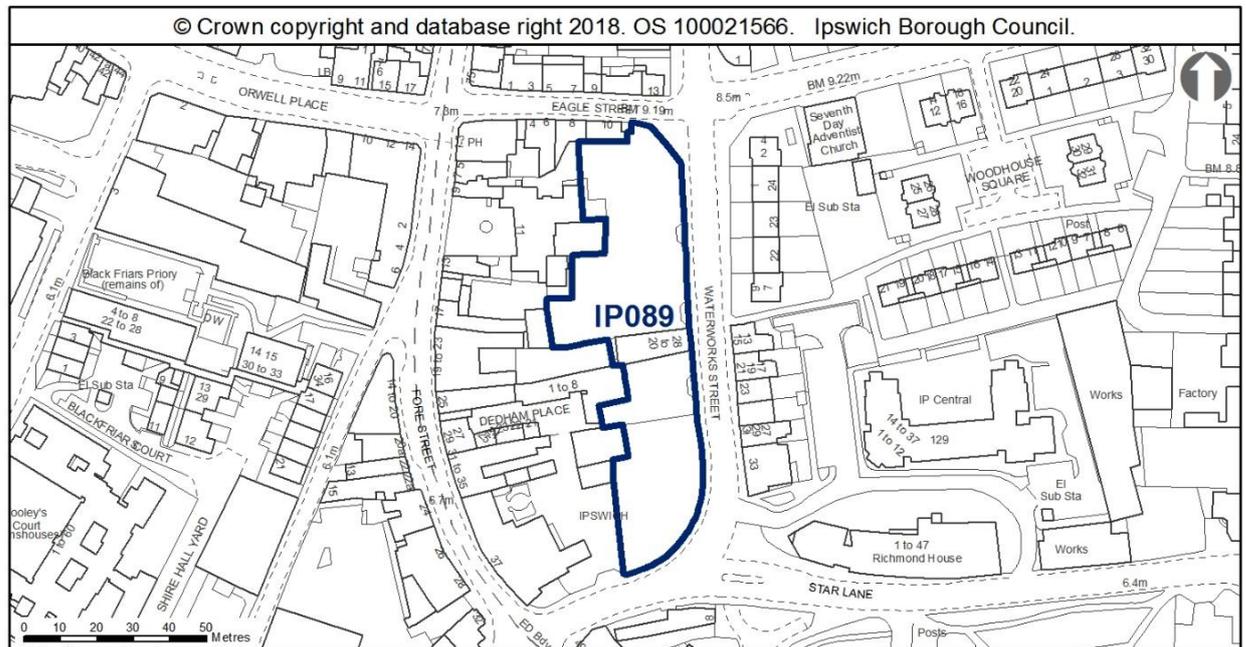
A cycle path should be provided through the southern part of the site as part of a continuous path along the River Gipping. FP61 is recorded along the site's southern and western boundaries. In accordance with the aspiration to provide cycling along the River Path, the development should support the upgrade to BR or conversion to cycle track. Sections of the route may also need physically widening.

Replacing the retaining walls covered by graffiti in particular would be encouraged, whilst wider footpaths with improved lighting and access from Princes Street bridge would also be beneficial to the appearance and amenity value of the river path.

There is presently a piece a public art, The Navigator, which occupies the western section of the site, which would need to retained or moved to the lower river path, with the space around the public art landscaped to improve the experience of the sculpture.

Site ref: IP089 (UC096) Waterworks Street

Site area: 0.3ha



Allocation Policy SP2

| Use(s) | Indicative capacity |
|-------------|------------------------------------|
| Residential | 23 (90dph* less 5 existing houses) |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Plan 2017

As above

Current use

Car park

Development constraints / issues

The site is partly within Central Conservation Area and adjacent to listed buildings. It contains a fragment of the Ipswich Ragged School, founded in 1849.

TPOs on site (an application for Tree Works may be needed), possible contamination and close to an Air Quality Management Area. May need to incorporate some parking at northern end of site to Co-op Education Centre. Surface water flooding local to site - will need to be considered at planning application stage. See Appendix 1 of the Ipswich SFRA.

This site is a large area in the Anglo-Saxon and Medieval core and within the Area of Archaeological Importance (IPS 413). Detailed early pre-application discussions with

Suffolk County Council Archaeological Service would be required to agree the scope of required assessment-and to inform design (e.g. to allow for preservation in-situ of deposits or appropriate programmes of work). Archaeological remains may be complex and important, and mitigation could involve significant costs and timescales.

Sections of this allocation site are located within the Central Conservation Area, with the excluded areas falling within the setting of the Conservation Area. The site includes the façade of the Boys Ragged School, a Victorian building, although this has been principally demolished and extended. The site is bounded to the west by the rear ranges of numerous listed buildings along Fore Street, and to the east 13-33 Waterworks Street, which is a grade II listed terrace.

More modern development generally exists to the east of the allocation site, and so this site is an important transitional area between the historic core of the town and the 20th century redevelopment of the edges of the town centre. Development proposals therefore must have regard for the sensitive historic context of this allocation site.

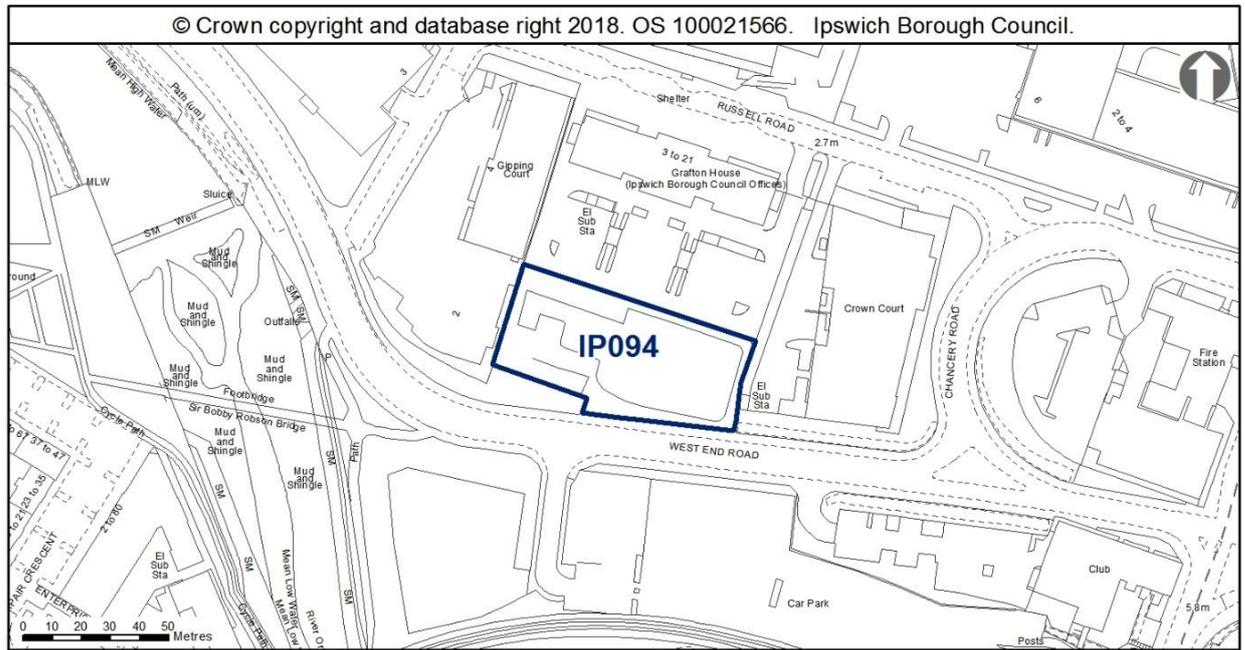
Development should look to reinstate a building frontage to Waterworks Street, respecting the 2-2.5 storey scale of surrounding development. The former ragged school should be integrated into development proposals, although a more modern design approach would be encouraged, perhaps using traditional materials in a more contemporary way. The north and south corner plots could both accommodate more of a landmark building to frame the development, however should still be of a scale which respects the scale of adjacent buildings, and historic character of the area.

Owing to the setback position of 11 Fore Street, and that the land to the east of this building has historically been undeveloped and used as the garden to the listed building, development around to the rear of this building requires particular attention, and should provide a respectable buffer to allow for breathing room between new development and the listed building.

Although this site is currently of low wildlife value, we strongly recommend the retention of the mature trees on site. In addition, the buildings could support bats and consequently further surveys are recommended. The lime tree should also be assessed for potential bat roosting features. To achieve biodiversity net gain, the recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate.

Site ref: IP094 (UC104) Land to rear of Grafton House

Site area: 0.31ha



Allocation Policy SP5

| Use(s) | Indicative capacity |
|--------------|---------------------|
| B1a (office) | 3,000sqm |

Adopted Plan 2017

As above

Current use

Vacant site

Development constraints / issues

Access constraints, possible contamination, flood risk and there is a TPO adjacent to the site (an application for Tree Works may be needed). Surface water flooding local to site - will need to be considered at planning application stage. See Appendix 1 of the Ipswich SFRA. A transport assessment and travel plan will be required. An alternative link to Constantine Road should be investigated with a potential opening up of the West End Road/ Constantine Road junction to address possible access issues from West End Road. There is no archaeological objection in principle to development, but any permission may require a condition relating to archaeological investigation, depending on the nature of the groundworks.

This site is used as car parking and for the temporary storage of building materials, and is bounded to the south by hoardings and scrub. To the south east of this building is the grade II listed Paul's Maltings, prominent in the landscape owing to its long façade and kiln. Development of this site must therefore have regard for the setting of this listed building.

The development of this site should look to enhance West End Road with an active and engaging frontage, which reflects the contemporary character and appearance of the more recent introductions to the Portman Quarter area.

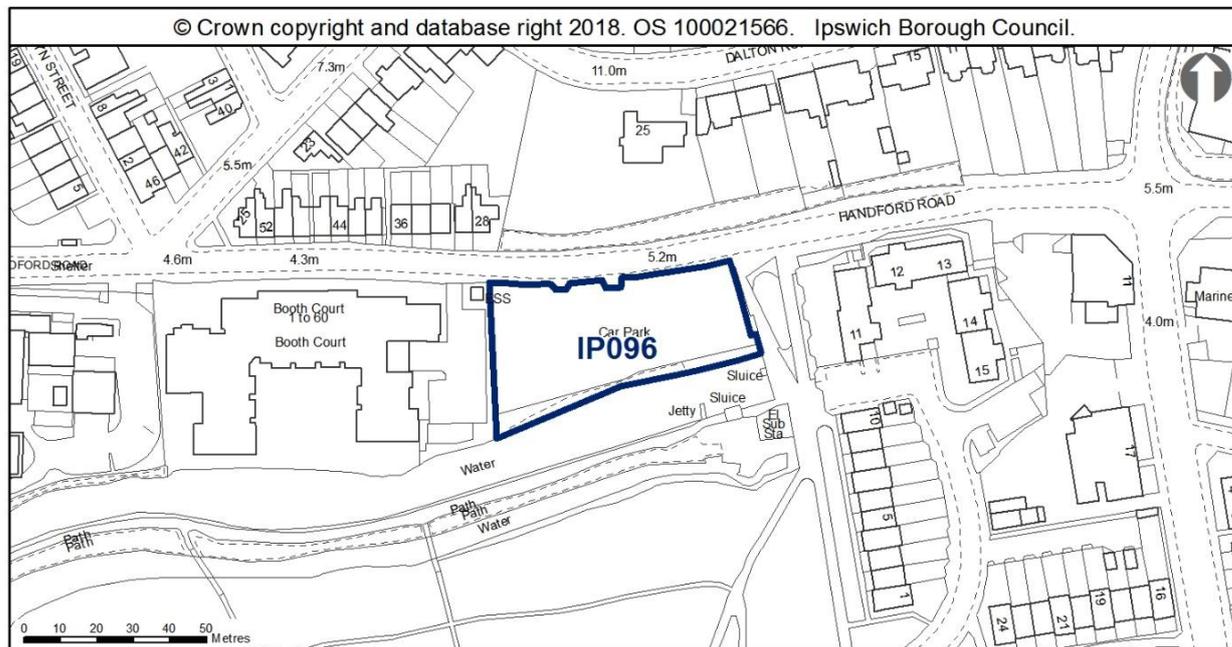
Parking should be concentrated to the rear of the site to allow the built development to have a greater street scene presence, rather than being set back by car parking.

Proposals should be sensitively designed to relate to Grafton House, so as not to completely obscure the outlook and appearance of this dual frontage building.

An ecology survey should be completed and submitted alongside an application for development.

Site ref: IP096 (UC109) Car Park, Handford Road East

Site area: 0.22ha



Allocation Policy SP2.

| Use(s) | Indicative capacity |
|-------------|---------------------|
| Residential | 22 (100dph*) |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Plan 2017

As above

Current use

Car park (temporary permission 18/00384/FUL).

Development constraints / issues

Within the Portman Quarter, housing densities should be high (Policy DM23) therefore the capacity has been increased. The development by McCarthy and Stone on the site to the west also suggests that high density may be appropriate here. Design and layout would need to support the wildlife corridor function of the canal which is also a County Wildlife Site and Local Nature Reserve. The site is part within the flood plain. There is also possible contamination on site. Surface water flooding local to site - will need to be considered at planning application stage. See Appendix 1 of the Ipswich SFRA.

This site is close to Roman, Anglo-Saxon and prehistoric excavated sites. It has a high potential for archaeological remains. Detailed early pre-application discussions with Suffolk County Council Archaeological Service would be required to agree the scope of required assessment and to inform design (e.g. to allow for preservation in-situ of deposits or appropriate programmes of work). Archaeological remains may be complex and important and mitigation could involve significant costs and timescales.

The site is opposite Burlington Road Conservation Area.

Development must take account of the River Corridor Buffer (10m) where no development should take place.

The site is located to the south of the Burlington Road Conservation Area, and is an important route when travelling into Ipswich from the west, marking the entrance to the more built up part of the town.

The north of Handford Road is generally well landscaped gardens associated with the large late 19th and early C20th dwellings built on Dalton Road, including the grounds of the grade II listed Firbank. The red brick boundary walls to the rear of the properties on Dalton Road run the length of Handford Road. The south side of Handford Road comprises more recent buildings, generally 3 storey apartment brick buildings. To the south of the application site is the canal and Alderman Road Recreation Ground. The site is bounded to the east by an area of public space which cuts through to the park, which is contemporary in its approach with a geometric central space and modern lighting scheme.

Development of the allocation site is welcomed, as a building would contribute to the street scene and infill an otherwise vacant plot.

There is scope for a more contemporary design of landmark status on this site, particularly as the site occupies a prominent corner plot. The area of public space to the east lends itself to a more modern architectural approach in the design of the building. Care should also be taken to ensure that the proposed building addresses the elevation to the east onto Alderman Road which should contribute positively to the public space. A contemporary, expressive design would have a greater contribution to the street scene, and contrast the more regular designs of adjacent C20th architecture.

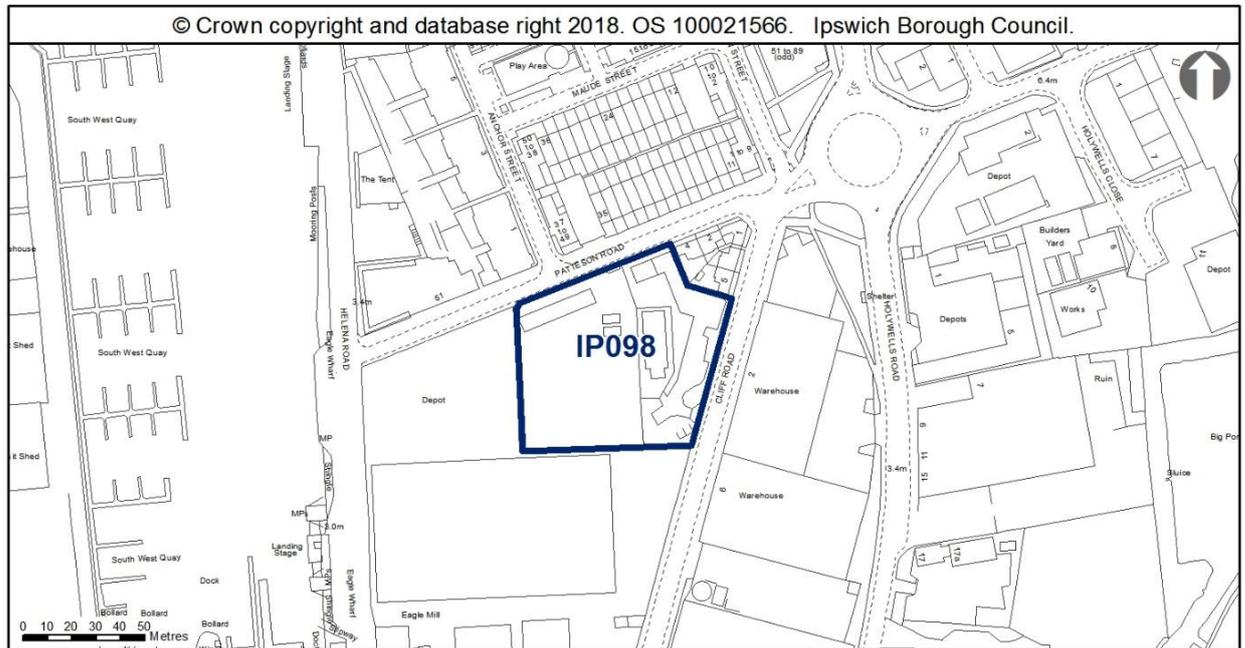
In designing the appearance and layout of the building, care should also be taken to utilise the canal to the south, addressing the canal with an expressive frontage which would be visible in views from the canal side walk.

New development should retain as much of the existing habitat as possible and integrate it within a landscaping scheme, particularly the tree belt along the southern boundary adjacent to the Alderman Canal CWS. The trees along the southern boundary should be assessed for their potential to support bat roost. The

recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate.

Site ref: IP098 (UC111) Transco south of Patteson Road

Site area: 0.57ha



Allocation Policy SP2

| Use(s) | Indicative capacity |
|-------------|--------------------------------------|
| Residential | 62 (110dph DM23 higher end of range) |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Plan 2017

As above

Current use

Gas governor.

Development constraints / issues

Need to relocate the existing gas governor first.

Flood risk, contamination and close to an Air Quality Management Area. Surface water flooding local to site - will need to be considered at planning application stage. See Appendix 1 of the Ipswich SFRA.

This site lies on the edge of the channel of the Orwell. There is potential for buried historic deposits. There is no archaeological objection in principle to development, but any permission will require a condition relating to archaeological investigation.

The site is close to uses which are safeguarded through the Suffolk Minerals Core Strategy.

This allocation site is located to the east of the Wet Dock Conservation Area, and to the west of the Holywells Park Conservation Area. The site currently holds low amenity value, with the western half containing the gas converter, and east used as a builders yard, and is bounded by a high brick wall.

The site occupies frontages onto Cliff Road and Patteson Road, with the corner of these roads being occupied by early 20th century ex-gas worker's cottages (which fall outside of the allocation site). To the north of the allocation site is a recent development which utilises traditional building forms, polychrome brick banding and timber weatherboarding which has introduced interesting architectural detailing to the area, which the development of this site should look to respond to. The use of masonry utilising textured and polychromatic brick bonds in the redevelopment of this allocation site would allow for the site to be distinctive whilst also relating to the existing development to the north.

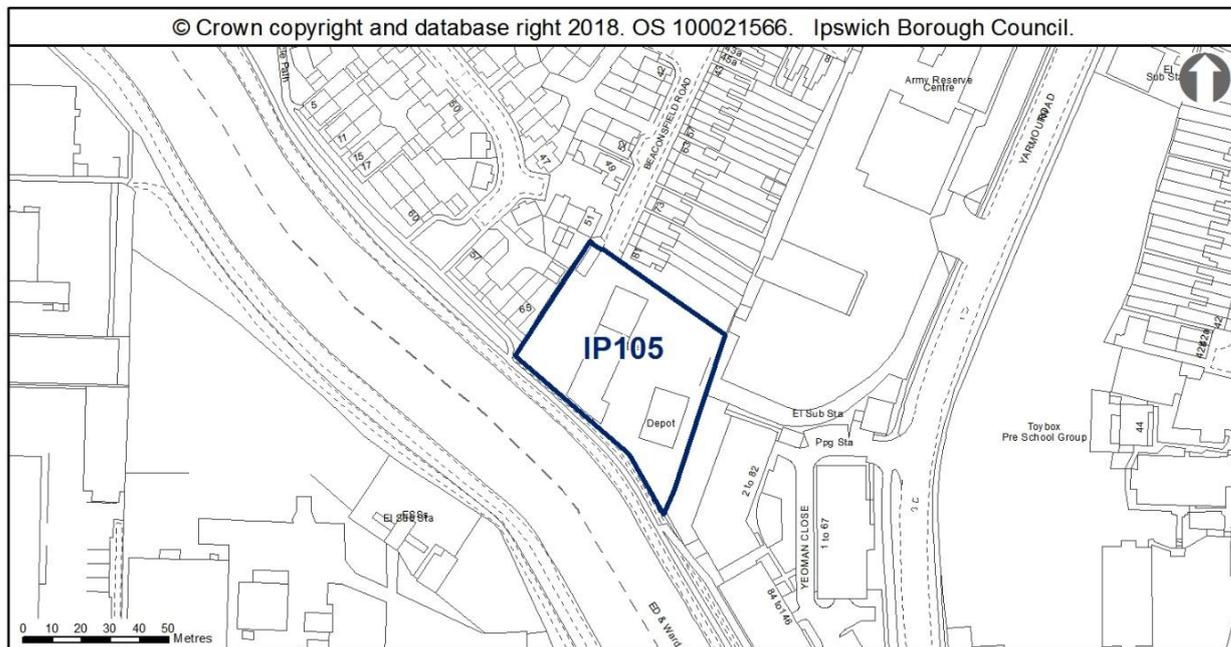
Proposals should ensure that new buildings have active elevations to both Patteson Road and Cliff Road which provide visual interest to the street scene, avoiding repetitive fenestration schemes and external finishes.

High quality design and finish of development is expected to respond positively to the character of the area, and enhance the setting of the adjacent Conservation Areas.

To achieve biodiversity net gain, the recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate.

Site ref: IP105 (UC129) Depot, Beaconsfield Road

Site area: 0.33ha



Allocation Policy SP2

| Use(s) | Indicative capacity |
|-------------|---------------------|
| Residential | 15 (45dph*) |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Plan 2017

As above

Current use

Depot.

Development constraints / issues

Need to relocate the existing business first.

The site lies within a flood zone and a Landfill Consultation Zone. Possible contamination. The design and layout would need to address the river frontage and support its wildlife corridor function. The recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate.

This site affects an area of archaeological interest, on land formerly in the low lying plains of the River Gipping. There is potential for palaeo-environmental remains to

exist. Saxon remains were recorded at IPS 395 in a similar topographic location to the site, 250m to the west. There is no objection in principle to development, but any permission will require a condition relating to archaeological investigation. A desk-based assessment in the first instance will establish impacts of past land use.

This site falls within the 91.4m height consultation zones surrounding Wattisham airfield, therefore any proposed structures which may exceed this height would need to be reviewed by the Ministry of Defence.

A cycle path should be provided along the south west boundary of the site, linking with the existing riverside cycle paths to the north and south of the site.

Development must take account of the River Corridor Buffer (10m) where no development should take place.

This allocation site is a depot located at the end of the principally residential Beaconsfield Road, an area of early 20th terraces housing to the east and more recent development to the west.

A residential use of this allocation site would be more in keeping with the character of the area, and should contribute positively to the streetscene, as well as provide an active frontage to the River Gipping, improving the southern boundary of the allocation site and its appearance from the river path.

been allocated for leisure use which is likely to be in the form of a river-related activity given the ability to access the river at this specific location. 15% of the site is allocated for employment uses and this is likely to come forward on the northern-most part of the site.

A bat survey should be carried out before any works commence. If the site is left for any significant period of time so that the northern part of the site becomes more vegetated, then a reptile survey should also be undertaken. However, woody vegetation clearance must take place outside bird nesting season (March – end of August inclusive), unless immediately preceded by a nesting bird check undertaken by a suitably qualified ecologist. To achieve biodiversity net gain, the recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate.

Depending on the nature of proposed groundworks, a condition relating to a programme of archaeological work may be applied to planning consents, with desk-based assessment and palaeoenvironmental assessment in the first instance.

Part of the site is located in Flood Zone 3 but benefits from flood defences. A Flood Risk assessment would be required for any new development on site.

Riverside cycle paths to the north and south of the site. For Development Guidelines see Opportunity Area G. FP62 is recorded running along the entire length of the site's eastern boundary.

Development must take account of the River Corridor Buffer (10m) where no development should take place. Development in this location should seek to enable greater recreational, leisure and sports use of the River Gipping, for example through the provision of 'portage' easy river access and facilities including possible canoe/ kayak/ boat storage facilities. Given the fairly static and relatively unclean quality of the water here, efforts to minimise refuse and food waste should be considered as part of this to make this section of the river more amenable to recreational and leisure use.

This site is partially vacant, with the southern area used for car showrooms and industrial uses. West End Road is an important arterial route into and out of the town, and so the comprehensive redevelopment of this site is encouraged, and should make a positive architectural statement.

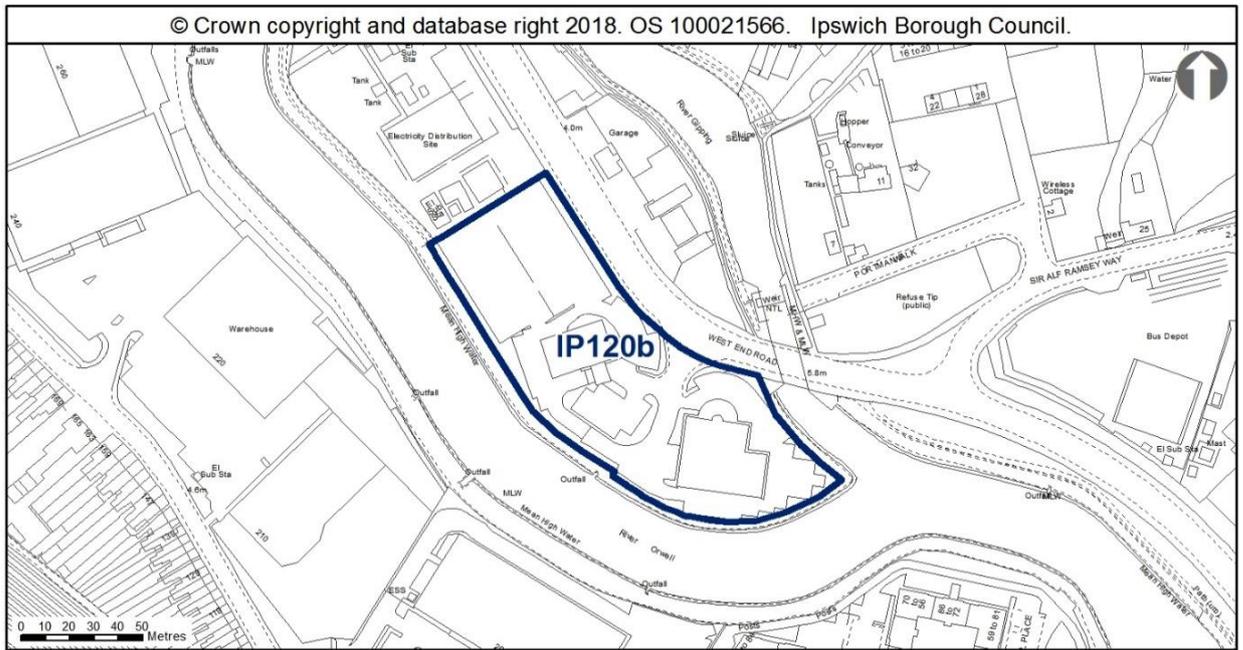
Proposals should consider the long principal frontage onto West End Road, and avoid a single scale and repetitive approach to fenestration directly adjacent to the highway. A varied approach to design and scale should help break up the massing of the proposed development, utilising smaller scale modules, fenestration and a varied

finish to provide a visually interesting scheme which would not dominate the street scene.

This site also backs onto the River Gipping Riverside Walk, and so development proposals should address this east elevation with an active and engaging frontage which contributes positively to the riverside walk and to the enjoyment of users of the river.

Development of this site should also look to incorporate biodiversity enhancements.

Site ref: IP120b Land west of West End Road
Site area: 1.03ha



Allocation Policy SP2

| Use(s) | Indicative capacity |
|-------------|---------------------------------|
| Residential | 103 (125dph on 80% of the site) |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Plan 2017

N/A – New site. Considered at Preferred Options 2007 site UC201 for 100% housing at medium density (57 dwellings).

Current use

Car showrooms.

Development constraints / issues

Existing uses would need to relocate.

Part of the site is located in Flood Zone 3 but benefits from flood defences. A Flood Risk assessment would be required for any new development on site.

Providing access to the river frontage, and relating well to nearby residential developments across the river will be requirements. For Development Guidelines see Opportunity Area G.

The adjacent River is a County Wildlife Site. It is also a key part of the Wildlife Network. There is an opportunity to strengthen the local ecological network by enhancement of onsite habitats adjacent to this feature. The scrub along the river provides valuable habitat for birds and as much as possible should be retained. Further bird surveys should be undertaken. To achieve biodiversity net gain, the recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate. Potential contamination on the site. 80% of the site assumed available to allow the layout to visually buffer the nearby substation.

Depending on the nature of proposed groundworks, a condition relating to a programme of archaeological work may be applied to planning consents, with desk-based assessment and palaeoenvironmental assessment in the first instance.

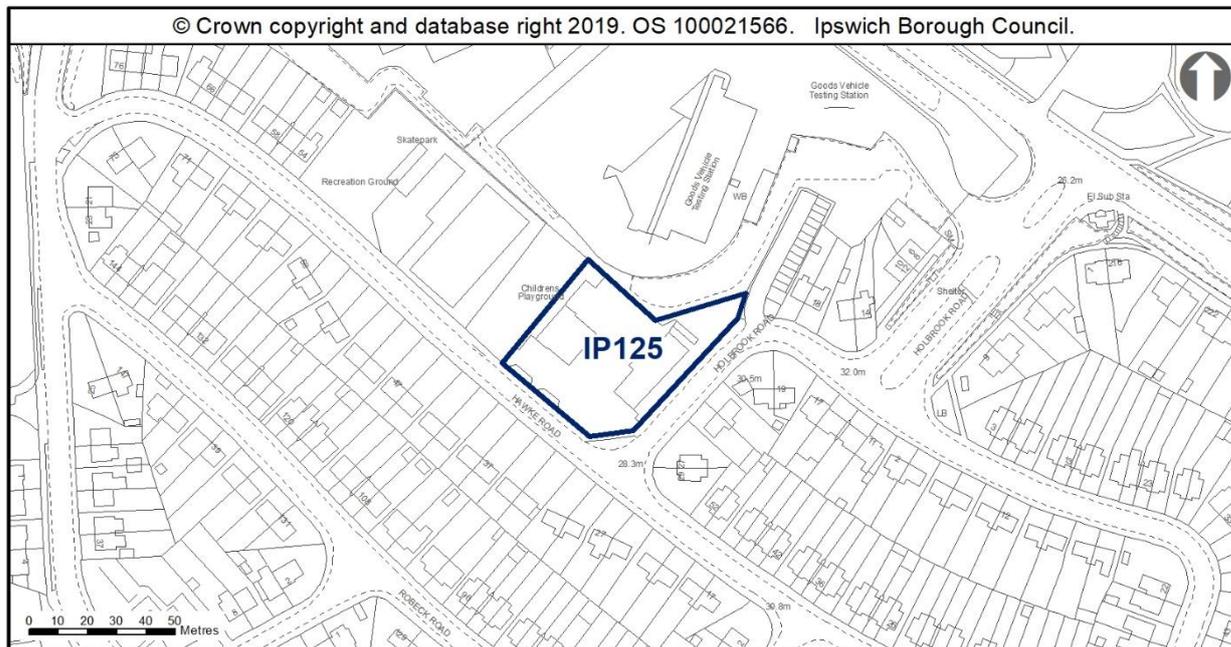
Development must take account of the River Corridor Buffer (10m) where no development should take place.

The allocation site has the potential for a contemporary landmark residential development at the southern point where the two rivers meet. Proposals would need to address both principal frontages, maximising the relationship with the River Orwell. The repetitive design and single volume massing of the flats opposite on Ranelagh Road should be avoided, with a more bespoke approach design approach being employed for this landmark site.

The electricity substation should be integrated into development proposals and be screened by a well-designed enclosure so as not to detract from the residential development of the allocation site.

A scheme should look for opportunities for biodiversity enhancements and provide a riverside walk along the Orwell to provide an island loop for pedestrian access.

Site ref: IP125 Corner of Hawke Road and Holbrook Road
Site area: 0.25ha



Allocation Policy SP2

| Use(s) | | Indicative capacity |
|---------|-------------|---------------------|
| Primary | Residential | 15 (60dph) |

* see Core Strategy policy DM23 for minimum and average densities

Adopted Plan 2017

N/A – new site

Current use

The site is currently leased for business use, but this lease is coming to an end, providing an opportunity for the site to come forward for residential use.

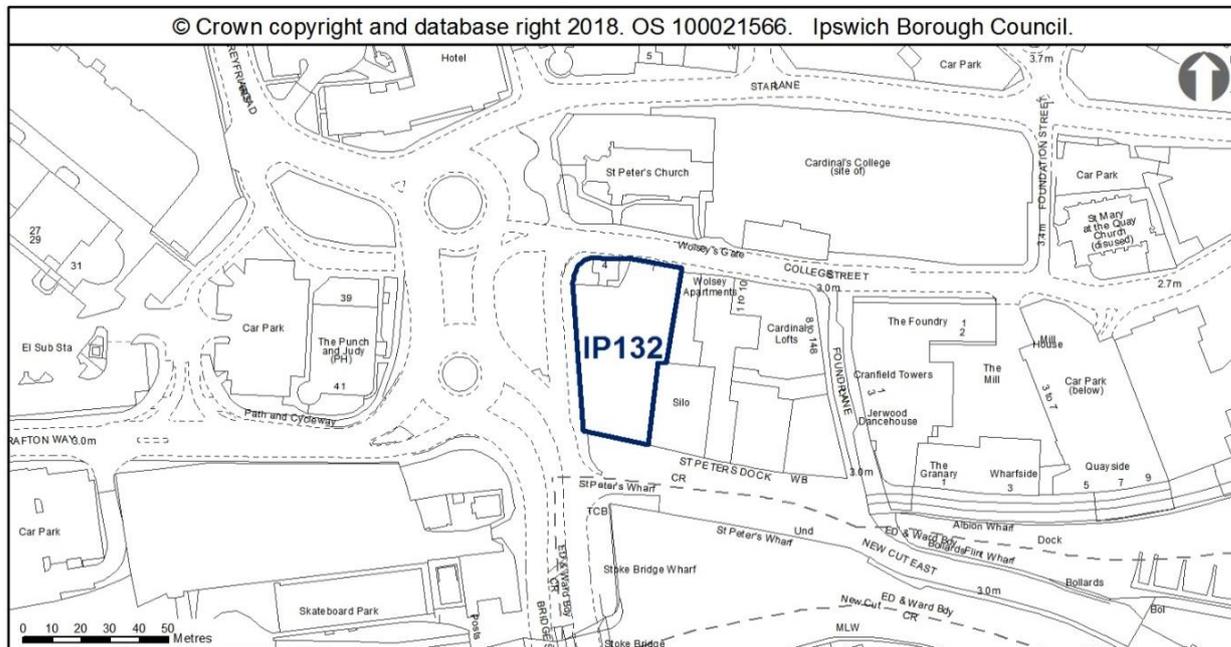
Development constraints/issues

Land to the rear has been granted permission for use as a builder’s yard, therefore it is important that existing boundary trees are retained to the rear and supplemented, and consideration is given to noise reduction measures if required.

New Planning Regulations require that the biodiversity connected with the site should be increased by 10% and that this is incorporated into any biodiversity statement and landscaping scheme connected with site development. Site development will require contribution to upgrading the adjacent play area to the site.

The site may be subject to site contamination connected with previous site uses which will require further investigation.

Site ref: IP132 Former St Peter's Warehouse, 4 Bridge Street
Site area: 0.18ha



Allocation Policy SP2

| Use(s) | | Indicative capacity |
|-----------|---|--|
| Primary | Residential | 73 (high end of high density range expected) |
| Secondary | Offices (B1a) Leisure / Retail (small scale) | Subject to layout of site - assume commercial uses on 2 lower floors |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Plan 2017

As above

Current use

Temporary car park.

Development constraints / issues

The site contains the grade II listed No. 4 College Street and lies within an area of archaeological importance and the Central Conservation Area. It is located adjacent to the Wet Dock Conservation Area and close to, and within the setting of, the grade II* listed Church of St Peter and the scheduled monument of Wolsey's Gate.

The site lies in an area of archaeological importance (IPS 413), on the Anglo-Saxon and medieval waterfront of Ipswich. This site potentially represents that last surviving section of 'early' waterfront. There is high potential for archaeological remains of possible national significance, such as important waterlogged remains and the potential remains of bridges dating from at least the 10th century.

Detailed early pre-application discussion with Suffolk County Council Archaeological Service and Historic England would be required to agree the scope of required assessment, the principle of development and to inform design (e.g. to allow for preservation in-situ of deposits or appropriate programmes of work). Where development is accepted in principle, archaeological remains will be complex and important and mitigation could involve significant costs and timescales.

The site is within Flood Zones 2 and 3 and within the Air Quality Management Area. The site is expected to require improvements to the existing water supply and foul sewerage networks to enable development.

This allocation site includes the grade II listed 4 College Street, which has been on the County's Buildings at Risk Register for a considerable period of time.

Development of this site which would facilitate the repair and reuse of 4 College Street is therefore encouraged, and proposals for the development of this allocated site should include a comprehensive scheme of repairs for this building.

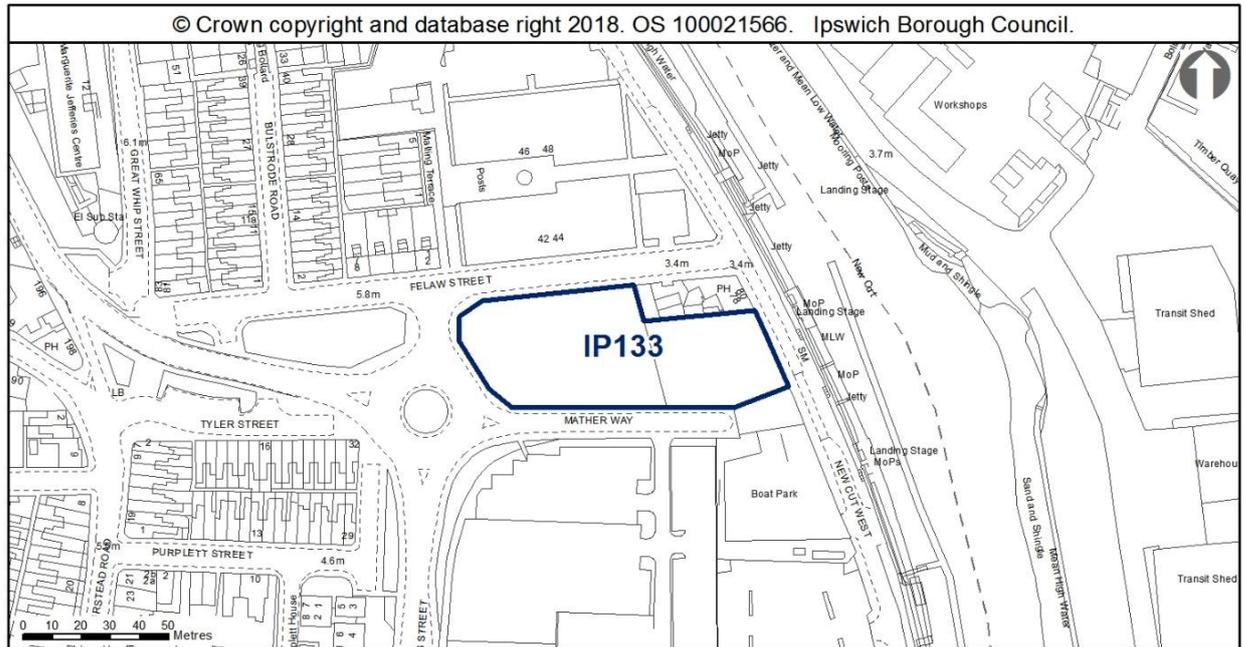
In designing a scheme for this allocation site, attention should be paid to its sensitive location, which not only includes the grade II listed 4 College Street, but also sits to the immediate south of several highly significant heritage assets including the grade II* listed St Peter's Church, grade I and Scheduled Ancient Monument of the Gateway to Wolsey's College, the grade II 1-5 College Street. In addition, the site is located within the Central Conservation Area, adjacent to the Stoke Conservation Area and the gateway to the waterfront and Wet Dock Conservation Areas. This allocation site is thus in a particularly sensitive position, and development on the site will have the potential to affect the significance of numerous heritage assets through introducing development within their setting. As such, a sensitive approach to design, scale and massing would be required in the development of a proposal.

A smaller scale development to the north of the site onto College Street would be encouraged, to reinstate an active frontage which would respect the scale of the 1-5 and 4 College Street. The development could then increase in scale southwards towards the waterfront. Any future proposals should also offer 4 College Street breathing space, and avoid overdevelopment which would give the listed building a contrived curtilage, or being dominated by over scaled development to the south. The scheme should present active frontages which address the west elevation, as well as those which would front onto College Street and the waterfront, and make a positive architectural statement acting as a landmark development which signals the gateway to the waterfront.

Development of this site should also consider the heritage and design comments provided on the allocated site references IP136 and IP035.

Site ref: IP133 (N/A) South of Felaw Street

Site area: 0.37ha



Allocation Policy SP2

| Use(s) | Indicative capacity |
|-------------|--|
| Residential | 45 (120dph based on 10/00418/VC for 47 dwellings expired Feb 2014 & pre-application discussions) |

DM23Adopted Plan 2017

As above

Current use

Grassed area.

Development constraints / issues

Site previously had permission for 47 flats and ground and first floor commercial uses B1, A3 and retail with underground car park (now lapsed). Site lies within the Ipswich Waterfront in Flood Zones 2 and 3.

This site is located adjacent to the River Orwell CWS. New development should retain as much of the existing habitat as possible and integrate it within a landscaping scheme. Greenspaces should be interlinked to provide functional ecological corridors for a range of species and as much as possible they should connect with wider offsite ecological networks, particularly the River Orwell CWS. To

achieve biodiversity net gain, the recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate.

The site is adjacent to the Wet Dock Conservation Area. The grade II listed Felaw Maltings is also located to the north of the site, occupying a prominent position on the west bank of the waterfront, whilst adjacent to the maltings is the 1914 Wm Paul Tenement Trust row of cottages. The site also wraps around the Steamboat Tavern, an early 20th century public house, which is also within the Wet Dock Conservation Area.

This site lies close to Anglo-Saxon remains (IPS 683, IPS 230) and within the Area of Archaeological Importance (IPS 413). Detailed early pre-application discussions with Suffolk County Council Archaeological Service would be required to agree the scope of required assessment and to inform design (e.g. to allow for preservation in-situ of deposits or appropriate programmes of work). Archaeological remains may be complex and important and mitigation could involve significant costs and timescales.

Surface water flooding local to site - will need to be considered at planning application stage. See Appendix 1 of the Ipswich SFRA.

On-site open space provision will be required in accordance with policy DM6.

The site is close to uses which are safeguarded through the Suffolk Minerals Core Strategy.

Development principles for the Island Site (incorporating part of the 'Over Stoke' area) are set out in chapter 6 of the plan (see Opportunity Area A).

A contemporary design approach for the development of this site would be encouraged, taking design influences from the modern architectural precedent from recent development along the west bank of the waterfront.

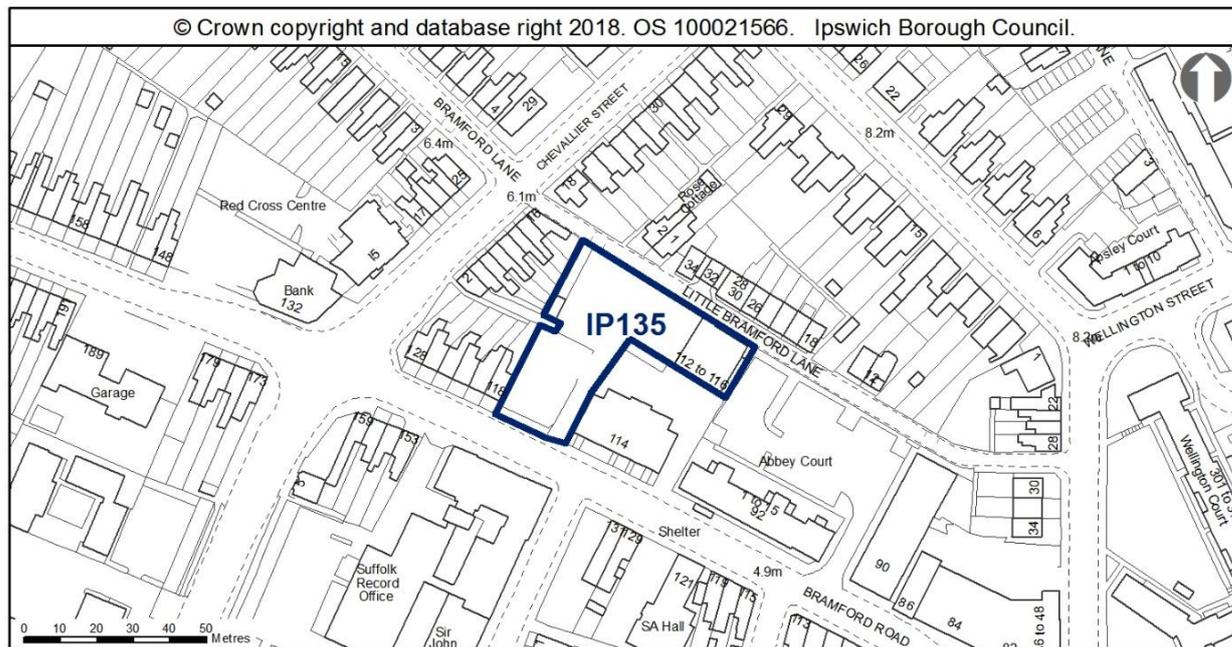
A bespoke approach to design of development on this site will be required, to both respect the height of the Steamboat Tavern which the site wraps around, whilst also being of a scale and design which has a presence which contributes positively to the appearance of the Wet Dock, visible in views across the Conservation Area. Perhaps a stepped approach to the scale of development to break up the massing of development would assist with this. If the scheme were to include a modest tower element, then perhaps the indicative housing density could be increased.

Development must also respond to the grade II maltings to the north, and be of an appropriate scale so as not to provide a tunnel effect to Felaw Street, nor harm the significance of the listed building through the introduction of development which would compete with the maltings' prominence.

The design of new development should also address Mather Way and the roundabout to the west with a distinctive, landmark design which contributes positively to the street scene and setting of the Wet Dock Conservation Area.

Site ref: IP135 (UC 250) 112-116 Bramford Road

Site area: 0.17 ha



Allocation Policy SP2

| Use(s) | | Indicative capacity* |
|---------|-------------|---|
| Primary | Residential | 19 (110dph based on location within Local Centre) |

* see Core Strategy policy DM23 for minimum and average densities

Adopted Plan 2017

As above

Current use

Car sales

Development constraints / issues

No requirement for archaeology. Previous monitoring revealed disturbance and clean sands.

Opposite a listed building.

TPOs on the site (an application for tree works may be required). Possible contamination.

The site is expected to require improvements to the existing water supply and foul sewerage networks to enable development.

This allocation site is located opposite the grade II listed Suffolk Record Office on Bramford/Gatacre Road, a 19th century former school building with prominent street frontages utilising polychromatic brickwork with decorative terracotta detailing. The allocation site also includes TPO hornbeams along the Bramford Road frontage of the site.

Development of this allocation site should provide a frontage to Little Bramford Lane, as well as Bramford Road with expressive facades which contribute positively

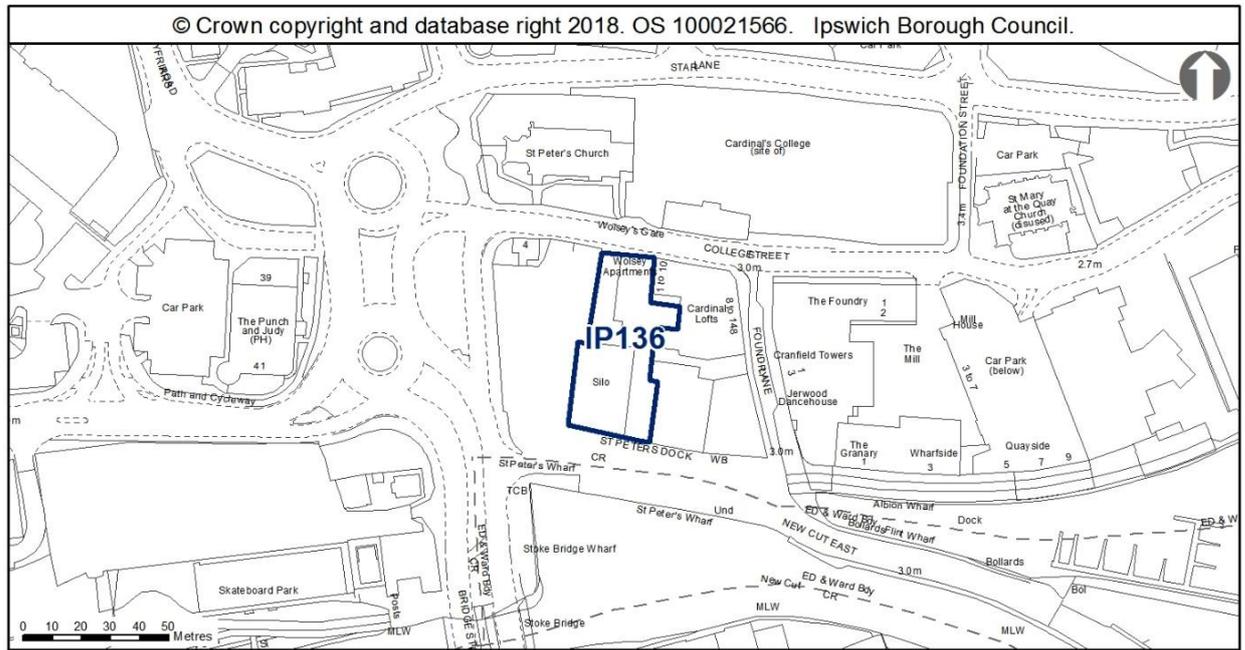
to the street scene. The design and layout of the buildings should reflect the linear layout of adjacent development, whilst the scale should reflect the existing domestic scale of adjacent terraced housing.

The TPOs to the front of the site will need to be integrated into the design proposals, ensuring that any new development along Bramford Road has an attractive outlook.

A contemporary design approach would be welcomed, perhaps interpreting the architectural influences of the Suffolk Record Office, particularly with regard to masonry and decorative/textured brickwork bonds to provide visual interest to the proposed development.

Site ref: IP136 (UC251) Silo, College Street

Site area: 0.16ha



Allocation Policy SP2

| Use(s) | | Indicative capacity |
|-----------|--|---|
| Primary | Residential | 48 (assumed 10 storey building with c. 6 flats per floor and commercial on lower two floors) |
| Secondary | Offices (B1a) / Leisure / Retail (small scale) | Scale of office and leisure accommodation subject to layout of site Some individual retail units up to 200sqm – subject to Core Strategy policy DM32 |

Adopted Plan 2017

As above

Current use

Vacant building, temporary car park.

Development constraints / issues

Access constraints, Air Quality Management Area, possible contamination, and flood risk. The site is within Central and Wet Dock Conservation Areas and opposite the grade I listed and scheduled Wolsey Gate.

This site lies in an area of international archaeological importance, on the Anglo-Saxon and medieval waterfront of Ipswich and within the Area of Archaeological Importance (IPS 413). Ground works could involve surviving sections of 'early' waterfront. There is a potential for nationally important archaeological remains outside of scheduled areas. Detailed early pre-application discussions with Suffolk County Council Archaeological Service would be required to agree the scope of required assessment and to inform design (e.g. to allow for preservation in-situ of deposits or appropriate programmes of work). Archaeological remains may be complex and important and mitigation could involve significant costs and timescales.

A transport assessment and travel plan will be required. Pedestrian capacity along College St footways, and the one-way traffic system will need to be considered.

Surface water flooding local to site - will need to be considered at planning application stage. See Appendix 1 of the Ipswich SFRA. There is a Flood Incident Report for this site.

The northern half of this allocation site is located within the Central Conservation Area, whilst the southern half occupied by the silo is found within the Wet Dock Conservation Area. The site is located to the immediate south of the grade I Scheduled Ancient Monument of Wolsey's Gate, and falls within the setting of numerous other listed buildings, including 4 College Street, St Peters Church, 1-5 College Street, and occupies an important position on the waterfront. This site is therefore highly sensitive to development and must consider the historic environment which surrounds this allocation site.

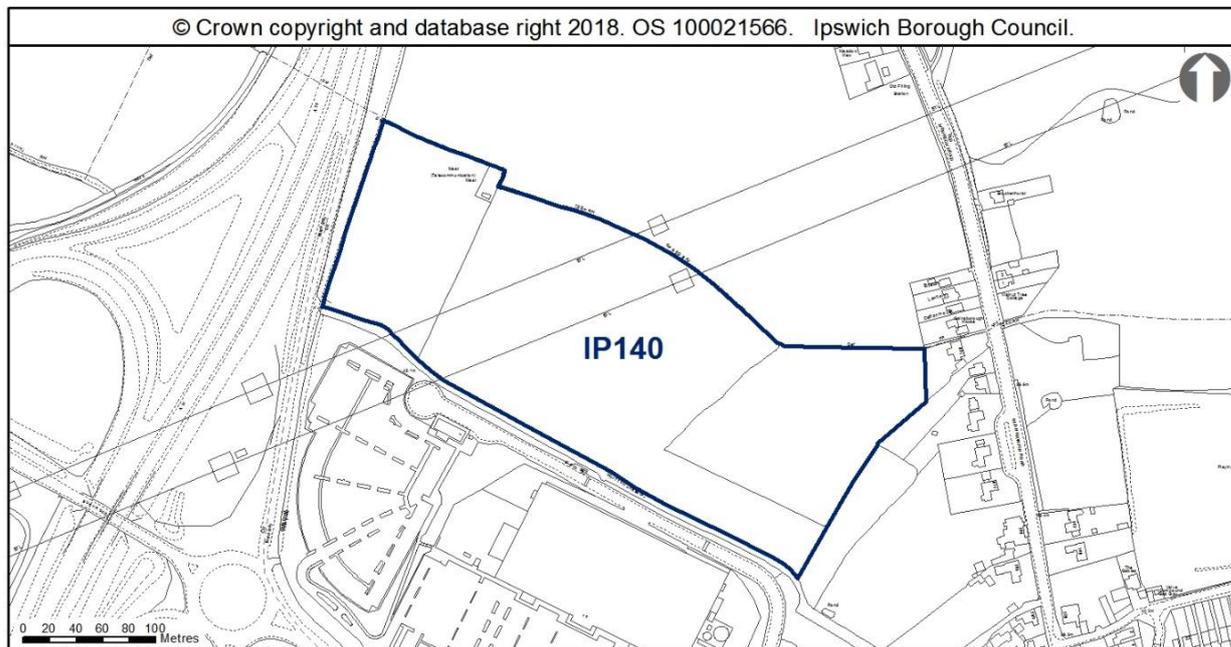
It is anticipated that the existing silo will be reused in the redevelopment of this site. The silo features the lettering R & W Paul Ltd in a mosaic tile which overlooks the waterfront and is a relic of the industrial use of the port. It is recommended that this lettering is retained in the redevelopment of this site. Other improvements to the external appearance of the building should be made, possibly with a high quality cladding system and introduction of additional fenestration, whilst still respecting the utilitarian character of the existing building and former industrial use of the area.

Development onto College Street would need to be a much more modest scale to respect the historic grouping of heritage assets which bound the site to the north. Development along College Street in particular will affect the setting of Wolsey's Gate, and the surroundings in which the gateway is experienced. A modest scale development sensitive to this historic context should therefore be employed along College Street and contribute positively to the character and appearance of the area.

Improved pedestrian access through the site should also be incorporated into development proposals to improve permeability between the waterfront and town centre. A central courtyard could perhaps be provided, possibly featuring a piece of public art/sculpture (perhaps reimagining the Digby on the side of the building).

Site ref: IP140 (UC257) Land North of Whitton Lane

Site area: 6.93ha



Allocation Policy SP5

| Use(s) | | Indicative capacity |
|---------|---|---------------------|
| Primary | Employment Park | |
| | Business (B1 including offices B1a) | 10,000 sq m |
| | Other B class and appropriate employment-generating sui generis uses as defined through policy DM33 | 10,000 sq m |

Adopted Plan 2017

As above

Current use

Grassland.

Development constraints / issues

Access constraints and need to consider impacts on rights of way. Old Norwich Road/ Bury Road and Whitton Church Lane junctions both have contributions from the

recent appeal site. It is likely that this site will need to contribute to a larger mitigation package for these junctions. Development will need to consider noise from the A14 and the need to support wildlife ecological network and recreational green corridor functions associated with the 'green trail' (see e.g. Core Strategy Policies CS16, DM8 and DM10). Detailed and up-to-date surveys are required for flora, bats, hazel dormouse, reptiles and amphibians, breeding birds and Priority species. The recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate.

The site is adjacent to the Whitton Conservation Area. Any cumulative impacts on the conservation area with the development of sites IP005 and IP032 will need to be taken into account.

Roman, Medieval and Anglo-Saxon finds are recorded in the vicinity of the site area (IPS 093). The site has been subject to geophysical survey and trenching, which has confirmed that crop marks visible on it relate to Bronze Age and Iron Age archaeological remains in particular, including a settlement with some Roman activity. Conditions on any consent would be required to secure programmes of archaeological investigation, analysis, archiving and public dissemination of information. Suffolk County Council Archaeological Service can advise on the scope of works.

A comprehensive planning approach is required with land north of the site within Mid Suffolk District.

This site falls within the 91.4m height consultation zones surrounding Wattisham airfield, therefore any proposed structures which may exceed this height would need to be reviewed by the Ministry of Defence.

Surface water flooding local to site - will need to be considered at planning application stage. See Appendix 1 of the Ipswich SFRA.

A transport assessment and travel plan will be required.

A pipeline traverses the site. It could affect the layout or require diversion. National Grid will need to be consulted on any development proposals for the site.

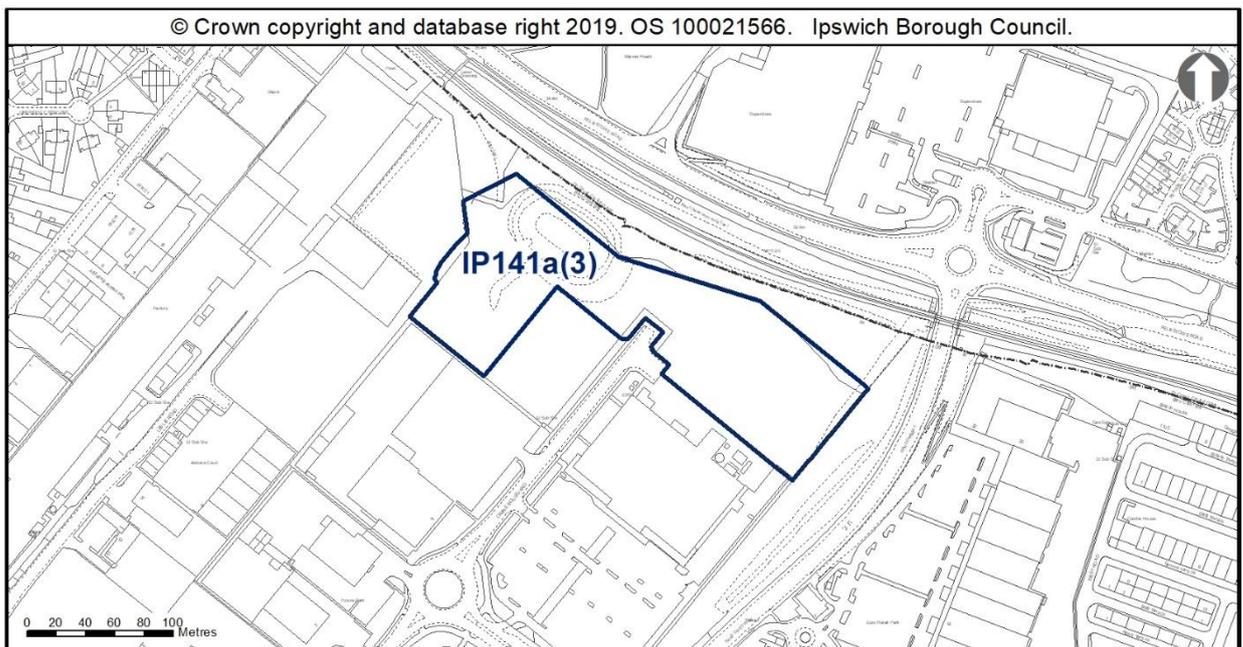
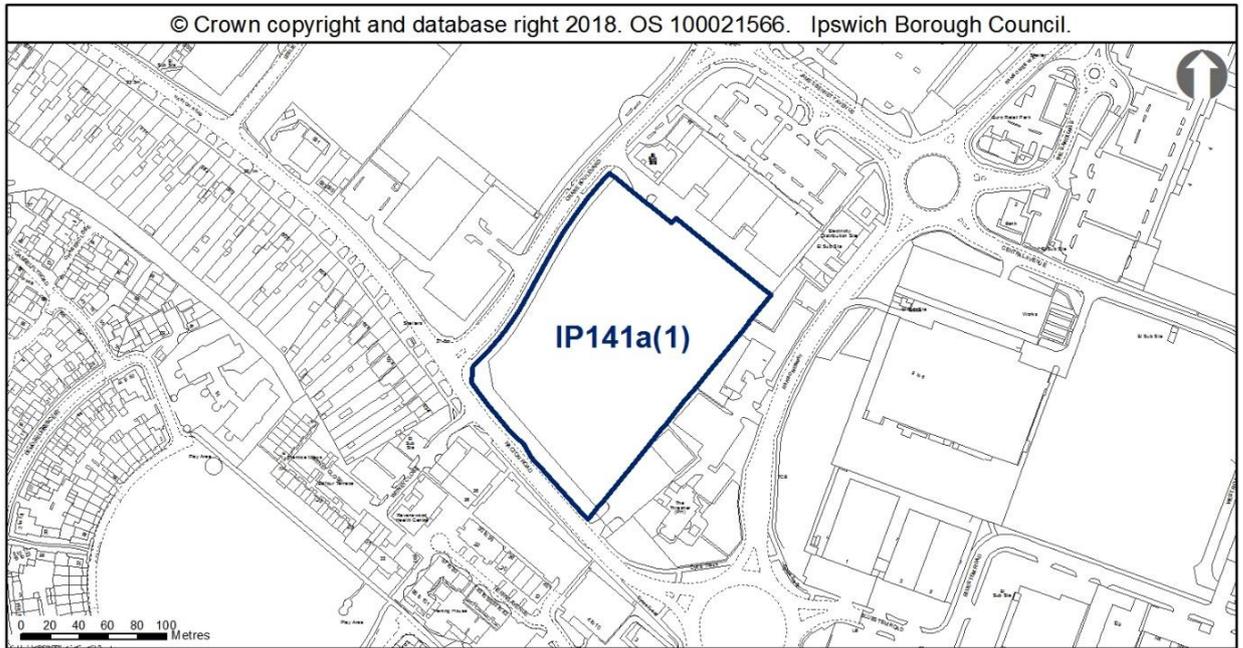
This allocation site is located to the immediate west of the Whitton Conservation Area which includes numerous listed and non-designated heritage assets in the historic settlement of Whitton. The north of this allocation site marks the borough boundary with the neighbouring Mid Suffolk District Council. To the south of the allocation site is an existing retail park which is separated from this allocation site by a public right of way, and to the west is the A14.

Owing to the rural setting of the allocation site, a sympathetic design approach could be to look at clustering smaller units to provide a layout that might be expected on an agricultural holding, such as 1.5-2 storey 'U' shaped units around courtyards, with additional linear units. Materials such as traditional clay roof tiles, red brick and timber weatherboarding would soften the impact of the development of this site on the Whitton Conservation Area.

A development scheme would need to include a comprehensive landscaping strategy to help screen new development from Whitton Conservation Area, whilst also respond to the open landscape of the neighbouring Mid Suffolk District to the north.

Site ref: IP141a (UC258) Land at Futura Park, Nacton Road (formerly the Cranes Site)

Site area: 4.78ha



Allocation Policy SP5

| Use(s) | Indicative capacity |
|---------|---|
| Primary | B-Class uses (excluding office use B1a) and appropriate employment- |
| | 18,000 sq m |

| | | |
|--|--|--|
| | generating sui generis uses as defined through policy DM33 | |
|--|--|--|

Adopted Plan

As above, but third site IP141a(2) now removed

Current use

Vacant employment land (previously allocated as a strategic employment site).

Development constraints / issues

TPOs nearby fronting Nacton Road (an application for tree works may be required). Possible contamination. Adjacent to railway wildlife corridor and buffer. Surveys for reptiles, bats and detailed terrestrial invertebrate surveys are required. The recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate.

The site is expected to require improvements to the existing water supply and foul sewerage networks to enable development.

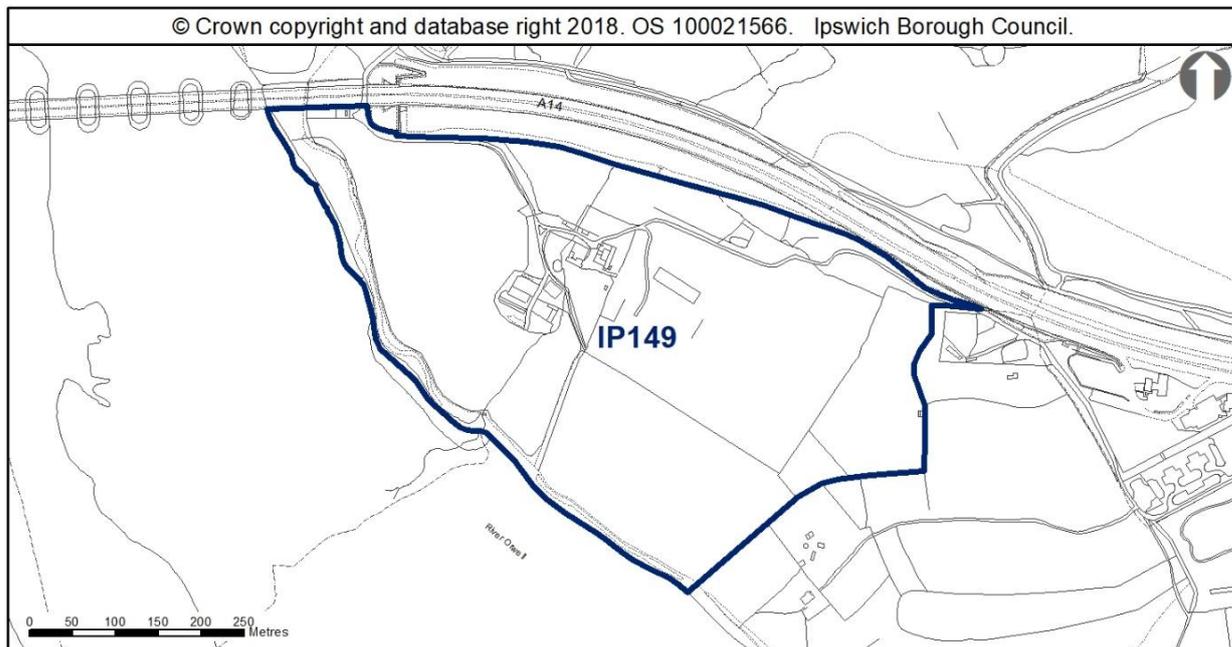
The site is likely to require junction capacity enhancements from the A1189 / Nacton Road to A1156 Warren Heath junctions. A transport assessment and travel plan will be required.

These allocation sites are located on Futura Park, an out of town industrial/retail park to the south east of Ipswich. Recent development of Futura Park has been to a high architectural standard, with units generally being illustrative of quality design, using varied cladding systems, canopies, and fenestration. It is recommended that the design of new buildings continues this architectural precedent, and avoids utilitarian single volume warehousing.

Development along IP141a should address both Nacton Road and Crane Boulevard with active frontages, and avoid being set back from the highway by extensive car parking to allow for a greater street scene impact. Development along Nacton Road should include a landscape buffer to contribute to the leafy, suburban character of the area to the west.

Site ref: IP149 (UC266) Pond Hall Farm

Site area: 24.7ha



Allocation Policy SP8

| Use(s) | Indicative capacity |
|------------------------|---------------------|
| Country Park extension | - |

Adopted Plan 2017

As above

Current use

Farm land

Development constraints / issues

Access constraints, possible contamination, adjacent to a Ramsar Site and Special Protection Area for birds. The site would need to be master planned and measures put in place to manage public usage and link routes with neighbouring sites. The impacts of proposals on the SPA will be tested.

This land lies on a slope overlooking the Orwell Estuary, close to prehistoric sites and in a topographically favourable location in archaeological terms. Suffolk County Council Archaeology Service should be consulted up front on proposals for ground works or formal landscaping.

This site allocation for the extension of the Country Park includes the grade II listed Pond Hall, several historic outbuildings listed in curtilage to Pond Hall, and also some more modern structures associated with the former use of the site for farming. The hall itself is a rendered timber framed building which dates from at least the 16th century, and is a rare survival of a remote farmhouse within the borough which has retained its intended isolated setting.

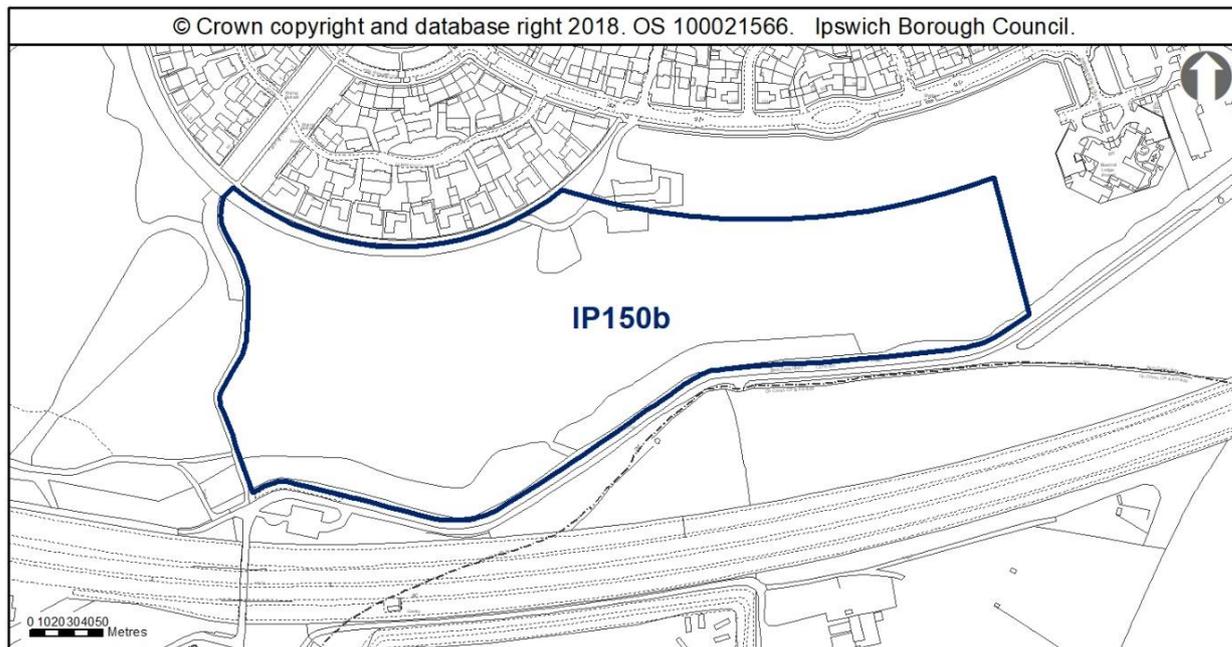
Pond Hall is presently unoccupied, and the adjacent farm buildings are also vacant and are deteriorating in condition. A new use for Pond Hall and its associated outbuildings which would complement the Country Park extension would be encouraged, even if only an informal or infrequent use, such as a field study or visitors centre.

The use of this site for the Country Park extension should look to enhance the significance of Pond Hall, through removing the modern 20th century farm buildings located within the setting of the listed building, and allow for the farmstead to be interpreted as a historic cluster buildings which illustrate the expansion of the farm throughout the 19th century.

Pedestrian routes through the Country Park extension should look to guide public access across the Pond Hall site and its associated outbuildings, to encourage a level of natural surveillance of the buildings to protect these heritage assets from damage and decay which could arise from the isolated setting of these buildings.

The proposal should be supported by an ecological appraisal and detailed surveys including, but not confined to flora, breeding birds, overwintering birds, reptiles and bats and should encompass both the farmhouse and associated buildings and surrounding land.

Site ref: IP150b (UC267 part) Land south of Ravenswood
Site area: 7.8ha



Allocation Policy SP7

| Use(s) | Indicative capacity |
|-------------|---------------------|
| Sports Park | |

Adopted Plan 2017

As above on larger site.

Current use

Vacant land, informal open space.

Development constraints / issues

Access constraints – The site will need to be master planned comprehensively with the Ravenswood allocations IP150b through to IP150e and the Airport Farm Kennels site to the south to satisfactorily address potential significant impact on junctions which are already under strain and identify any junction (capacity) enhancements needed to avoid cumulative residual severe impact. Opportunities for sustainable access improvements should also be identified, and improvements to this part of the Nacton Road corridor between junction 57 and the Ransomes Way/Nacton Road junction to create an attractive gateway to Ipswich. It should link into cycling and pedestrian route networks. Possible drainage constraints.

The site has potential wildlife interest – ecological surveys including breeding birds, reptiles and invertebrates will be needed prior to any vegetation clearance, and mitigation where appropriate. To achieve biodiversity net gain, the recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate. Also part of wildlife network and existing wildlife corridors should be safeguarded within future proposals.

The site is in close proximity to the Stour and Orwell SPA/ Ramsar site. Information to inform a project level HRA will be required to demonstrate that urbanisation impacts on the site are prevented.

This large site lies in the vicinity of Prehistoric remains and cropmarks, and as such any necessary mitigation measures for archaeology should be addressed at an appropriate stage in the planning process.

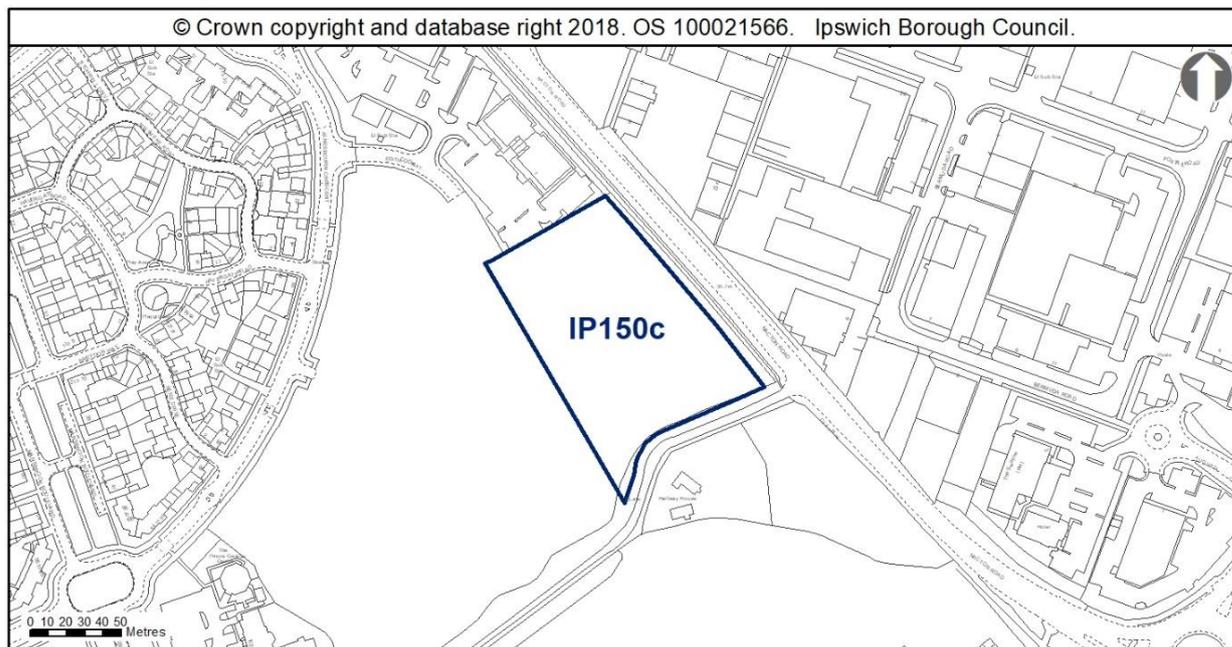
Surface water flooding local to site - will need to be considered at planning application stage. See Appendix 1 of the Ipswich SFRA.

A transport assessment and travel plan will be required.

Ravenswood has a distinctive spoke and wheel layout which allows for the residential development to blend comfortably into the heathy landscape of Ravenswood Park. The curved design of the estate acts as a soft edge to the development, leading to the rural edge of the borough. Introducing development which would reduce the distinctive legibility of Ravenswood and its relationship with this rural edge therefore requires careful consideration.

Site ref: IP150c (UC267) Land south of Ravenswood

Site area: 1.18ha



Allocation Policy SP5

| Use(s) | | Indicative capacity |
|------------|--|---------------------|
| Employment | B1 uses (research & development, light industrial uses appropriate in a residential area, excluding office use B1a) and appropriate employment-generating sui generis uses as defined through policy DM33 (subject to policy DM18) | 11,000sqm |

Adopted Plan

20,000sqm of Employment on larger allocation.

Current use

Vacant land, informal open space.

Development constraints / issues

Access constraints – The site will need to be master planned comprehensively with the Ravenswood allocations IP150b through to IP150e and the Airport Farm Kennels site to the south to satisfactorily address potential significant impact on junctions which are already under strain and identify any junction (capacity) enhancements needed to avoid cumulative residual severe impact. Opportunities for sustainable access improvements should also be identified, and improvements to this part of the Nacton Road corridor between junction 57 and the Ransomes Way/Nacton Road junction to create an attractive gateway to Ipswich.

Drainage constraints. Surface water flooding local to site – will need to be considered at planning application stage. See Appendix 1 of the Ipswich SFRA. A transport assessment and travel plan will be required.

It should link into cycling and pedestrian route networks.

The site has potential wildlife interest – ecological, reptile and invertebrate surveys will be required prior to any vegetation clearance, and mitigation where appropriate. To achieve biodiversity net gain, the recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate.

The site is in close proximity to the Stour and Orwell SPA/ Ramsar site. Information to inform a project level HRA will be required to demonstrate that urbanisation impacts on the site are prevented.

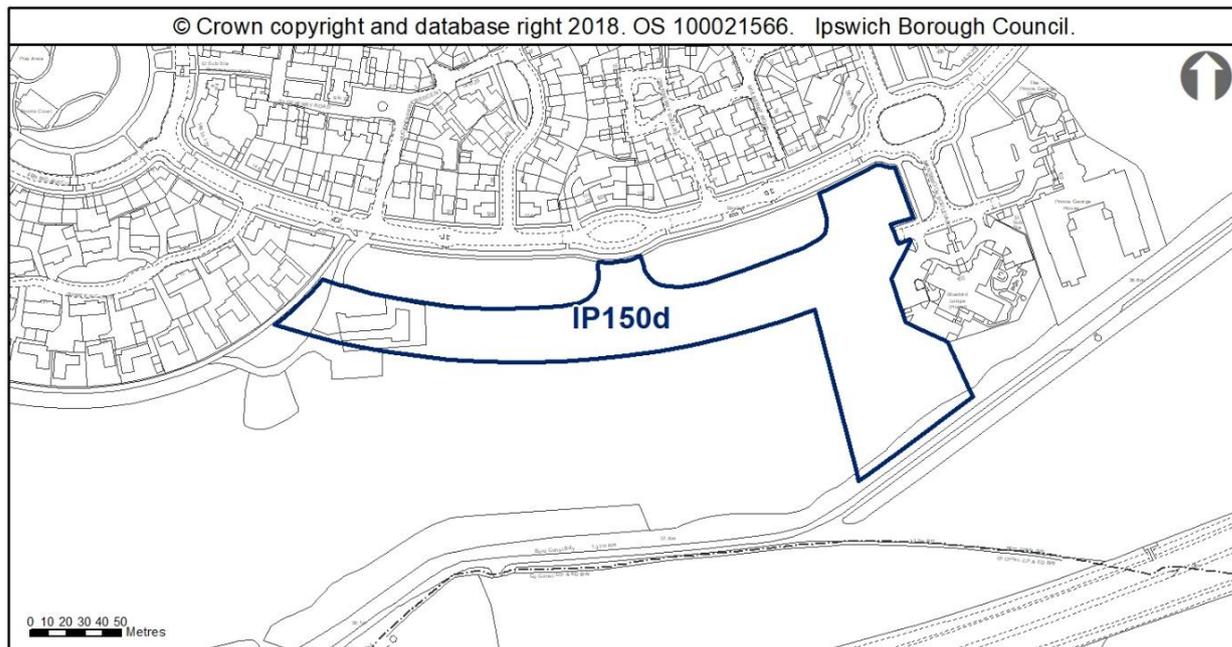
This site lies in the vicinity of Prehistoric remains and crop marks, and as such any necessary mitigation measures for archaeology should be addressed at an appropriate stage in the planning process.

Development proposals should look to address Nacton Road with an active frontage which makes a positive architectural statement to signal the entrance to the town from the east, and avoid a design and layout which turns its back to Nacton Road (like those to the north of the allocation site).

Proposals should also consider the relationship of the allocation site with IP150e, addressing any new residential development to the southwest with an engaging façade and landscaping.

Site ref: IP150d (UC267) Land south of Ravenswood

Site area: 1.8ha



Allocation Policy SP2

| Use(s) | | Indicative capacity |
|---------|-------------|----------------------------------|
| Primary | Residential | 34 (35dph on around 50% of site) |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Plan 2017

In adopted plan as the whole of IP150b allocated as sports park

Current use

Vacant land, informal open space.

Development constraints / issues

Access constraints – The site will need to be master planned comprehensively with the Ravenswood allocations IP150b through to IP150e and the Airport Farm Kennels site to the south to satisfactorily address potential significant impact on junctions which are already under strain and identify any junction (capacity) enhancements needed to avoid cumulative residual severe impact. Opportunities for sustainable access improvements should also be identified, and improvements to this part of the Nacton Road corridor between junction 57 and the Ransomes Way/Nacton Road junction to create an attractive gateway to Ipswich. Drainage constraints.

The site has potential wildlife interest – ecological, reptile and invertebrate surveys will be needed prior to any vegetation clearance, and mitigation where appropriate. To achieve biodiversity net gain, the recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate. Also forms part of wildlife network.

The site is in close proximity to the Stour and Orwell SPA/ Ramsar site. Information to inform a project level HRA will be required to demonstrate that urbanisation impacts on the site are prevented.

Surface water flooding local to site – will need to be considered at planning application stage. See Appendix 1 of the Ipswich SFRA. A transport assessment and travel plan will be required.

It should link into cycling and pedestrian route networks.

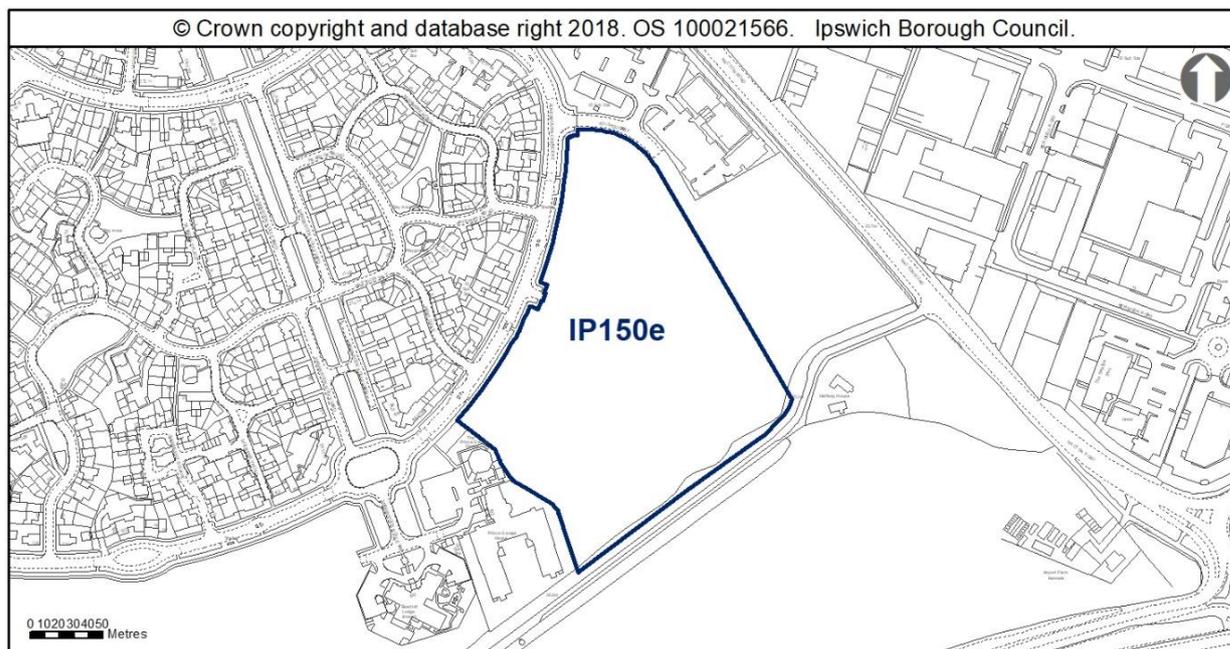
This site lies in the vicinity of Prehistoric remains and crop marks, and as such any necessary mitigation measures for archaeology should be addressed at an appropriate stage in the planning process.

New residential development could occupy the arc to the south of Alnesbourne Close, mimicking the spoke and wheel layout of much of the Ravenswood estate. These units would need to be low density, one plot deep to reflect the character of the area.

Ravenswood features distinctive pockets of development, utilising varied architectural approaches and palette of materials which gives the estate an interesting character, such as dwellings along Downham Boulevard and Cranberry Square. This individual approach should be employed on this allocation site to provide a bespoke design which contributes to the character of Ravenswood and avoid replicating some of the plainer neo-Georgian design of Alnesbourne Close to the north.

Site ref: IP150e (UC267) Land south of Ravenswood

Site area: 3.6ha



Allocation Policy SP2

| Use(s) | | Indicative capacity |
|---------|-------------|-----------------------------|
| Primary | Residential | 126 (35dph on 100% of site) |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Plan 2017

Previously formed part of site IP150c which was allocated for employment.

Current use

Vacant land, informal open space.

Development constraints / issues

Access constraints – should be master planned comprehensively with the Airport Farm Kennels site to the south and improvements to this part of the Nacton Road corridor between junction 57 and the Ransomes Way/Nacton Road junction to create an attractive gateway to Ipswich.

Drainage constraints. Surface water flooding local to site – will need to be considered at planning application stage. See Appendix 1 of the Ipswich SFRA.

A transport assessment and travel plan will be required. It should link into cycling and pedestrian route networks.

The site has potential wildlife interest – ecological, reptile and invertebrate surveys will be required prior to any vegetation clearance, and mitigation where appropriate. To achieve biodiversity net gain, the recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate. It also forms part of the wildlife network and existing wildlife corridors should be safeguarded within future proposals.

The site is in close proximity to the Stour and Orwell SPA/ Ramsar site. Information to inform a project level HRA will be required to demonstrate that urbanisation impacts on the site are prevented.

This site lies in the vicinity of Prehistoric remains and crop marks, and as such any necessary mitigation measures for archaeology should be addressed at an appropriate stage in the planning process.

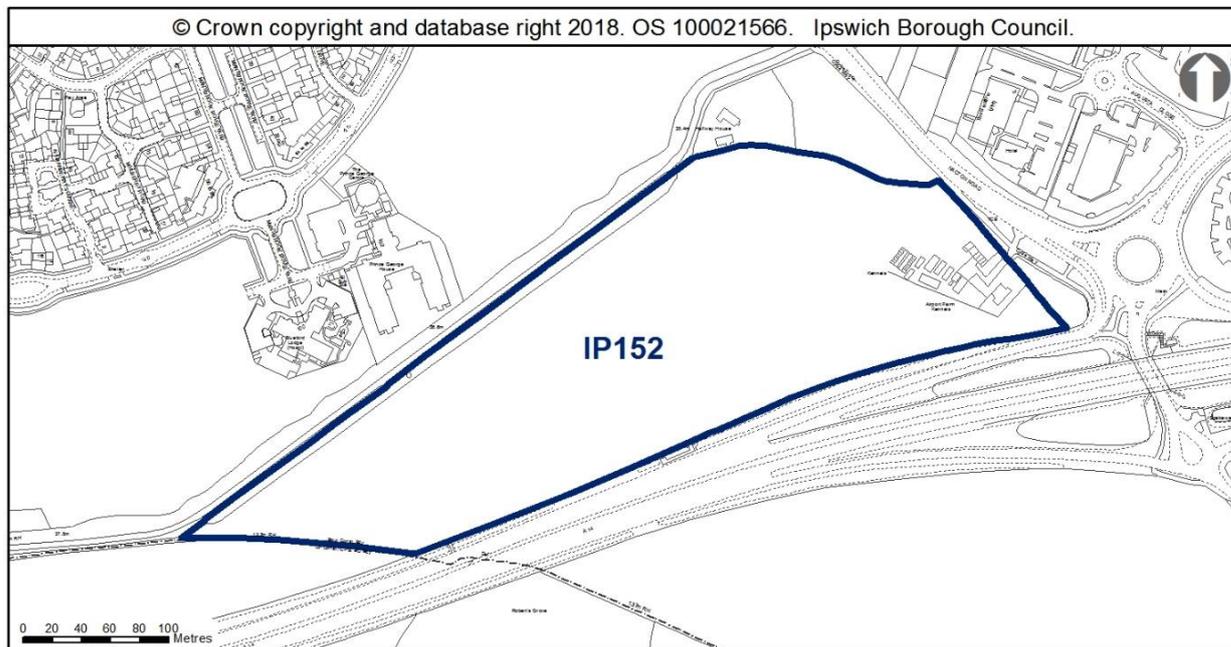
Development should respect the architectural approaches and principles previously established at Ravenswood, with its distinctive spoke and wheel layout which provides long ranging sightlines along runways, illustrating the former airport use of the site.

Ravenswood features distinctive pockets of development, utilising varied architectural approaches and palette of materials which gives the estate an interesting character. This individual approach should be employed on this allocation site to provide a bespoke design which contributes to the character of Ravenswood and avoid replicating some of the plainer neo-Georgian design of Alnesbourne Close to the north.

Ravenswood benefits from several sculptures and public art commissions, which enhance the environment in which the estate is experienced, and responds to the high quality design and aspirations of the Ravenswood development. Development of this allocation site should look to introduce further public art to well designed and integrated public space to the allocation site.

Site ref: IP152 (UC269) Airport Farm Kennels, north of the A14

Site area: 7.37ha



Allocation Policy SP5 & SP9

| Use(s) | | Indicative capacity |
|-----------|---|---------------------|
| Primary | B1, B2, B8 excluding B1a offices Appropriate employment-generating sui generis uses as defined through policy DM33 | 20,000sqm |
| Secondary | Feasibility of small section for Park & Ride to be explored | |

Adopted Local Plan 2017

As above apart from park and ride. Park and ride was previously considered at this site through the First Deposit Draft Local Plan in 2001.

Current use

Farm land.

Development constraints / issues

Access constraints – should be master planned comprehensively with the Ravenswood site to the north (IP150e) and improvements to this part of the Nacton Road corridor between junction 57 and the Ransomes Way/Nacton Road junction to create an attractive gateway to Ipswich. A transport assessment and travel plan would be a requirement for any future planning application to clarify and address highway issues.

Possible area of archaeological importance, possible contamination, TPO on site or nearby, Area of Outstanding Natural Beauty on part and noise from the A14. Development will be subject to the preparation of a development brief, which will be expected to address a range of matters including impact on landscape and the purpose of conserving and enhancing the natural beauty of the Area of Outstanding Natural Beauty.

The site has potential wildlife interest – an ecological appraisal and detailed surveys for bats, badgers, invertebrates, breeding birds and reptiles will be required prior to any vegetation clearance, and mitigation where appropriate. Mature oaks and hedges should be retained. New development should retain as much of the existing habitat as possible and integrate it within a landscaping scheme, in particular the hedgerows and mature trees along the boundaries. To achieve biodiversity net gain, the recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate.

The site is in close proximity to the Stour and Orwell SPA/ Ramsar site. Information to inform a project level HRA will be required to demonstrate that urbanisation impacts on the site are prevented.

This site includes a known Bronze Age barrow (IPS 027). This indicates that there are other prehistoric archaeological remains of high importance in the vicinity. As such any necessary mitigation measures for archaeology should be addressed at an appropriate stage in the planning process, ahead of submitting any planning application.

Water infrastructure and/or treatment upgrades will be required to serve the proposed growth or diversion of assets may be required.

This allocation site is located to the south of Ravenswood and north of the A14. The site wraps around Halfway House, one of few remaining buildings in the South East Urban Character Area which pre-date the 20th century.

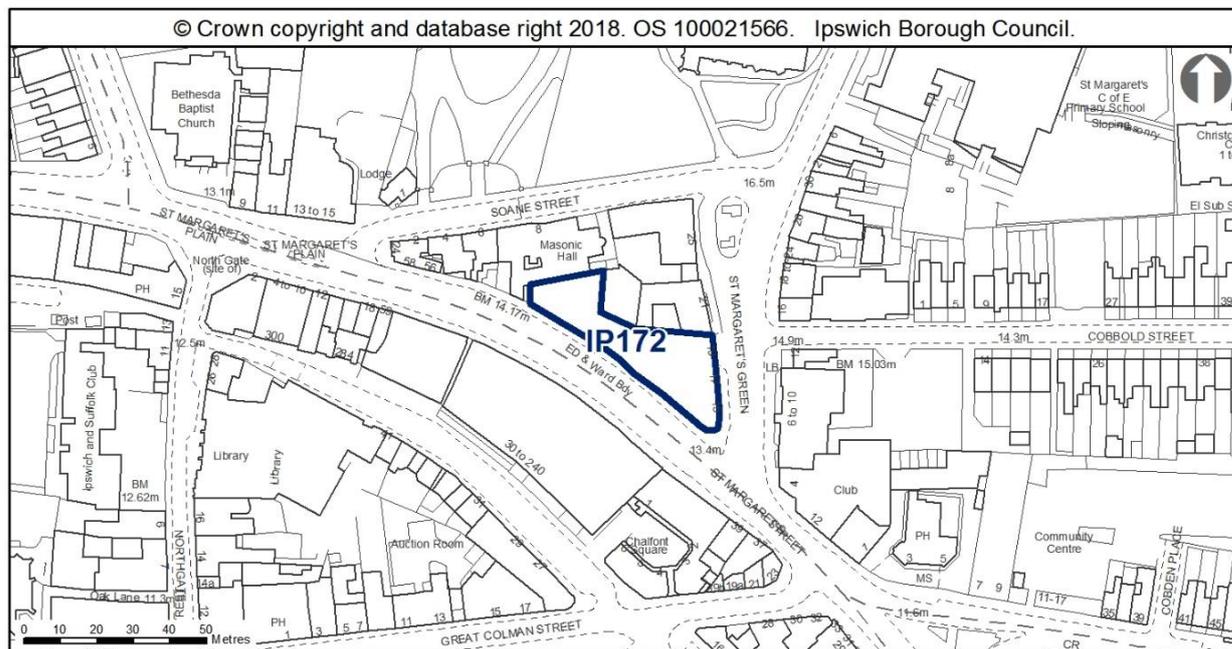
This site is bounded to the north by trees which are covered by TPOs, as well as further TPOs within the site, which will need to be considered in development proposals.

Proposals for a park and ride would need to include extensive landscaping to soften the impact of extensive hardstanding associated with car parking and bus terminals, perhaps laid out in concentric rings rather than a grid, to reflect the spoke and wheel character of the southeast of the town (seen at Ravenswood and the Nacton estate).

B1, B2 and B8 development should be well designed and make a positive architectural statement, as they are likely to be the first buildings users of the Park and Ride will see when entering Ipswich from the east. Development should respect the adjacent Halfway House, and should have a layout which allows for a buffer between this existing dwellinghouse and new development.

Site ref: IP172 (UC088) 15-19 St Margaret's Green

Site area: 0.08ha



Allocation Policy SP2

| Use(s) | Indicative capacity |
|-------------|---------------------|
| Residential | 9 (110dph) |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Plan 2017

As above

Current use

Vacant land.

Development constraints / issues

Previous permissions have not been implemented (for flats and student accommodation). The site is below the threshold size for allocation but because of its prominent location, it is considered appropriate to plan positively for the site. It is within an Air Quality Management Area and may have possible contamination. The site is within the Central Conservation Area, with a nearby scheduled monument, and adjacent listed buildings.

This part of town lies outside the Late Saxon defences, but was part of the Middle Anglo-Saxon town and is within the Area of Archaeological Importance (IPS 413) and close to a scheduled monument (NHLE 1005989). Detailed early pre-application

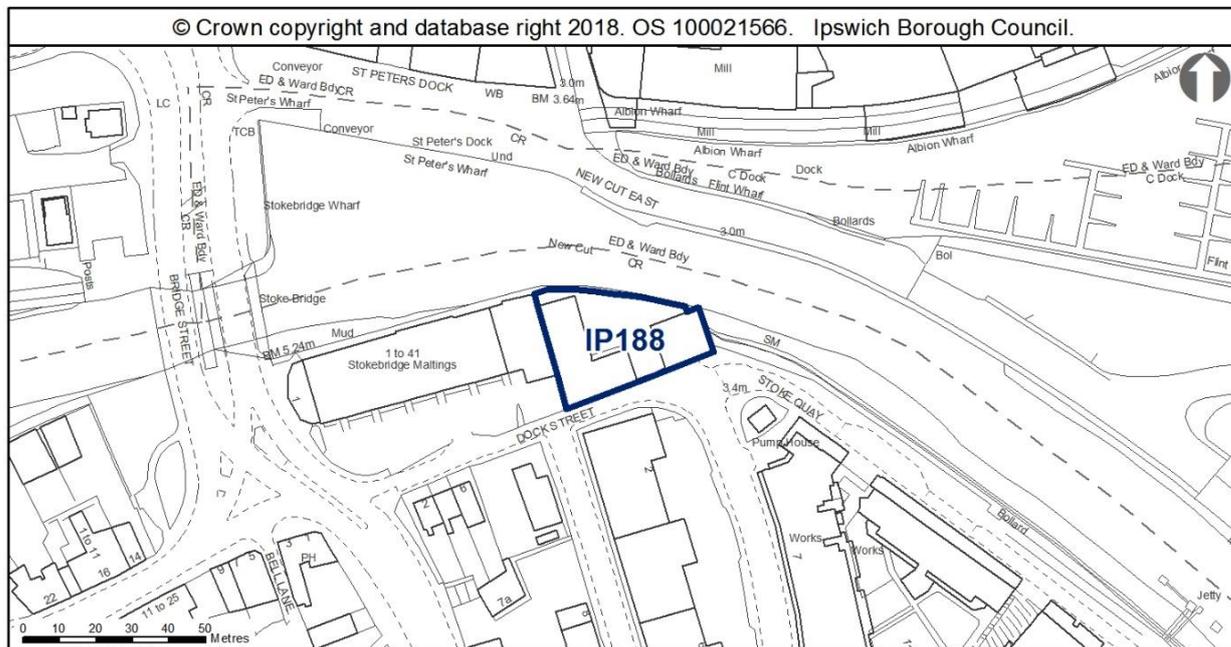
discussions with Suffolk County Council Archaeological Service would be required to agree the scope of required assessment and to inform design (e.g. to allow for preservation in-situ of deposits or appropriate programmes of work). Archaeological remains may be complex and important and mitigation could involve significant costs and timescales.

This allocation site is located within the Central Conservation Area and within the setting of numerous listed buildings, and occupies a prominent corner position on St Margaret's Green with considerable street scene potential. Development of this allocation site therefore has the potential to affect the setting of a number of designated heritage assets, as well as the character and appearance of the Conservation Area.

Development should reflect the surrounding 2.5-3 storey scale of historic buildings, and should provide an active frontage which engages with both St Margaret's Green and St Margaret's Street, with a distinctive design which turns the corner of the junction to provide a well-designed, landmark building.

Development should interpret adjacent architectural influences and materials to provide a contemporary proposal which responds to the historic setting of the allocation site.

Site ref: IP188 (N/A) Webster's Saleyard Site, Dock Street
Site area: 0.11ha



Allocation Policy SP2

| Use(s) | Indicative capacity |
|-------------|---|
| Residential | 9 (as per approved permission 19/00173/FUL) |

Adopted Plan 2017

As above

Current use

Vacant site. Planning permission for 9 flats.

Development constraints / issues

Contamination and flood risk. The site is within the Stoke Conservation Area, with an adjacent listed building. The conservation area is currently on the Heritage at Risk Register.

There is an aspiration for a cycle and pedestrian crossing to St Peter's Quay/ the Island Site immediately to the east of this site (policy SP15). To enable greater recreational, leisure and sports use of the Gipping and Orwell rivers, provision of Upper River Orwell (tidal) slipway or pontoon access and facilities including boat storage facilities should be considered.

Development would need to address the river frontage and support the wildlife corridor function of the river. Development principles for the Island Site Opportunity Area, within which the site is located, are set out in Chapter 6 of the Site Allocations and Policies plan (see 'Opportunity Area A').

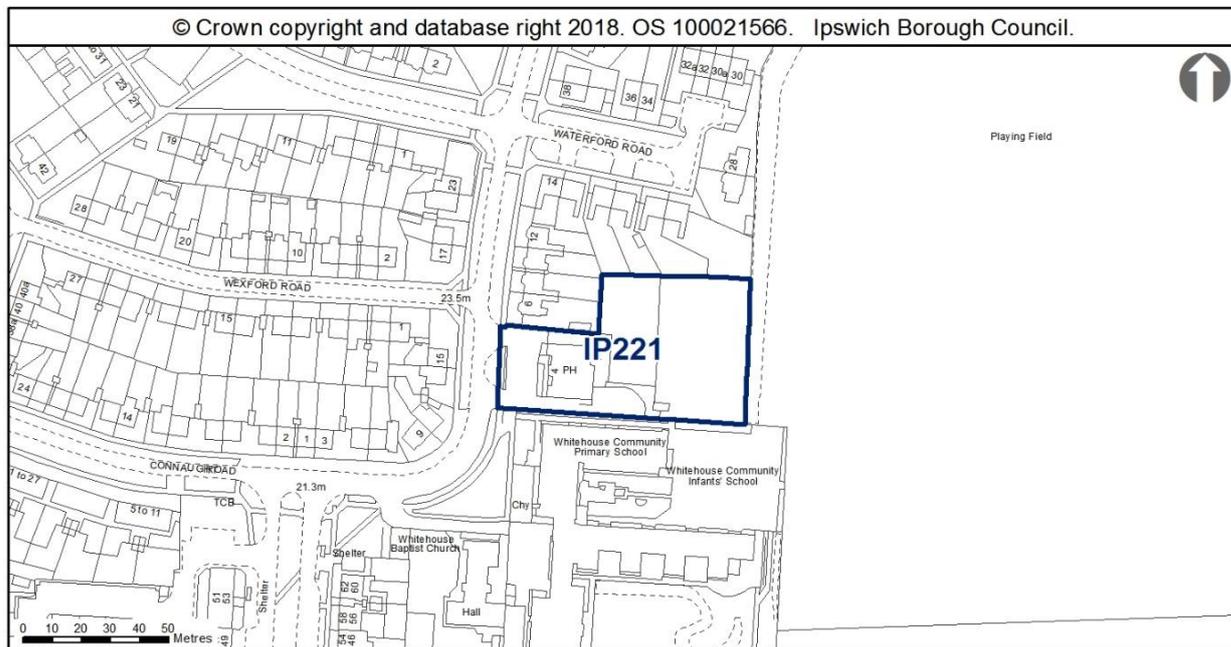
This site lies immediately adjacent to an area of Middle Saxon activity and routes leading to the river, and within the Area of Archaeological Importance (IPS413). Detailed early pre-application discussions with Suffolk County Council Archaeological Service would be required to agree the scope of required assessment and to inform design (e.g. to allow for preservation in-situ of deposits or appropriate programmes of work). Archaeological remains may be complex and important and mitigation could involve significant costs and timescales.

Surface water flooding local to site - will need to be considered at planning application stage. See Appendix 1 of the Ipswich SFRA.

Development must take account of the River Corridor Buffer (10m) where no development should take place, where practicable.

The buildings should be assessed for their bat potential prior to any demolition.

Site ref: IP221 (N/A) The Flying Horse PH, 4 Waterford Road
Site area: 0.35ha



Allocation Policy SP2

| Use(s) | Indicative capacity |
|--|---------------------|
| Residential (retaining the public house) | 12 (35dph*) |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Plan 2017

12 dwellings at 35dph

Current use

Pub and vacant land.

Development constraints / issues

Previous planning permission for 12 dwellings lapsed.

Possible access constraints, TPO on site or nearby.

This site falls within the 91.4m height consultation zones surrounding Wattisham airfield, therefore any proposed structures which may exceed this height would need to be reviewed by the Ministry of Defence.

This allocation site includes an existing public house and associated backland which is bounded by residential gardens to the north, playing fields to the east, and a school to the south. This site is located within a mid-20th century housing estate, the

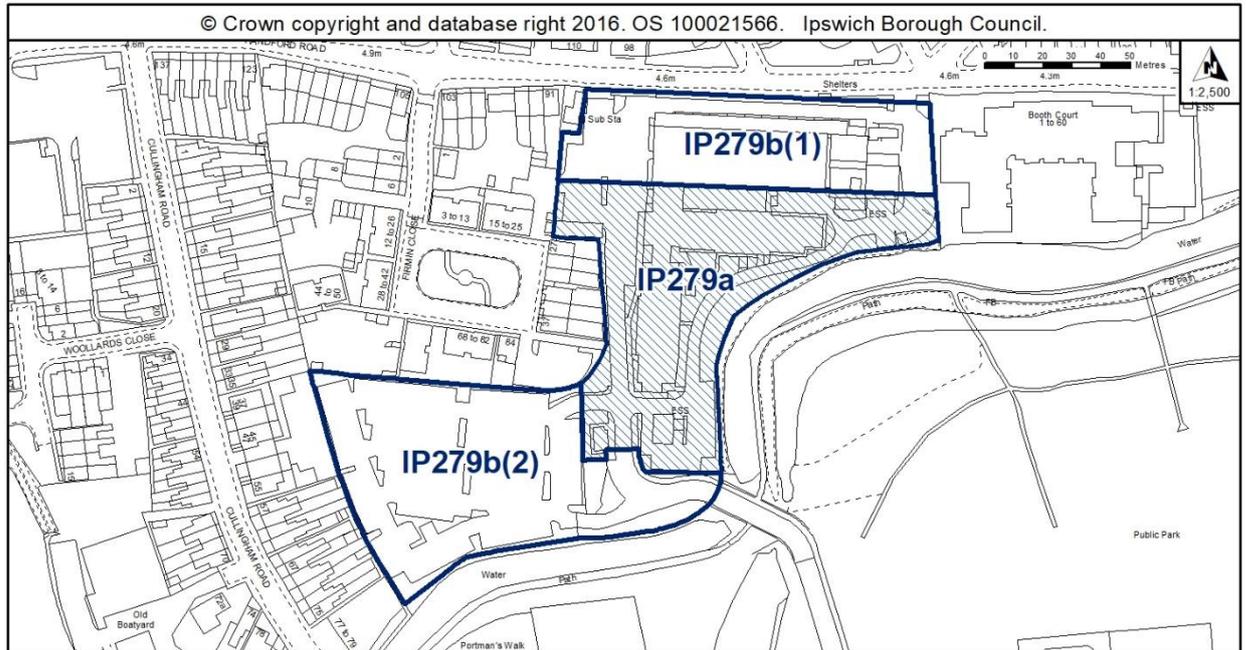
character of which is suburban with wide roads, verges and pavements; two-storey semi-detached or short terraces in brown brick with concrete tiled or pantiled roofs. The allocation site includes a willow tree which is the subject of a TPO.

Development of this allocation site should be of a distinctive design, bespoke to this backland plot which provides an interesting design which is visible in views from Waterford Road. Alterations to the public house are likely to be required to provide access to the site. This should look to enhance the appearance of the public house, whilst still respecting the mid-20th character of the building.

Further surveys should be undertaken for reptiles to inform a mitigation strategy, as slow worm is known to be on site. Consideration should also be given to the likely impact of vegetation clearance upon the local hedgehog population and ideally some of the scrub should be retained to preserve the local opportunities for hibernation. As this site is located adjacent to existing open space which in turn links to the wider ecological network, there is an opportunity to strengthen the local ecological network by retention and enhancement of onsite habitats adjacent to this feature. The recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate.

Site Ref: IP279a, b(1) and b(2) – Former British Telecom Office, Bibb Way

Site area: IP279a 0.63ha, IP279b(1) 0.44ha and IP279b(2) 0.61ha



Allocation Policy SP2, SP3

| | Use(s) | Indicative capacity |
|-----------|---|---|
| IP279b(1) | Residential | 18 (based on masterplanning work with IP279a and IP279b(2)) |
| IP279b(2) | Residential | 29 (based on masterplanning work with IP279a and IP279b(1)) |
| IP279a | Residential (SP3 – Land with planning permission) | 104 (18/00470/P3JPA) |

Adopted Plan 2017

N/A – New site

Current use

Mostly vacant offices and associated car parking. Prior approval granted on offices for 104 dwellings.

Development constraints / issues

The IP279 site has been split into three sites to reflect the approval of the recent prior approval (18/00470/P3JPA). As the two remaining parcels of land (IP279b(1) and IP279b(2)) are physically separated by the prior approval, they have been subdivided into two separate sites. The capacities indicated have been formulated on the basis of early masterplanning discussions with the site owner.

Development principles for the Upper Orwell River and Canalside within which the site is located, are set out in Chapter 6 of the Site Allocations and Policies Plan (See 'Opportunity Area G'). These include development addressing the canal frontage appropriately, and providing public cycle and pedestrian access through from Bibb Way to Handford Road.

Part of this site may be contaminated and a contaminated land assessment may be required.

Archaeological sites with remains relating to Prehistoric, Anglo-Saxon and the Roman periods in particular have been excavated in the vicinity of the development site, relating to activity on the higher ground over the town marsh and towards Handford Bridge. Further towards the marsh, deposits may have potential for organic and palaeo-environmental remains. Detailed early pre-application discussions with Suffolk County Council Archaeological Service would be required to agree the scope of required assessment and to inform design (e.g. to allow for preservation in-situ of deposits or appropriate programmes of work). Archaeological remains that may survive may be complex and important, with attendant costs and timescales. Water infrastructure and/or treatment upgrades may be required to serve the proposed growth, or diversion of assets may be required.

Surface water flooding local to site - will need to be considered at planning application stage. See Appendix 1 of the Ipswich SFRA.

A transport assessment and travel plan will be required.

County Wildlife Site and Local Nature Reserve adjacent to the site. The site may have wildlife interest an ecological and reptile survey will be needed. Development of this site should ensure that the Alderman Canal corridor and its associated habitats are buffered and enhanced. Any future green space should be sited adjacent to the canal, to complement it and the design and layout would need to support the wildlife corridor function. To achieve biodiversity net gain, the recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate.

This site contains cotoneaster and members of this group are listed on Schedule 9 of the Wildlife & Countryside Act 1981, as amended, because they can become dominant to the detriment of other species. As such, it is an offence to plant or otherwise cause these species to grow in the wild. If this plant is removed as part of a vegetation clearance programme then it should be disposed of in a way as not to contravene the legislation.

Development of IP279b(2) must take account of the River Corridor Buffer (10m) where no development should take place. Development of IP279b(2) must also include pedestrian/ cycle access along the river to allow for a continuous route to connect to site allocation IP355.

This allocation site is located to the south of the Burlington Road Conservation Area opposite an area of 19th century dwellinghouses. The site is bounded to the south by the River Gipping, and the far west by early 20th century terraced housing. The site was once occupied by Handford Hall, a large detached residence which fronted onto Handford Road situated in large grounds which included a boat house and lake.

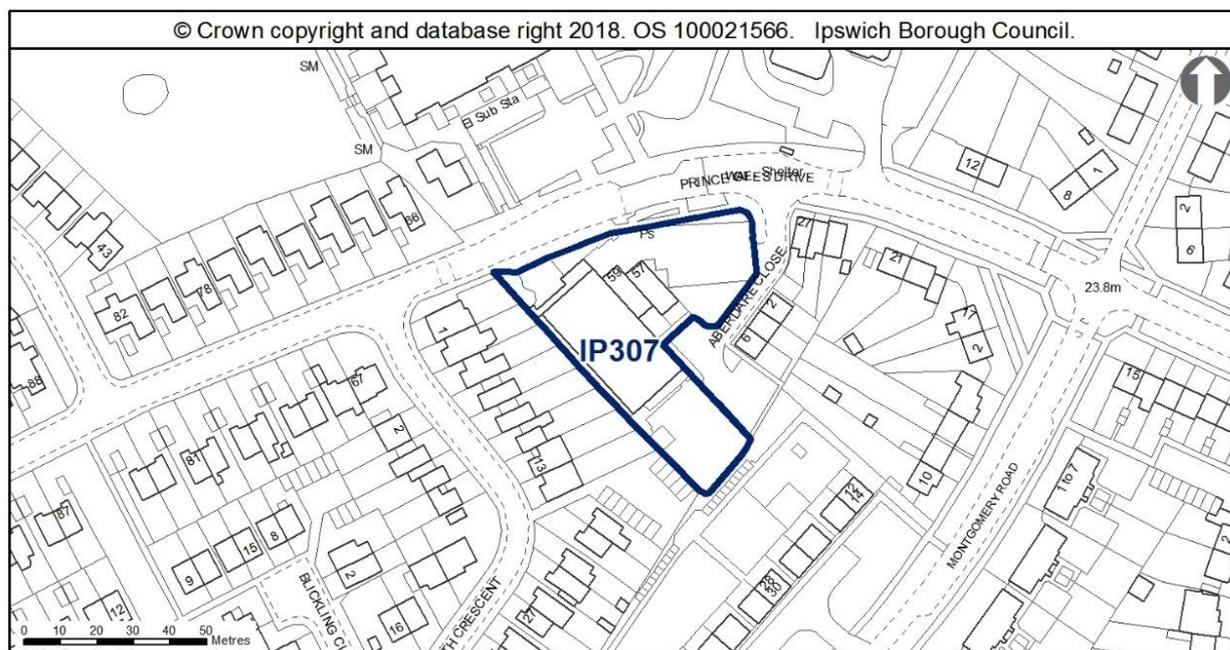
New development should respect the 2-3 storey domestic scale of adjacent buildings, and provide an active frontage onto Handford Road creating a positive architectural statement upon the entrance to the site. Proposals should also address the southern boundary which looks onto the River Gipping, and will be visible in views from the river walk.

A contemporary architectural approach would be encouraged, considering adjacent architectural influences to inform a modern development proposal, maximising views southwards over the river and recreation ground.

Development of this site should improve both pedestrian and vehicular access between Handford Road and Portman's Walk along Bibb Way to improve permeability through the town.

Site Ref: IP307 – Princes of Wales Drive

Site Area: 0.27ha



Allocation Policy SP2

| Use(s) | | Indicative capacity |
|---------|-------------|---------------------|
| Primary | Residential | 12 (45dph) |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Plan 2017

A designated local centre under DM24. New site for residential.

Current use

Retail units – Partially vacant

Development constraints / issues

A transport assessment and travel plan may be required.

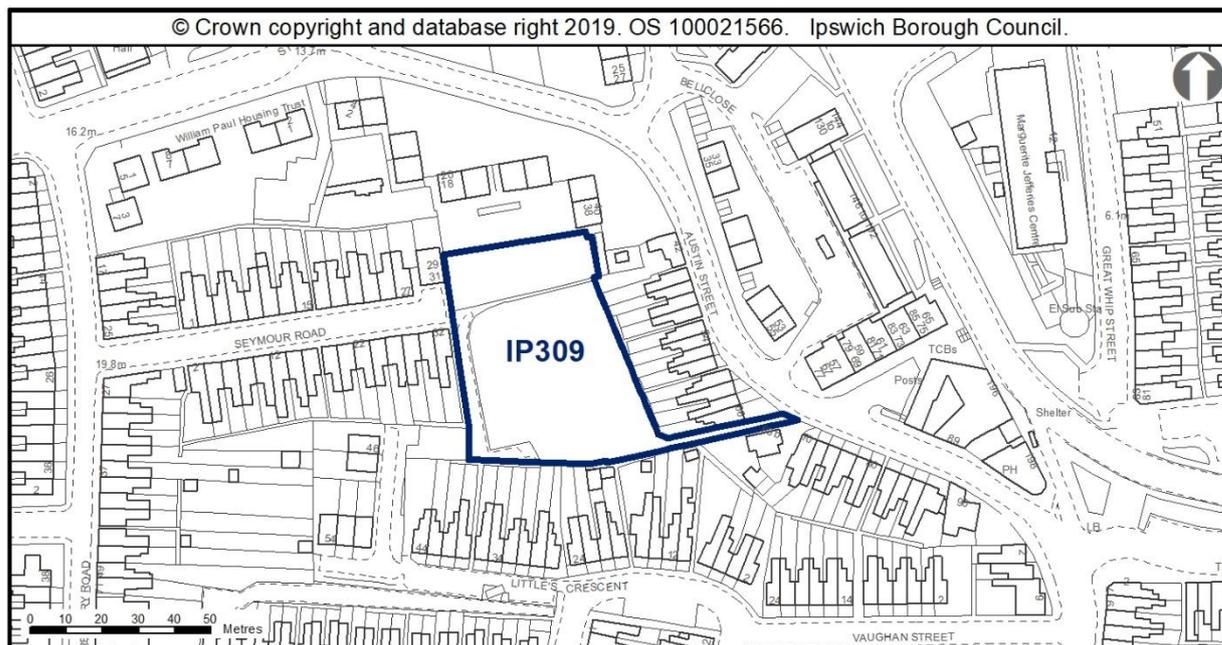
When approaching the allocation site from the west, the existing dwellinghouses on the north side of Prince of Wales Drive are characterised by distinctive monopitched roofs, whilst adjacent on Chatsworth Drive is an unusual design of two storey dwellinghouses with pitched roofs with single storey side ranges which link the houses together at first floor. The architecture surrounding the allocation site therefore includes some attempts at producing distinctive and varied designs. This innovative approach to architectural design would be encouraged in the development of this allocation site.

The position of this allocation site on the curve of Prince of Wales Road opposite Halifax Primary School as land drops away to the east does provide the site with opportunity to provide a more distinctive, landmark scheme, which would contribute to the varied architectural character of the area.

Across the site looking southeast are views over the estuary and of the Orwell Bridge. Views out of the borough across the surrounding landscape are identified in the Chanty, Stoke Park and Maidenhall Ipswich Urban Characterisation Study as being significant to this area. Although these are views which can only currently be glimpsed, the creation of new sightlines across the estuary would be encouraged to be incorporated in the development of this site.

To achieve biodiversity net gain, the recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate.

Site Ref: IP309 - Former Bridgeward Social Club, 68a Austin Street
Site Area: 0.28



Allocation Policy SP2

| Use(s) | | Indicative capacity |
|---------|-------------|--|
| Primary | Residential | 15 (54dph Higher end based on proximity to local centre) |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Plan 2017

N/A – New Site.

Current use

Vacant social club.

Development constraints / issues

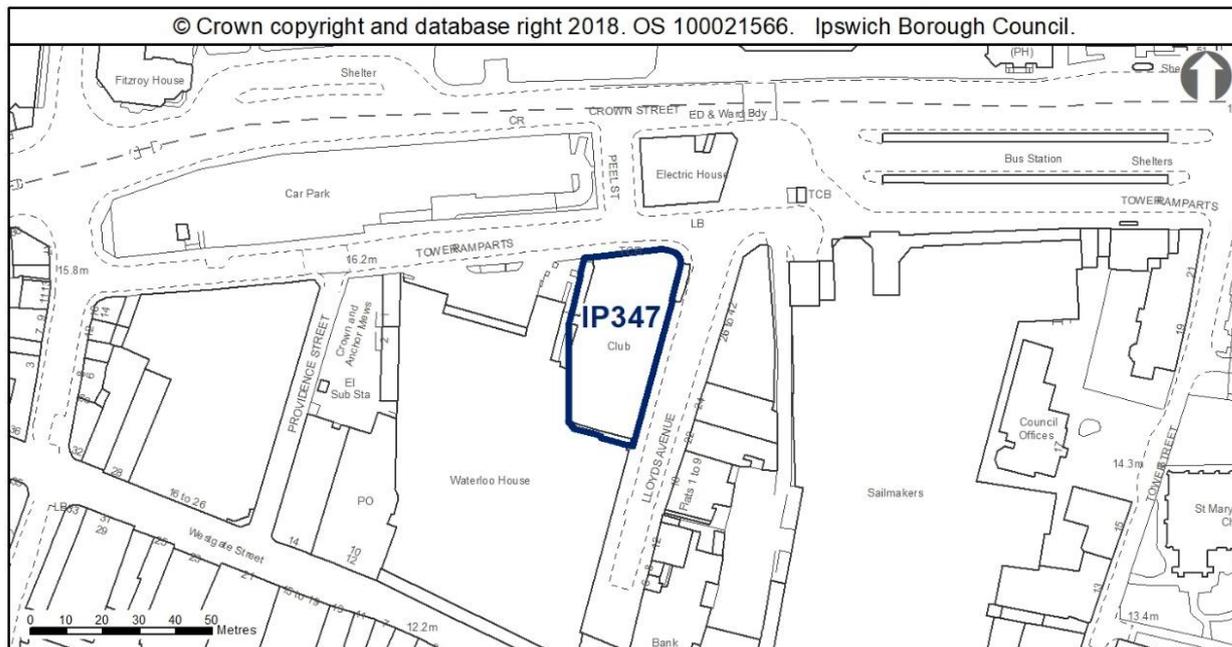
A transport assessment likely to be required. Access constraints onto Austin Street.

The wildlife interest of this site is improving as time progresses. Prior to any development a Preliminary Ecological Appraisal should be undertaken alongside any specific detailed surveys recommended in that report which may include, but not be limited to, botanical, reptiles and invertebrates. Due to the nature of the site, the report should also include the potential for invasive species. The recommendations

of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate.

Site ref: IP347 – Mecca Bingo

Site area: 0.12ha



Allocation Policy SP10

| Use(s) | | Indicative capacity |
|-----------|-------------|---------------------|
| Primary | Retail – A1 | 650sqm |
| Secondary | Bingo | - |

Adopted Plan 2017

N/A – new site

Current use

D2 Bingo Hall

Development constraints / issues

Within the Area of Archaeological Importance, adjacent to Central Conservation Area.

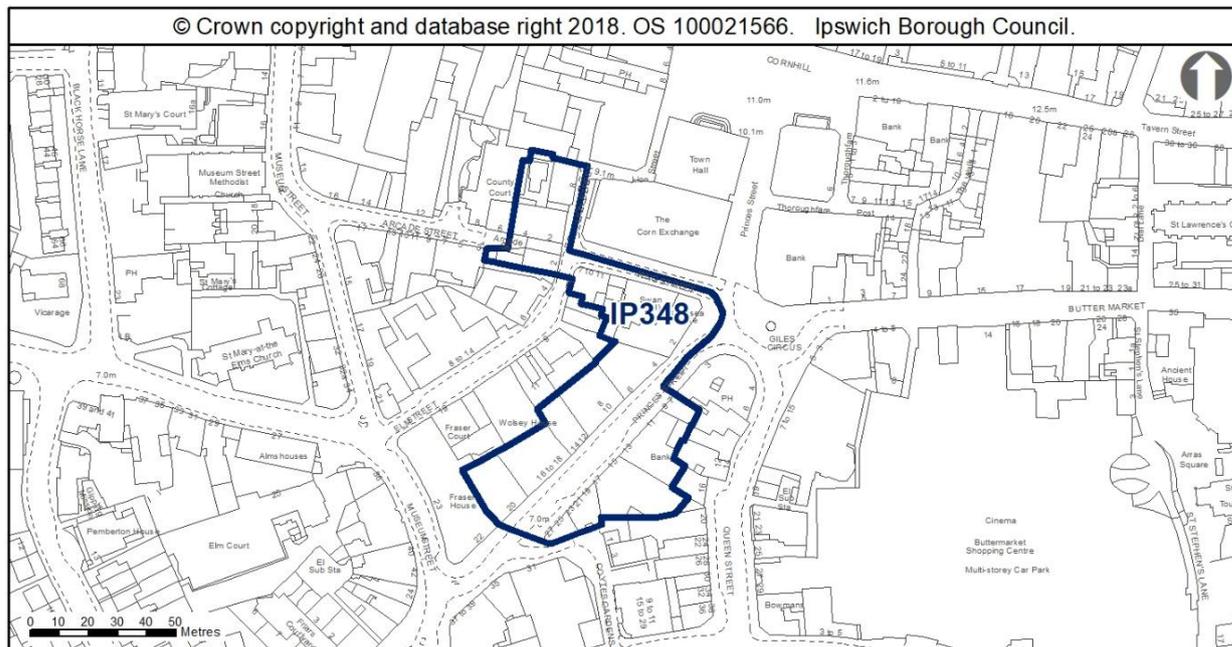
This allocation site is occupied by the Odeon Cinema, erected in 1936, although currently in use as a bingo hall. The building is of the art deco era, with ‘faience’ ceramic cladding, pilasters, decorative urns, and red brickwork. The former use as a cinema is evident through the limited fenestration along the façade of the building.

Elevated on high ground atop Lloyds Avenue, the hall is a dominant building in the streetscape, which contributes positively to the character of the Central Conservation Area and adjacent early 20th century buildings.

The use of this allocation site should respect the art deco design of the building, and avoid alterations which would erode the building's appearance as a 1930s cinema, which is of historic and townscape interest.

Site ref: IP348 – Upper Princes Street

Site area: 0.53ha



Allocation Policy SP10

| Use(s) | | Indicative capacity |
|---------|-------------|---------------------|
| Primary | Retail – A1 | 400sqm |

Adopted Plan 2017

N/A – new site

Current use

A2 Banks and professional services, some vacant units

Development constraints / issues

The site is located in the Central Conservation Area and adjacent to a number of Listed Buildings

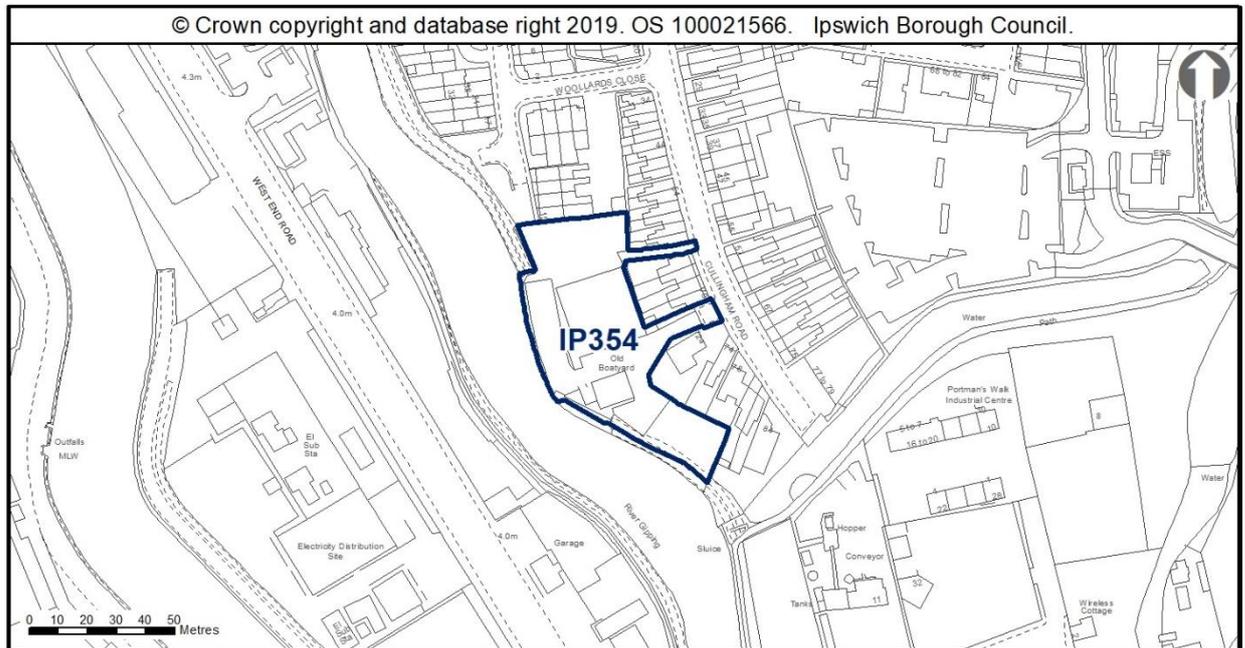
Also within an Area of Archaeological Importance.

The allocation encourages the change of use of existing A2 units to A1.

This allocation site is located within the Central Conservation Area, and includes numerous listed buildings, many of which already have uses which are appropriate and sympathetic to these historic buildings.

These buildings are an important approach to the town centre when approaching from the south, and so uses should make a positive statement upon the entrance to the town, reuse historic shop fronts, and look for opportunities to enhance the Conservation Area with regard to modern shop fronts and signage.

Site Ref: IP354 – 72 (Old Boatyard) Cullingham Road
Site Area: 0.34



Allocation Policy SP2

| Use(s) | | Indicative capacity |
|---------|-------------|--|
| Primary | Residential | 24 (70dph. Less than 90dph due to site constraints and pre-application feedback) |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Plan 2017

N/A – New Site.

Current use

Boatyard.

Development constraints / issues

In Flood Zones 2 and 3.

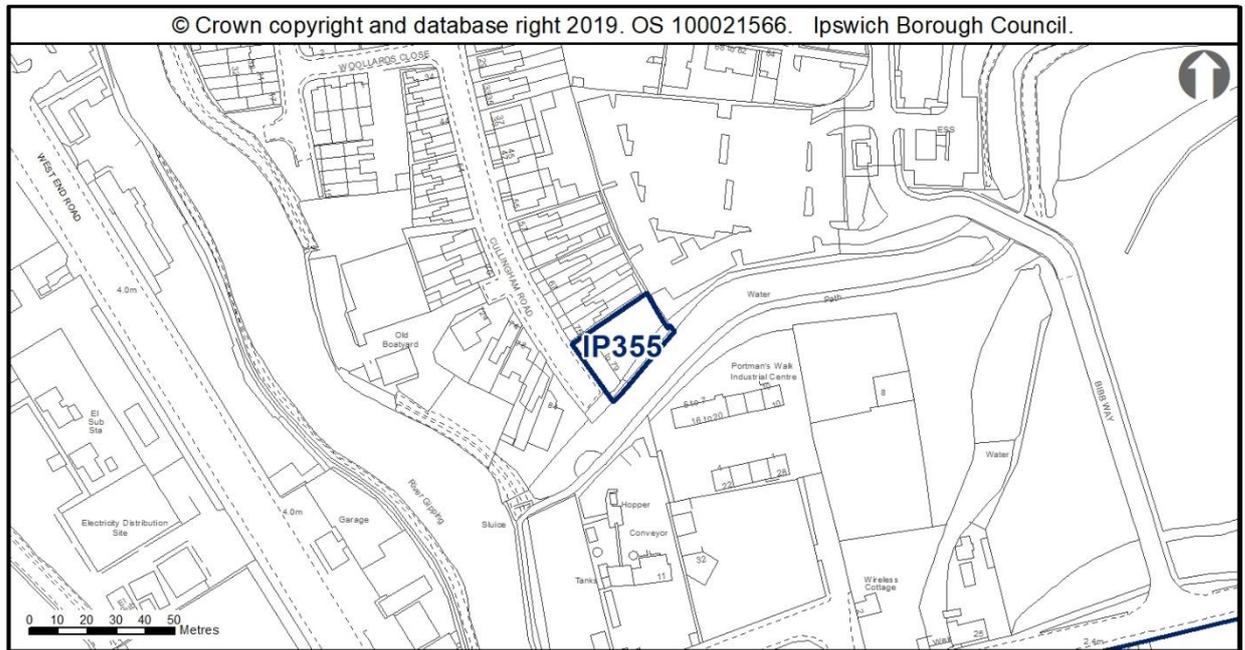
Possible contamination from former use of site.

River Wildlife Corridor immediately adjacent and site within buffer zone.

Development must take account of the River Corridor Buffer (10m) where no development should take place.
In IP-One Opportunity Area G.

Site Ref: IP355 – 77-79 Cullingham Road

Site Area: 0.06ha



Allocation Policy SP2

| Use(s) | | Indicative capacity |
|---------|-------------|---|
| Primary | Residential | 6 (100dph based on location in Portman Quarter) |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Plan 2017

N/A – New Site.

Current use

Light industrial unit.

Development constraints / issues

In Flood Zones 2 and 3.

Possible contamination from former use of site.

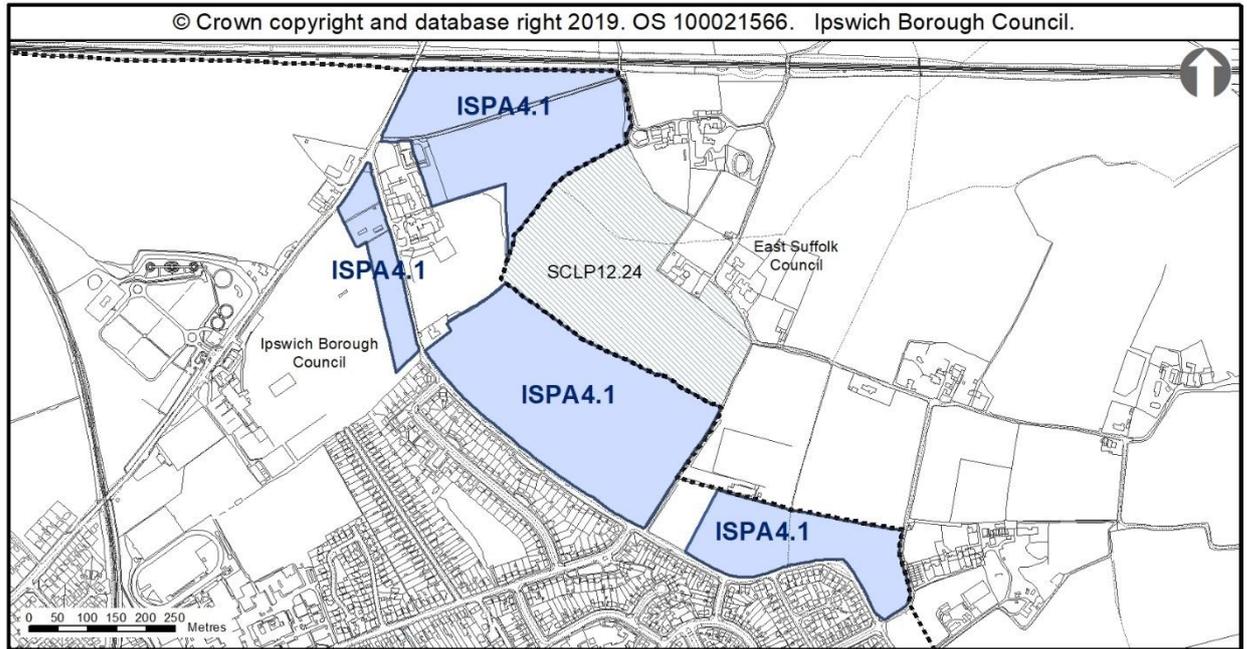
River Wildlife Corridor immediately adjacent and site within buffer zone.

Development must take account of the River Corridor Buffer (10m) where no development should take place.

In IP-One Opportunity Area G.

Development must safeguard capacity for a footpath and cycle path through the site to connect Cullingham Road with Bibb Way.

Site Ref: ISPA 4.1 – Northern End of Humber Doucy Lane
Site Area: 23.62ha (within IBC Land)



Allocation Policy SP2 & ISPA4

| Use(s) | | Indicative capacity |
|-----------|---|----------------------------------|
| Primary | Residential | 496 (35dph on circa 60% of site) |
| Secondary | Green Rim Trail Highways Improvements Early Years Setting Sports Pitches Suitable Accessible Natural Green Space (SANGS) Associated Infrastructure | Approximately 40% of site |

Adopted Plan 2017

N/A – New Site. Previously allocated as Countryside.

Current use

Predominantly greenfield arable land.

Development constraints / issues

The principles and requirements set out in policy ISPA4 of the Core Strategy and Development Management Policies DPD must be followed.

The development of this land will need to be masterplanned along with the adjacent land allocated under the Suffolk Coastal Local Plan Review Policy SCLP12.24.

Development will need to be phased and delivered in coordination with the delivery of the Ipswich Garden Suburb to ensure that there is sufficient infrastructure capacity to meet demand.

An Early Years setting and replacement sports facilities, if needed, will need to be incorporated into any future development. Development will need to make appropriate provision towards primary school places which are likely to be accommodated within the three new primary schools planned through the Ipswich Garden Suburb.

The layout and design of any future development must incorporate the provision of a Green Trail walking and cycling route to help deliver the wider Green Trail around Ipswich. The provision of Suitable Accessible Natural Green Space and other forms of open space will be required in accordance with the Open Space Standards set out in Appendix 5 of the Core Strategy and Development Management Policies DPD.

Highways and junction improvement works along Humber Doucy Lane are likely to be required. A transport assessment and travel plan will be required to ensure that development contributes towards delivering a significant modal shift to sustainable transport modes.

A high-quality of design will be required which also respects the countryside setting around the site. Biodiversity will need to be preserved and must incorporate net gain. The Ipswich Wildlife Audit 2019⁸ provides further information on ecological surveys that will be required, as well as recommendations for how biodiversity net gain can be incorporated into new development, unless other means of biodiversity enhancement are appropriate. There are rows of Tree Preservation Orders (TPOs) along the boundary with Westerfield House that will need to be preserved unless there are overriding reasons for their removal. Where possible existing hedges onto Humber Doucy Lane shall be preserved and protected during the development process as applicable.

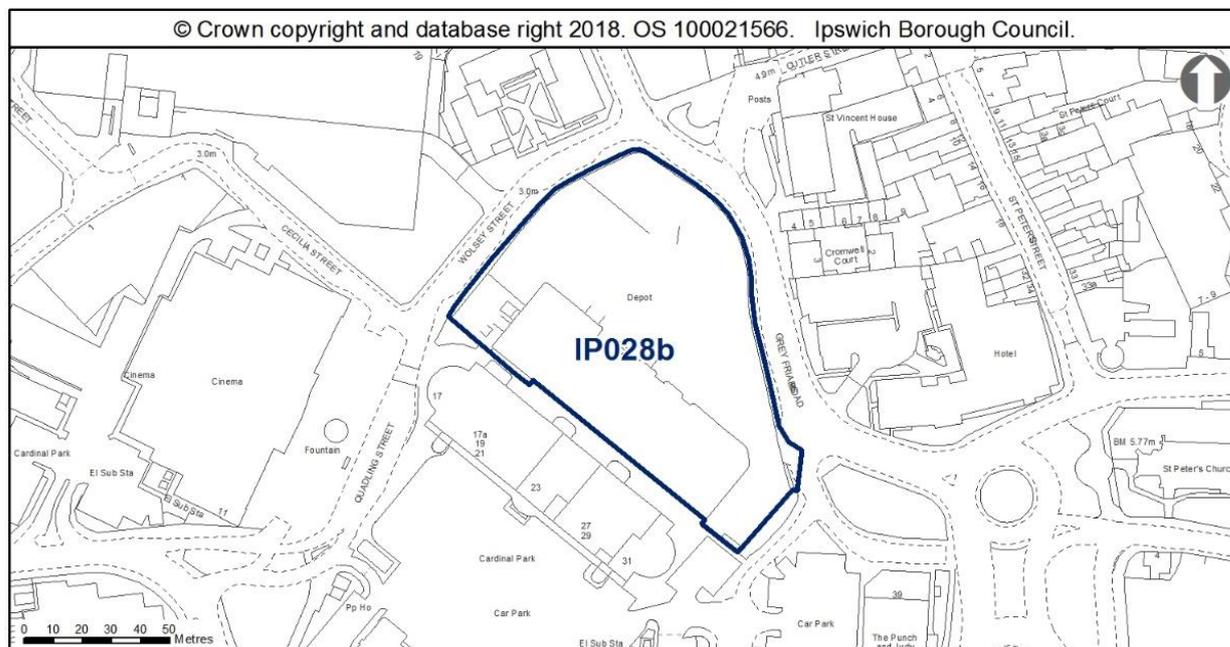
⁸ See Wildlife Audit Site Sheets for IP184a, IP184c, IP303, IP309, IP344 and IP350

The setting of the grade II Listed Westerfield House Hotel must be preserved and enhanced as part of any future development of the site. The development should also seek to preserve the significance of the Listed Buildings to the north and east of the site. These are Allens House, Laceys Farmhouse, and the Garden Store north of Villa Farmhouse.

Appendix 4 – Opportunity Areas

Site ref: IP028b Jewsons, Greyfriars Road

Site Area: 0.90ha



Allocation Policy SP4

| Use(s) | | Indicative capacity |
|-----------|-------------------------------------|-------------------------------|
| Primary | Residential | 40 (90dph on 50% of the site) |
| Secondary | Leisure (A3/A5, D2) Office (B1a) | 4,000sqm |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Plan 2017

N/A – new site but was previously considered as part of a larger site UC029 for 20% housing through IP-One Area Action Plan Preferred Options 2007 and again with IP028a at Preferred Options Stage in January 2019.

Current use

Builders' yard.

Development constraints/issues

This site likely lies in the former extent of the town marsh. Palaeo-environmental assessment and mitigation for impacts on deeper deposits may be required. Deep excavations may encounter waterlogged features. Stratigraphy may be expected to be

particularly deep in former streams and watercourses, and waterlogged features are recorded in the Urban Archaeological Database.

Adjacent to a scheduled ancient monument.

Water infrastructure and/or treatment upgrades will be required to serve the proposed growth, or diversion of assets may be required.

A transport assessment and travel plan will be required.

Surface water flooding local to site - will need to be considered at planning application stage. See Appendix 1 of the Ipswich SFRA.

Adjacent to site with possible contamination, a contaminated land assessment would be required.

This allocation site is presently used as a builders' yard, with a large warehouse to the southwest of the site, with the land along Greyfriars Rose and Wolsey Street principally used for the outdoor storage of materials.

The site is adjacent to the boundary of the Central Conservation Area, and nearby the Grade II* listed Church of St Nicholas and its heritage assets within the churchyard. Some distance to the south is the prominent Church of St Mary at Stoke, which is elevated above the river valley. St Nicholas and St Mary's churches presently benefit from a degree of intervisibility, which needs careful consideration in planning for the site.

Further north along Franciscan Way is the Grade I Willis building. Depending on design, scale and massing, development of the allocation site also has the potential to affect the significance of the Willis building, with the introduction of built mass to the allocation site likely to be visible in the reflective finish of the Willis building.

As the allocation site backs onto Cardinal Park and the rear of the block of commercial units, the tallest part of the development should seek to screen the utilitarian rears of these buildings from Grey Friars Street with a building of high quality design.

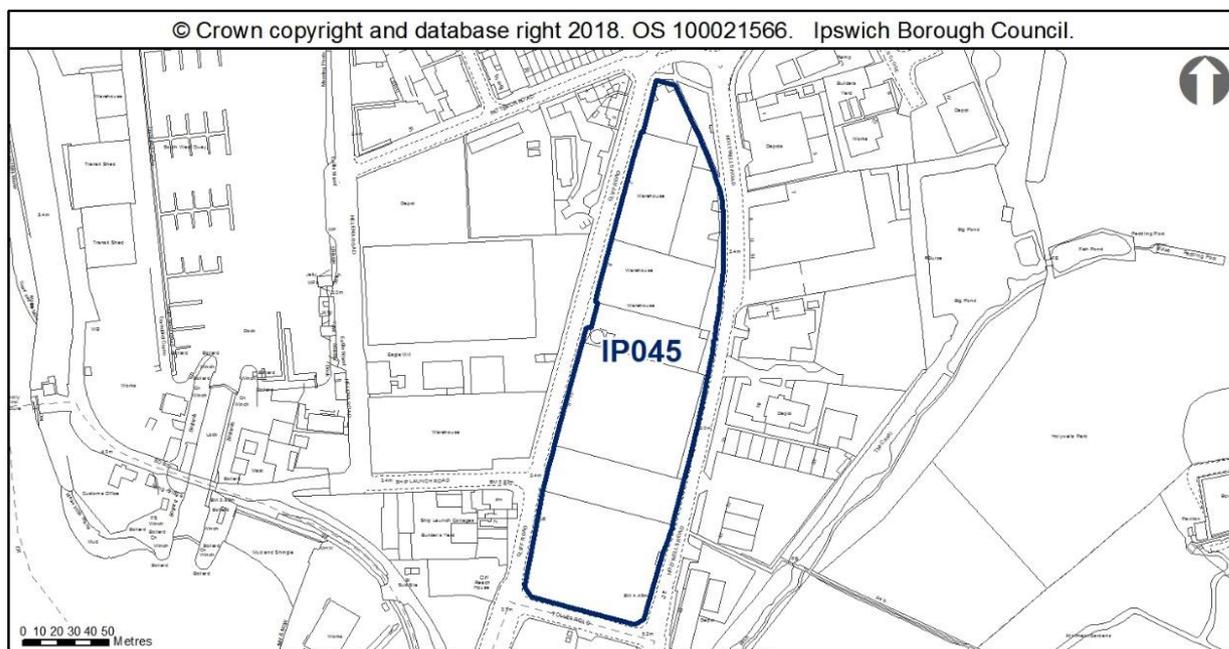
The development of the site should be master planned to ensure that the residential, leisure and office use of the allocation site form a cohesive scheme, with the design and layout of development responding to the varied uses of the site. Redevelopment is dependent on the mitigation of noise from the adjacent nightclub. A contemporary approach to design, would be encouraged, with a contrasting use of scale and form to break up the massing of buildings to provide architectural interest and engaging frontages.

The development of this site should include a high quality, usable public realm, which should encourage users between Cardinal Park, the allocation site and the town centre.

Given the nature of this site, an invasive plant species assessment should be included part of any Preliminary Ecological Assessment to accompany a planning application. To achieve biodiversity net gain, the recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate.

Site ref: IP045 Land bounded by Cliff Road, Toller Road and Holywells Road

Site area: 2.06ha



Allocation Policy SP4

| Use(s) | | Indicative capacity |
|-----------|--|-----------------------------|
| Primary | Residential | 148 (90dph on 80% of site*) |
| Secondary | Employment (B1a uses such as offices) Community, Arts, Culture, Open Space, Tourism | 2,500sqm |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Plan 2017

N/A new site previously considered under Preferred Options 2007 for 50% residential and 50% employment.

Current use

Employment uses, part of employment area E11.

Development constraints / issues

Existing land uses would need to relocate. Potential contaminated land, contaminated land assessment required.

A transport assessment and travel plan will be required.

Site is located in a flood zone; this will need to be considered at planning application stage. See Appendix 1 of the Ipswich SFRA.

The allocation is within 250m of the access to the safeguarded minerals facility, rather than the facility itself. The relationship with the safeguarded facility would need to be considered as part of the masterplanning and design of development.

This site lies on the edge of the historic channel of the Orwell. There is potential for buried historic deposits. Conditions relating to archaeological work are likely to be relevant on any consent. Desk-based assessment and review of geotechnical data would be appropriate in the first instance.

This allocation site is located between the Wet Dock Conservation Area and Holywells Park Conservation Area and presently in use for industrial uses. The site is wedge shaped and occupies land between Cliff Road and Holywells Road, and is located to the north of the cluster of Grade II listed buildings associated with Cliff Quay Brewery, and is opposite The Ship Launch Inn, which is included on the Local List SPD. This allocation site is therefore in a sensitive historic environment which requires careful attention when developing a proposal for the redevelopment of this site, considering the impact on the introduction of development within the setting of these heritage assets, as well as considering views into and out of adjacent Conservation Areas.

The warehouses which presently occupy the site are not of historic or architectural interest, and the comprehensive redevelopment of this site is encouraged. Recent residential development off Patteson Road is of three and four storey domestic scale which utilises polychrome brick banding and timber weatherboarding which has introduced interesting architectural detailing to the area, which the development of this site should look to respond to. The use of masonry utilising textured and polychromatic brick bonds would in the redevelopment of this allocation site would allow for the site to be distinctive whilst also relating to the existing development to the north.

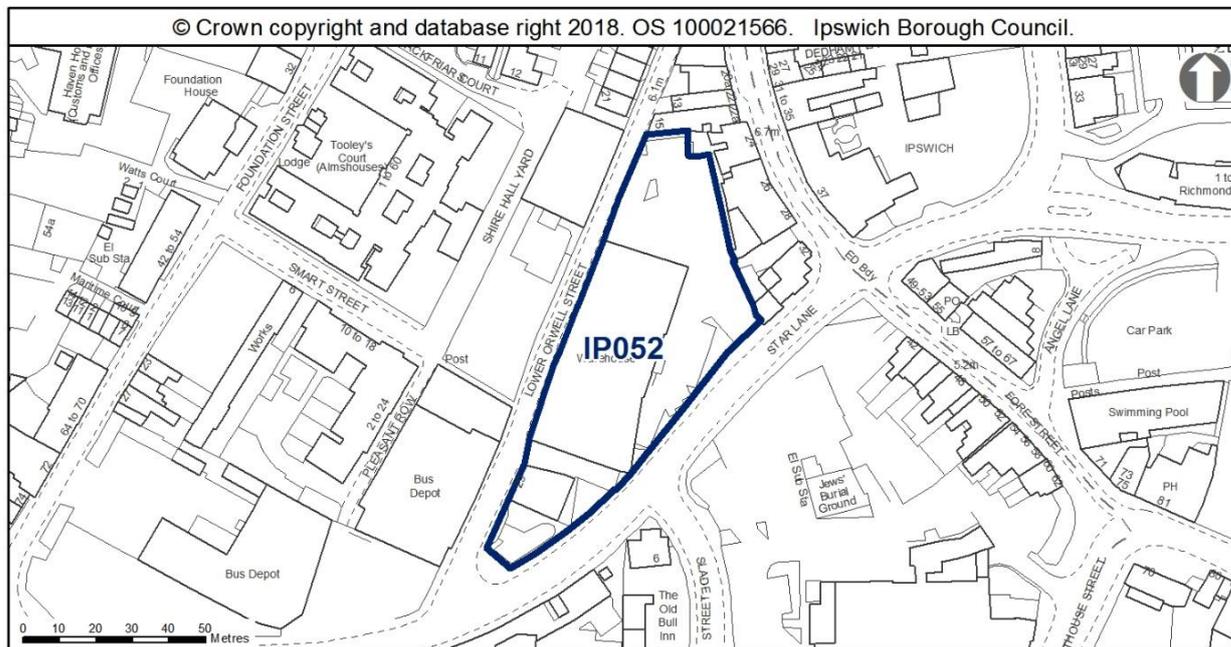
Development should principally be of domestic scale, reflecting the scale established at Patteson Road, however some taller units to accommodate flats could be incorporated into the site, but they should still respect the overriding domestic scale of architecture. Proposals should ensure that new buildings have active elevations to all four frontages which provide visual interest to the street scene, avoiding repetitive fenestration schemes and external finishes.

The proposed secondary use for the site should respect the architectural principles employed for the residential core of the site, responding positively to the design and appearance of the redevelopment site. This secondary use could be concentrated in a landmark building to provide visual interest and break up the massing of surrounding residential development. It should be noted that a landmark building need not necessarily be tall and/or overscaled. A careful, bespoke design respecting the scale of adjacent residential buildings could still provide a building of distinctive landmark quality.

As this site is located close to Holywells Park CWS and is east of the River Orwell CWS, there is an opportunity to create smallscale steppingstone habitats which will in turn contribute to the wider ecological network. To achieve biodiversity net gain, the recommendations of the Ipswich Wildlife Audit 2019 could be incorporated into future development, unless other means of biodiversity enhancement are appropriate.

The site is in close proximity to the Stour and Orwell SPA/ Ramsar site. Information to inform a project level HRA will be required to demonstrate that urbanisation impacts on the site are prevented.

Site ref: IP052 (UC055) Land between Lower Orwell Street & Star Lane
Site area: 0.40ha



Allocation Policy SP4 & SP5

| Use(s) | | Indicative capacity |
|-----------|---|----------------------------|
| Primary | Residential | 29 (90dph on 80% of site*) |
| Secondary | Employment (B1 uses such as offices, research & development, light industrial uses appropriate in a residential area) | 1,000sqm |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Plan 2017

As above but identified through policy SP3

Current use

Vacant warehouse, gym, hand car wash, temporary car parking.

Development constraints / issues

Within an Air Quality Management Area. The site is adjacent to the Central Conservation Area, scheduled monuments, grade II* 24 Fore Street and grade II 26-28 Fore St.

Listed buildings bound the site to the east along Fore Street, whilst development could also affect the wider setting of nearby heritage assets along Foundation Street, Key Street and the waterfront. This allocation site includes a boundary marker, which is included on the Local List (Buildings of Townscape Interest) SPD. This marker is located on Lower Orwell

Street within the rear boundary wall of the central warehouse which occupies the site, however the degree of local listing applies only to the marker, and not the full extent of the wall.

Redevelopment of this allocation site therefore needs to accommodate the locally listed boundary marker. Owing to the historic function of boundary markers to signal the extent of parish boundaries to serve administrative functions, it is important that the boundary marker is located in the same position as existing if the rear wall is to be replaced in the redevelopment of the site.

Development should have regard for the domestic 2-3 storey scale of historic development along Fore Street which backs onto this site, and should ensure to address both Lower Brook Street and Star Lane, providing a landmark element which turns the corner at the junction of these routes. The building line could be set back from Star Lane, or provide a staggered frontage so as not to create a tunnelling effect, particularly to the south west of the side adjacent to Premier Inn. Development along Lower Orwell Street should look to enliven the street scene, which is currently overshadowed by the trees to the west of the site which are covered by a TPO, and owing to the lack of fenestration along the rear of the allocation site, provide Lower Orwell Street with a particularly dark and unwelcoming character.

Improvements to the public realm should also be incorporated to the development of the allocation site, improving the pedestrian experience of Star Lane and accessing the waterfront from the town centre, and also allowing for pedestrian access through the site east to west.

Possible contamination and part within a flood zone.

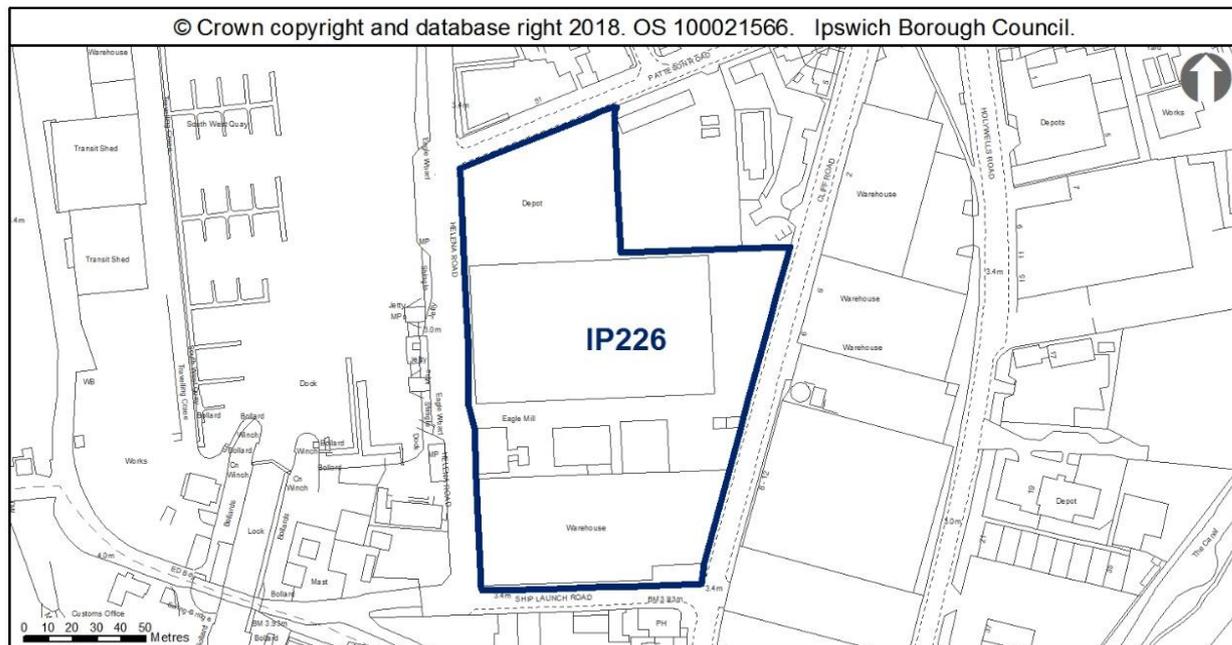
Development principles for the Merchant Quarter, within which the site is located, are set out in Chapter 6 of the Site Allocations and Policies plan (see 'Opportunity Area B').

This site is within the urban core and the Area of Archaeological Importance (IPS 413) and close to scheduled areas of Middle Saxon and medieval occupation (NHLE 1005985 and NHLE 1002966). There is a potential for nationally important archaeological remains outside of scheduled areas. Detailed early pre-application discussions with Suffolk County Council Archaeological Service would be required to agree the scope of required assessment and to inform design (e.g. to allow for preservation in-situ of deposits or appropriate programmes of work). Archaeological remains may be complex and important and mitigation could involve significant costs and timescales.

Any opportunity to widen the Star Lane footways through redevelopment would be welcomed, to deliver potential improvements to the visual, pedestrian, cyclist and air quality environment of Star Lane.

Site ref: IP226 Helena Road/Patteson Road

Site area: 1.87ha



Allocation Policies SP4

| Use(s) | | Indicative capacity |
|-----------|---|---|
| Primary | Residential | 337 dwellings (200dph on 90% of the site) |
| Secondary | Uses compatible with Waterfront Policy SP11 | |

* see Core Strategy policy DM23 for minimum and average densities.

Adopted Plan 2017

Site was previously safeguarded under SP3 of the plan but has not come forward for development. Previously 540 dwellings at high density.

Current use

Timber storage

Development constraints / issues

Historic planning application for 566 dwellings, which has now expired.

Potential contamination land and a contaminated land assessment is required

Adjacent to a Listed Building and the Wet Dock Conservation Area. The site lies on the edge of the channel of the Orwell. There is potential for buried historic deposits. No objection in

principle to development but it will require a condition relating to archaeological investigation attached to any planning consent.

This allocation site is located immediately adjacent to the Wet Dock Conservation Area to the west, occupying a key waterfront position towards the south of the Conservation Area. The Ship Launch Inn is to the immediate south of the allocation site, which is included on the Local List (Buildings of Local Townscape Interest) SPD.

The site is occupied by warehousing to the south, with the northern section being used as a depot. The southernmost building is a large brick warehouse dating from the early 20th century, and does have some architectural interest, with polychromatic masonry, round headed windows, and three prominent gables. This building could be reused in the redevelopment of the site, utilising the existing openings and lantern roof to provide natural light to the development.

The remainder of the site holds little architectural interest, and the replacement of these buildings would be encouraged. Development should address Helena Road which faces towards the waterfront with an interesting, eye catching design which contributes positively to the Wet Dock Conservation Area. Development should encourage pedestrian movement around the waterfront. The western side of the allocation site could accommodate taller development, whilst stepping down to a more domestic scale eastwards.

The design of development should also recognise Cliff Road to the east as a principal elevation and provide an expressive frontage which engages with the street scene to the west. A contemporary design approach would be welcomed, responding to adjacent architectural influences, such as the polychromatic masonry in the southern warehouse, and also to the north at the recent development off Patteson Road.

The site is in close proximity to the Stour and Orwell SPA/ Ramsar site. Information to inform a project level HRA will be required to demonstrate that urbanisation impacts on the site are prevented.